



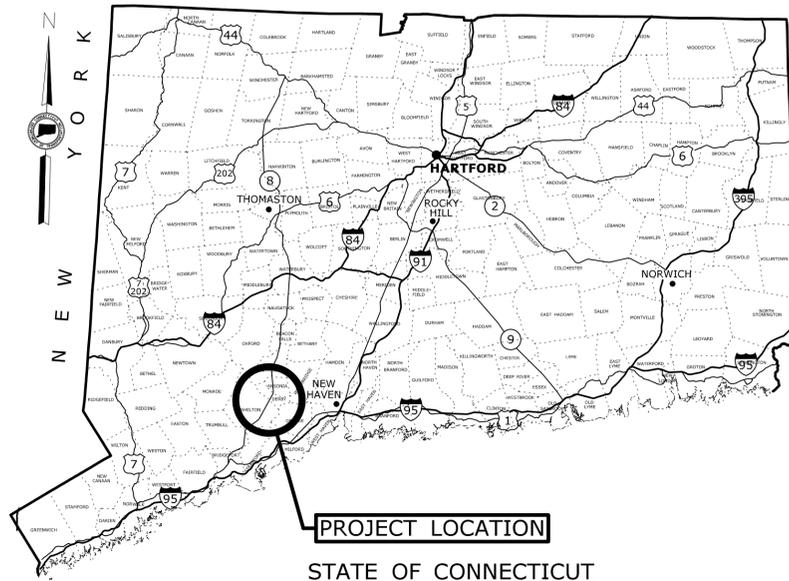
# CONNECTICUT DEPARTMENT OF TRANSPORTATION



Plans For

## COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS

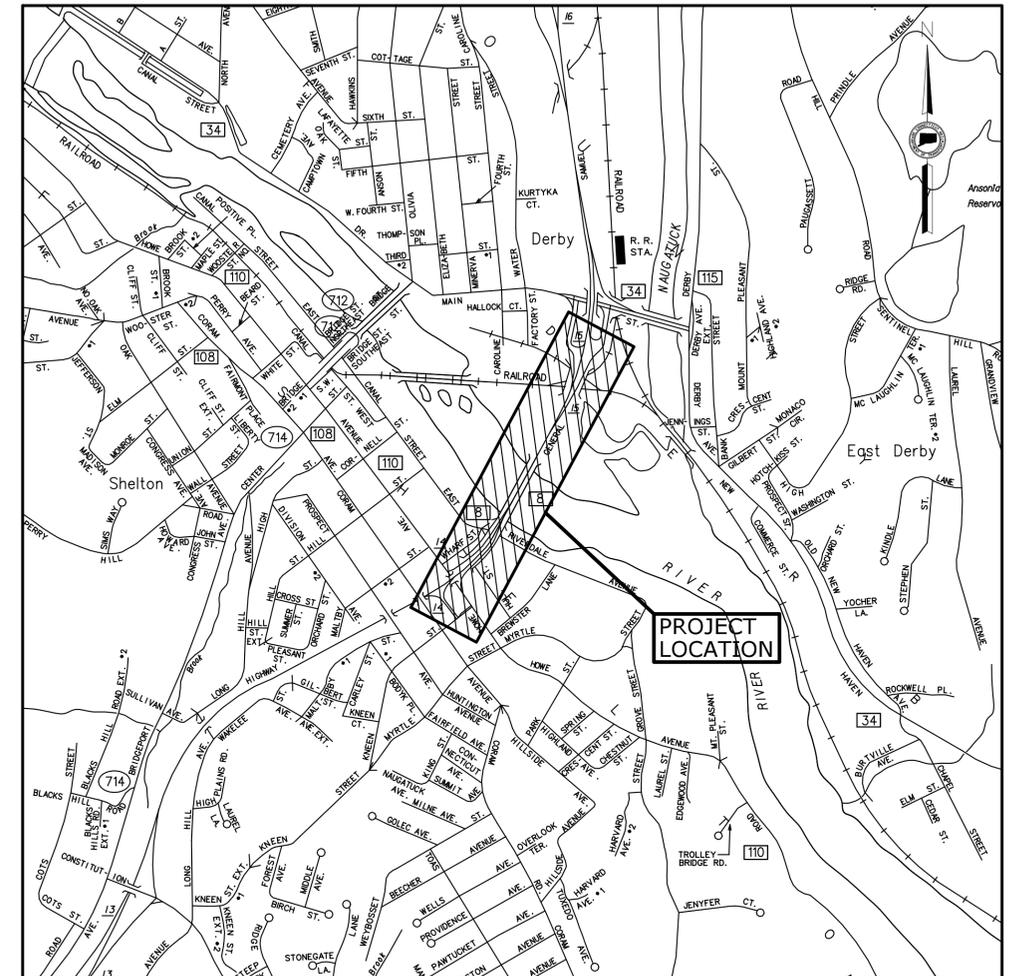
M A S S A C H U S E T T S



R H O D E I S L A N D

### Town of SHELTON DERBY

ROAD	MAINTENANCE RESPONSIBILITY	LENGTH
ROUTE 8	STATE	1573.67 FEET
F.A.P. #	MAINTENANCE RESPONSIBILITY	PROJECT #
###(###)	STATE	126-170



LOCATION PLAN  
NOT TO SCALE

#### GENERAL NOTES:

- FEDERAL AID PROJECT NO.
- CONSTRUCTION SPECIFICATIONS:  
Connecticut Department of Transportation, Standard Specifications for Roads, Bridges and Incidental Construction, Form 816, dated 2004; Supplemental Specifications, dated July 2015; and Special Provisions
- BASED ON CONNECTICUT COORDINATE SYSTEM N.A.D. 1983
- VERTICAL DATUM BASED ON NAVD 88

#### DISCLAIMER

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#### LIST OF SUBSETS

SUBSET NO.	SUBSET TITLE	SUBSET SHEET COUNT
1	GENERAL	X
2	REVISIONS	X
3	ROADWAY	XX
4	STRUCTURE	XX

#### LIST OF DRAWINGS 01 - GENERAL

DRAWING TITLE	DRAWING NO.
TITLE SHEET	TSH-01
DETAILED ESTIMATE SHEETS	EST-01

#### STANDARD CONVENTIONS

North Arrow W/No. Coord.	Grid Arrow	Chain Link Fence	Riprap
Edge Of Road	Limit Of Marsh	Rustic Fence	Hedge Row
Concrete Pavement	Stone Wall	Pipe Fence	Tree Line
Dirt Road	Ledge Outcrop	Board Fence	Shrub
B.C.L.C.	Inland Wetland Limits	Water Edge	Evergreen Tree
Concrete Curb	STATE LINE	Stream	Deciduous Tree
Guide Rail	Power Line	Ditch	Retaining Wall
Concrete Median Barrier	Swamp	TOWN LINE	Highway Line
Bit. Walk	Building	Transmission Tower	Street Line
Conc. Sidewalk			Property Line
Railroad Tracks			Lot Line
			Easement Line

DESIGNED BY:

AECOM  
500 ENTERPRISE DRIVE  
ROCKY HILL, CT 06067

THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

SUBMITTED BY: TRANSPORTATION PRINCIPAL ENGINEER -

APPROVAL RECOMMENDED BY: MANAGER OF -

APPROVED BY: TRANSPORTATION ENGINEERING ADMINISTRATOR - JAMES H. NORMAN P.E.

90% PROGRESS SUBMISSION

#### Plans For COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS

Town of  
SHELTON  
DERBY

STATE PROJECT NO. **126-170**

DRAWING NO. **TSH-01**  
SHEET NO. **01.001**









# 04 - STRUCTURE INDEX OF DRAWINGS

DRAWING NUMBER	DRAWING TITLE	DRAWING NUMBER	DRAWING TITLE
IND-01	STRUCTURE INDEX OF DRAWINGS	S-21	STEEL REPAIR DETAILS 3
S-1	GENERAL PLAN & ELEVATION	S-22	CONSTRUCTION STAGE STEEL BENT AT PIER 8
S-2	QUANTITIES AND NOTES	S-23	CONSTRUCTION STAGE STEEL BENT AT PIER 11
S-3	TEMPORARY BARRIER DETAIL	S-24	REPLACEMENT OF STEEL BENTS AT PIER 8
S-4	TYPICAL BRIDGE SECTIONS - SPANS 1-5 & 12	S-25	REPLACEMENT OF STEEL BENTS AT PIER 11
S-5	TYPICAL BRIDGE SECTIONS - SPANS 6 THRU 8	S-26	PLAN AND ELEVATION OF RIVER TRUSS SPANS
S-6	TYPICAL BRIDGE SECTIONS - SPAN 9	S-27	INSPECTION WALKWAY MODIFICATION DETAILS
S-7	TYPICAL BRIDGE SECTIONS - SPANS 10 & 11	S-28	BEARINGS - PIER 8 & PIER 11
S-8	SUBSTRUCTURE REPAIRS 1	S-29	FIXED BEARING RESTRAINTS AT TRUSS SPANS
S-9	SUBSTRUCTURE REPAIRS 2	S-30	FIXED BEARING RESTRAINTS AT APPROACH SPANS
S-10	SUBSTRUCTURE REPAIRS 3	S-31	LIMITS OF FENCING AND DRAINAGE REPAIR
S-11	SUBSTRUCTURE REPAIRS 4	S-32	DECK DRAINAGE REPAIR DETAILS
S-12	SUBSTRUCTURE REPAIRS 5	S-33	EXTEND EXISTING WEEPHOLE DETAILS
S-13	CONCRETE REPAIR DETAILS	S-34	FENCE RESET DETAILS
S-14	FRAMING PLAN 1 OF 3	S-35	FENCE REPAIR DETAILS
S-15	FRAMING PLAN 2 OF 3	S-36	EXPANSION JOINT DETAILS
S-16	FRAMING PLAN 3 OF 3	S-37	PARAPET ANCHORAGE DETAILS 1 OF 2
S-17	APPROACH SPANS PAINTING DETAILS	S-38	PARAPET ANCHORAGE DETAILS 2 OF 2
S-18	SPANS 9, 10 & 11 PAINTING DETAILS	S-39	MISCELLANEOUS DETAILS
S-19	STEEL REPAIR DETAILS 1	S-40	ELECTRICAL LIGHTING PLAN
S-20	STEEL REPAIR DETAILS 2	S-41	ELECTRICAL DETAILS

DESIGNED BY:  
  
  
 AECOM  
 500 ENTERPRISE DRIVE  
 ROCKY HILL, CT 06867

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 10/1/2015

DESIGNER/DRAFTER:  
G. HRICKO  
 CHECKED BY:  
C. CHUANG



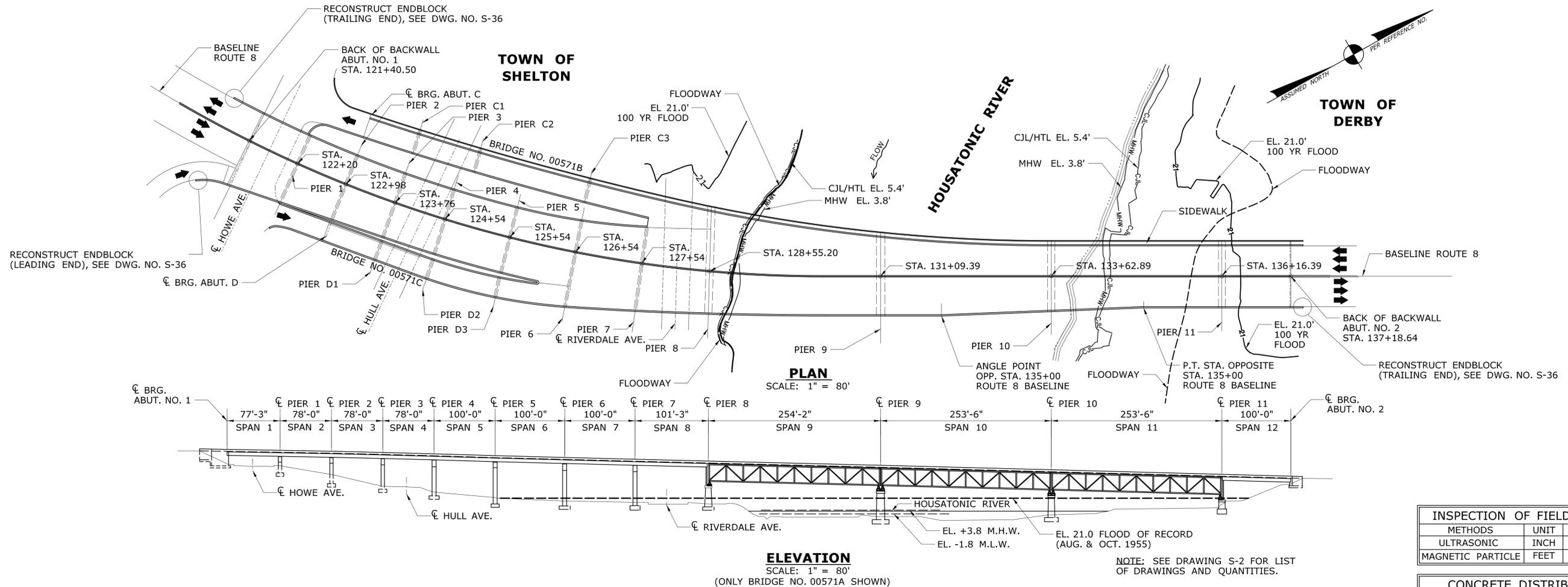
PROJECT TITLE:  
  
**COMMODORE HULL BRIDGE  
PAINTING AND STEEL REPAIRS**

TOWN:  
  
**SHELTON  
DERBY**

DRAWING TITLE:  
  
**04 - STRUCTURE  
INDEX OF DRAWINGS**

PROJECT NO.  
**126-170**  
 DRAWING NO.  
**IND-01**  
 SHEET NO.  
**04.000**

10/1/2015 P:\Resources\Structural\Public\Commodore Hull\Struct\_Bridge\MSA\_Sheet\_Files\SB\_Br00571A\_126-170\_V4-COVER.dgn



- GENERAL DESCRIPTION OF WORK**
- REMOVE EXISTING PAINT AND FIELD PAINT BEAM ENDS, TRUSS ENDS AND TRUSS NODES WITHIN LIMITS SHOWN ON PLANS.
  - REPAIR EXISTING DETERIORATED STRUCTURAL STEEL AS INDICATED ON THE PLANS.
  - PATCH EXISTING CONCRETE PIERS AND ABUTMENTS AS INDICATED ON THE PLANS.
  - REPAIR EXISTING BRIDGE FENCING AND BRIDGE DRAINAGE WITHIN LIMITS SHOWN ON THE PLANS.
  - EXTEND PVC PIPE WEEPHOLES AS SHOWN ON THE PLANS.
  - REPLACE/MODIFY EXISTING LOWER CATWALK RAILING.
  - REPLACE EXISTING NAVIGATIONAL LIGHTS.
  - REPLACE PORTIONS OF STEEL PIER BENTS AT PIER NO. 8 AND ENTIRE OF STEEL PIER BENTS AT PIER NO. 11.
  - MODIFY PARAPET ANCHORAGE END BLOCK TRANSITIONS TO RB-350 METAL BEAM RAIL.
  - PROVIDE RESTRAINERS FOR FIXED BEARINGS AT APPROACH SPANS AND TRUSS SPANS TO PREVENT UPLIFT.

\* COMPLETE WORK 4 & 5 PRIOR TO OTHER WORKS

**GENERAL NOTES:**

SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 816, SUPPLEMENTAL SPECIFICATIONS DATED JULY 2015 AND AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO STANDARD BRIDGE DESIGN SPECIFICATIONS (1996), AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003).

ALLOWABLE DESIGN STRESSES:  
 CLASS "F" CONCRETE: BASED ON F'C = 4,000 PSI  
 CLASS "S" CONCRETE: BASED ON F'C = 3,000 PSI  
 REINFORCEMENT (ASTM A615 GRADE 60): FY = 60,000 PSI  
 STRUCTURAL STEEL (AASHTO M270, GRADE 50): FY = 50,000 PSI

LIVE LOAD: HS-20.

FUTURE PAVING ALLOWANCE: NONE

STRUCTURAL STEEL: SEE "STEEL REPAIR DETAIL 1" SHEET FOR DESIGNATIONS AND REQUIREMENTS.

PAINT: PAINT SHALL CONFORM TO THE REQUIREMENTS OF THE SPECIAL PROVISION "ABRASIVE BLAST CLEANING & FIELD PAINTING OF STRUCTURE (SITE NO. 1)" THE COLOR OF THE TOPCOAT MATERIAL ON THE STRUCTURAL STEEL SHALL CONFORM TO FEDERAL STANDARD 595B COLOR NO. FS 26515 (GREY).

DIMENSIONS: WHEN DECIMAL DIMENSIONS ARE GIVEN TO LESS THAN THREE DECIMAL PLACES, THE OMITTED DIGITS SHALL BE ASSUMED TO BE ZEROS.

EXISTING DIMENSIONS: DIMENSIONS, ANGLES, AND ELEVATIONS OF THE EXISTING STRUCTURE SHOWN ON THESE PLANS ARE FOR GENERAL REFERENCE ONLY. THEY HAVE BEEN TAKEN FROM THE ORIGINAL DESIGN DRAWINGS AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL TAKE ALL FIELD MEASUREMENTS NECESSARY TO ASSURE PROPER FIT OF THE FINISHED WORK AND SHALL ASSUME FULL RESPONSIBILITY FOR THEIR ACCURACY. WHEN SHOP DRAWINGS BASED ON FIELD MEASUREMENTS ARE SUBMITTED FOR APPROVAL, THE FIELD MEASUREMENTS SHALL ALSO BE SUBMITTED FOR REFERENCE BY THE REVIEWER.

JOINT SEAL: SEE SPECIAL PROVISIONS.

EXPOSED EDGES: EXPOSED EDGES OF CONCRETE SHALL BE BEVELED 1" X 1" UNLESS DIMENSIONED OTHERWISE.

CONCRETE COVER: ALL REINFORCEMENT SHALL HAVE TWO INCHES COVER UNLESS DIMENSIONED OTHERWISE.

CONSTRUCTION JOINTS: CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN ON THE PLANS, WILL NOT BE PERMITTED WITHOUT PRIOR APPROVAL OF THE ENGINEER.

REINFORCEMENT: ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.

FLAME CUTTING ON OR ADJACENT TO STEEL MEMBERS WHICH ARE TO REMAIN IS NOT PERMITTED.

PAINT AND DEBRIS COLLECTED FROM PAINT REMOVAL SHALL BE STORED OUTSIDE THE 500 YEAR FLOOD ZONE.

TEMPORARY STRUCTURES, SUCH AS WORK PLATFORMS, DEBRIS CONTAINMENT SYSTEM, ETC., SHALL NOT AFFECT CLEARANCES ON NAVIGABLE CHANNEL AND VEHICULAR TRAFFIC.

EXISTING PLANS: PLANS FOR THE EXISTING BRIDGE ARE AVAILABLE AT THE CONNECTICUT DEPARTMENT OF TRANSPORTATION PLAN ROOM, 160 PASCOE PLACE, NEWINGTON.

INSPECTION OF FIELD WELDS		
METHODS	UNIT	QUANTITY
ULTRASONIC	INCH	000
MAGNETIC PARTICLE	FEET	000

CONCRETE DISTRIBUTION		
LOCATION	UNIT	QUANTITY
SUPERSTRUCTURE	C.Y.	0
SUBSTRUCTURE	C.Y.	0
TOTAL	C.Y.	0

A FLOOD MANAGEMENT GENERAL CERTIFICATION HAS BEEN OBTAINED TO PERFORM THE PROPOSED WORK. TEMPORARY WORK PLATFORMS, STAGING OR EQUIPMENT THAT EXTEND BELOW THE REGULATORY FLOOD ELEVATION MUST BE ABLE TO BE REMOVED IN A TIMELY MANNER IN THE EVENT OF A FLOOD WARNING. ELEVATIONS SHOWN UNLESS OTHERWISE NOTED WERE TAKEN FROM THE ORIGINAL CONTRACT PLANS AND ARE BASED ON THE NGVD29 DATUM.

HYDRAULIC DATA - TEMPORARY FACILITIES		
DRAINAGE AREA	1,578	SQ. MI.
DESIGN FREQUENCY - TEMPORARY	3	YR
DESIGN DISCHARGE - TEMPORARY	25,000	CFS
WATER SURFACE ELEVATION - TEMPORARY	9.3	FT-NAVD88

NOTICE TO BRIDGE INSPECTORS	
THE DEPARTMENT'S BRIDGE SAFETY PROCEDURES REQUIRE THIS BRIDGE TO BE INSPECTED FOR, BUT NOT LIMITED TO, ALL APPROPRIATE COMPONENTS INDICATED IN THE GOVERNING MANUALS FOR BRIDGE INSPECTION. ATTENTION MUST BE GIVEN TO INSPECTING THE FOLLOWING SPECIAL COMPONENTS AND DETAILS. (THE LISTING FOR COMPONENTS FOR SPECIFIC ATTENTION SHALL NOT BE CONSTRUED TO REDUCE THE IMPORTANCE OF INSPECTION OF ANY OTHER COMPONENT OF THE STRUCTURE.) THE FREQUENCY OF INSPECTION OF THIS STRUCTURE SHALL BE IN ACCORDANCE WITH THE GOVERNING MANUALS FOR BRIDGE INSPECTION, UNLESS OTHERWISE DIRECTED BY THE MANAGER OF BRIDGE SAFETY AND EVALUATION.	
COMPONENT OR DETAIL	STRUCTURE SHEET REFERENCE
FOLLOW NORMAL INSPECTION PROCEDURES	

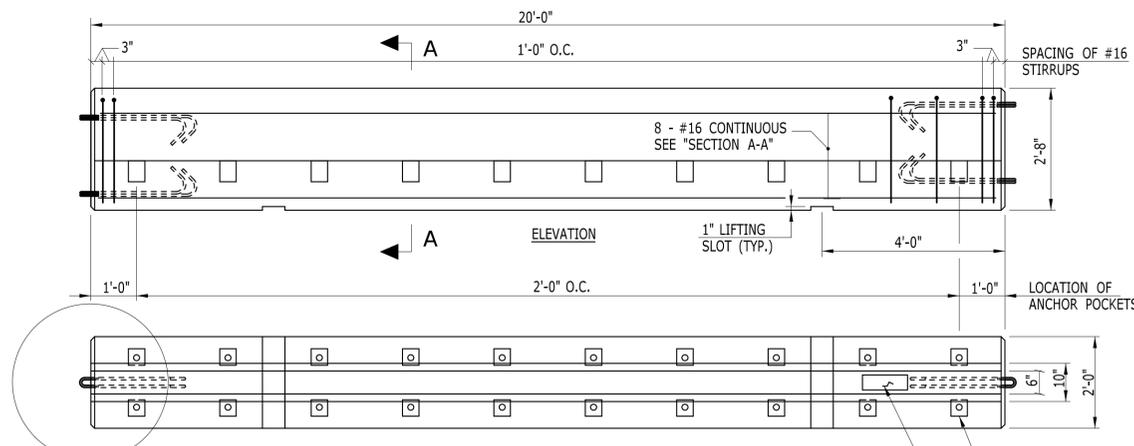
**90% PROGRESS SUBMISSION**

DESIGNER/DRAFTER: <b>KAB/ALH</b>	CHECKED BY: <b>CC</b>	SCALE AS NOTED	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SIGNATURE/BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b>
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.			FILENAME: ...\\SB_MSH_Br00571A_126-170_GPN1.dgn			DRAWING TITLE: <b>GENERAL PLAN AND ELEVATION</b>	DRAWING NO. <b>S-01</b>
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015			SHEET NO.

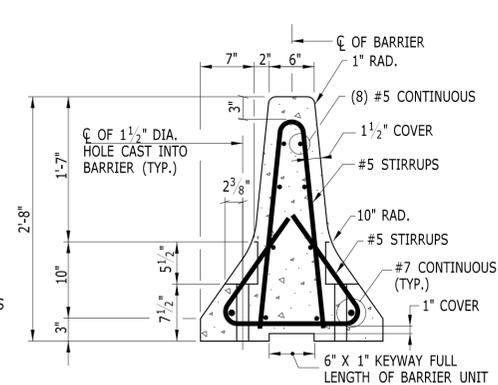
QUANTITIES		
ITEM	UNIT	AMOUNT
WATER TRANSPORTATION FOR INSPECTION PERSONNEL	DAY	120
REMOVAL OF EXISTING STEEL BENTS (SITE NO. 8)	L.S.	L.S.
REMOVAL OF EXISTING STEEL BENTS (SITE NO. 11)	L.S.	L.S.
REMOVAL OF PARAPET	C.Y.	3
CLEAN EXISTING SCUPPERS	EA.	30
REMOVAL AND REPLACEMENT OF EXISTING BRIDGE DRAINAGE SYSTEM	L.F.	50
8" PIPE FOR BRIDGE DRAINAGE (FIBERGLASS)	L.F.	6000
PERFORMED JOINT SEAL	L.F.	100
REPLACE BRIDGE BEARINGS	EA.	28
KEEPER ASSEMBLY	EA.	40
CLASS "S" CONCRETE	C.Y.	8
VARIABLE DEPTH PATCH	C.F.	140
CLASS "F" CONCRETE	C.Y.	3
EPOXY INJECTION CONCRETE REPAIR	L.F.	90
DEFORMED STEEL BARS - EPOXY COATED	LB	600
DRILLING HOLES AND GROUTING DOWELS	EA.	30
DRILLING HOLES AND GRINDING ANCHOR BOLTS	EA.	120
FIELD TOUCHUP PAINTING	S.F.	14500
DISPOSAL OF LEAD DEBRIS	BBL.	10
DECONTAMINATION FACILITY	MO	9
PORTABLE HANDWASH FACILITY	MO	9
TEMPORARY SUPPORT SYSTEM (PIER 11)	L.S.	L.S.
WALKWAY MODIFICATIONS	L.S.	L.S.
LEAD HEALTH PROTECTIN PROGRAM (LHPP)	EST.	EST.
CLASS 1 - CONTAINMENT AND COLLECTION OF SURFACE PREPARATION DEBRIS (SITE #1)	L.S.	L.S.
STRUCTURAL STEEL	LB.	8000
STRUCTURAL STEEL (SITE NO. 8)	L.S.	L.S.
ABRASIVE BLAST CLEANING AND FIELD PAINTING OF STRUCTURE (SITE NO. 1)	L.S.	L.S.
STRUCTURAL STEEL (SITE NO. 11)	L.S.	L.S.
REPAIR METAL BRIDGE RAIL	L.S.	L.S.
RESET METAL BRIDGE RAIL	L.F.	960
R-B 350 BRIDGE ATTACHMENT TRAILING END 10 GA.	EA.	2
R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET 10 GA.	EA.	1
1" RMC PVC COATED - SURFACE	L.F.	700
2" RMC PVC COATED - SURFACE	L.F.	100
REMOVE CONDUIT	L.F.	800
18"x18"x10" CAST IRON JUNCTION BOX	EA.	2
STEEL JUNCTION BOX COVER	EA.	6
1" LIGUID TIGHT FLEXIBLE METAL CONDUIT	L.F.	100
CABLE IN DUCT (THREE NO. 2)	L.F.	300
CABLE IN DUCT (TWO NO. 10 AND NO. 10 GROUND)	L.F.	2100
CABLE IN DUCT (NO. 8 GROUND)	L.F.	100
STEP DOWN TRANSFORMER, 1KVA	EA.	1
NAVIGATION LIGHTS	EA.	6
REMOVE NAVIGATION LIGHTS	EA.	6
EMBEDDED GALVANIC ANODES	EA.	20

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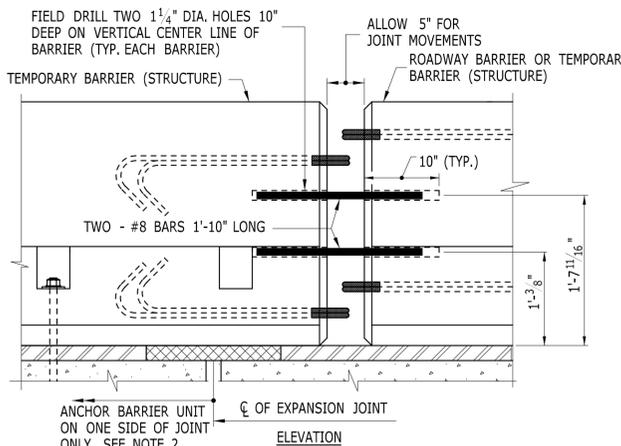
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	DESIGNER/DRAFTER: <b>KAB/ALH</b>	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b>
				CHECKED BY: <b>CC</b>	SCALE AS NOTED				FILENAME: ..._SB_MSH_Br00571A_126-170_GPN2.dgn	DRAWING TITLE: <b>QUANTITIES AND NOTES</b>



**TEMPORARY BARRIER (STRUCTURE)**  
SCALE: 1/2" = 1'-0"

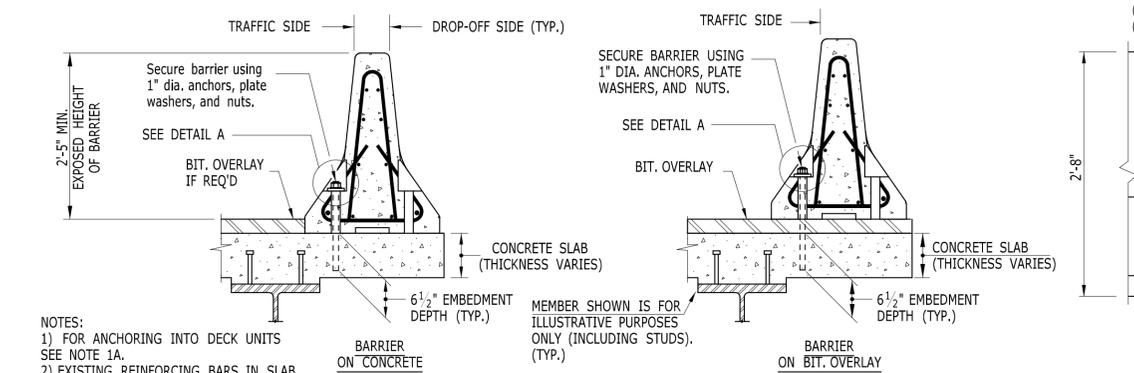


**SECTION A-A**  
SCALE: 1" = 1'-0"

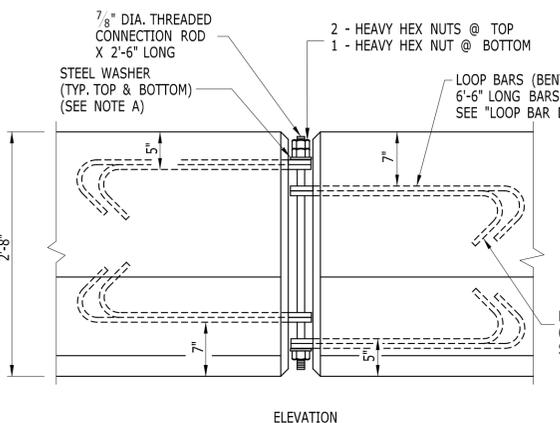


**BARRIER CONNECTION DETAILS AT EXPANSION JOINTS (CASE II SHOWN)**  
SCALE: 1" = 1'-0"

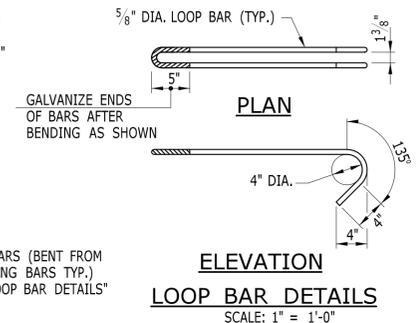
- NOTES**
- THE TEMPORARY BARRIER SHOWN ON THIS SHEET SHALL BE ANCHORED ONTO BRIDGE DECKS (SEE "TEMPORARY ANCHORAGE SYSTEM") WHEN IT IS USED TO PROTECT A VERTICAL DROP-OFF. THE TEMPORARY ANCHORAGE SYSTEM SHALL CONFORM TO THE FOLLOWING:
    - PRESTRESSED DECK UNITS: THREADED INSERTS SHALL BE USED FOR SECURING TEMPORARY BARRIER (STRUCTURE) TO PRESTRESSED DECK UNITS. THE THREADED INSERTS SHALL BE CAST INTO THE DECK UNITS DURING FABRICATION, AND SHALL BE LOCATED AS REQUIRED TO ACCOMMODATE THE STAGE CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
    - CHEMICAL ANCHORING: THIS CONSISTS OF DRILLING HOLES IN NEW OR EXISTING CONCRETE, PLACING REMOVABLE ANCHORS IN THE HOLES, AND SECURING THE ANCHORS WITH A PRE-APPROVED CHEMICAL ANCHOR MATERIAL WHICH CONFORMS TO M.03.01-15 OF THE STANDARD SPECIFICATIONS. HOLE DIAMETER SHALL BE DETERMINED BY THE MANUFACTURER OF THE CHEMICAL ANCHORING MATERIAL.
    - THROUGH-BOLTING: THIS CONSISTS OF DRILLING THROUGH DECK SLABS AND SECURING REMOVABLE ANCHORS ON THE UNDERSIDE WITH PLATE WASHERS AND NUTS. THROUGH-BOLTING IS NOT PERMITTED ON NEW CONSTRUCTION OR PRESTRESSED CONCRETE. MAXIMUM HOLE SIZE IN SLAB = 1 1/2".
  - NUMBER OF ANCHORS: ON THE TRAFFIC SIDE OF A TYPICAL BARRIER, ANCHORS SHALL BE INSTALLED IN ALL POCKETS. AT BARRIER UNITS WHICH STRADDLE BRIDGE EXPANSION JOINTS THE ANCHOR AND CONNECTION DETAILS SHALL CONFORM TO TABLE "A".



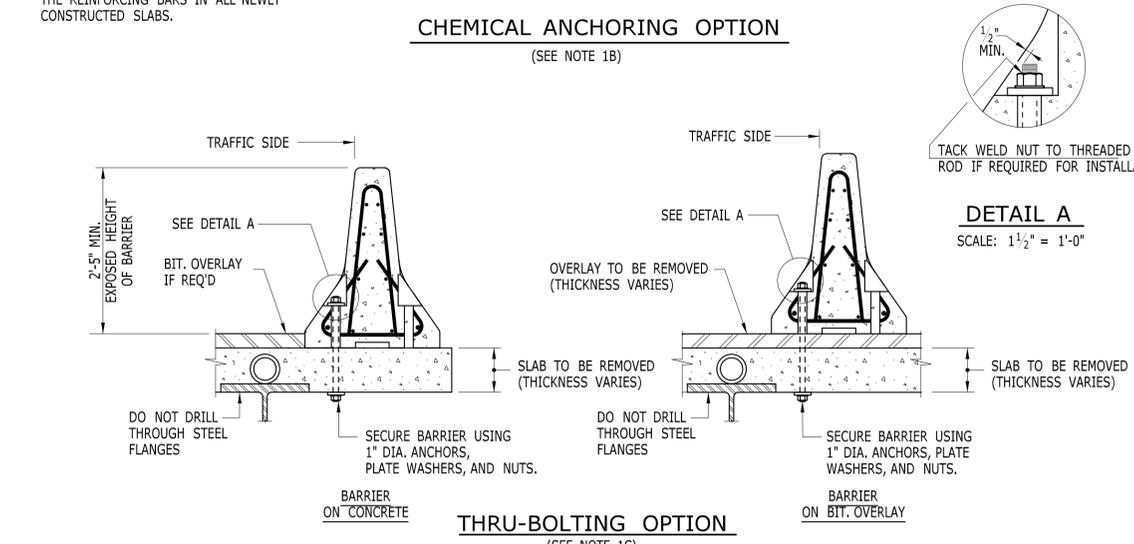
**CHEMICAL ANCHORING OPTION**  
(SEE NOTE 1B)



**BARRIER CONNECTION DETAILS**  
SCALE: 1" = 1'-0"



**LOOP BAR DETAILS**  
SCALE: 1" = 1'-0"



**THRU-BOLTING OPTION**  
(SEE NOTE 1C)

**TEMPORARY ANCHORAGE SYSTEM**  
SCALE: 3/4" = 1'-0"

**ENCAPSULATED LENS REFLECTIVE SHEETING TO CONFORM TO ARTICLE M18.09**

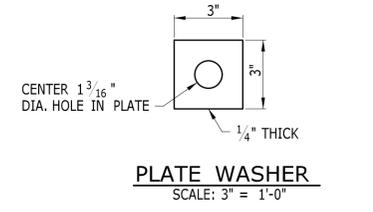
**COLOR APPLICATION:**  
LEFT SIDE OF ALL ROADWAYS AND RAMPS - YELLOW  
RIGHT SIDE OF ALL ROADWAYS AND RAMPS - SILVER

**COLOR OF DELINEATORS:**  
DE-7A ONE WAY YELLOW  
DE-7 ONE WAY SILVER  
DE-7B TWO WAY YELLOW  
DE-7C SILVER/YELLOW BACK TO BACK

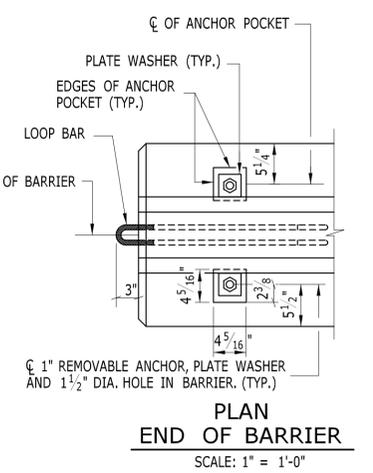
DELINEATORS SHALL BE MOUNTED IN THE CENTER OF TEMPORARY BARRIERS AS REQUIRED.

**SPACING OF DELINEATORS**  
ON LEADING TAPERED SECTIONS - EVERY UNIT (20'-0").  
ON THE FIRST 100'-0" OF PARALLEL SECTIONS - EVERY UNIT (20'-0") MINIMUM OF 2 IF LESS THAN 100'-0"  
ON THE REMAINING LENGTH - EVERY FIFTH (5TH) UNIT (100'-0").  
ALTERNATING ONE WAY TRAFFIC - EVERY UNIT (20'-0").  
ALL OTHER ROADWAYS SHALL BE DELINEATED IN ACCORDANCE WITH M.U.T.C.D.  
PAID FOR UNDER ITEM "DELINEATORS"

**DELINEATORS**  
SCALE: 1" = 1'-0"



**PLATE WASHER**  
SCALE: 3" = 1'-0"



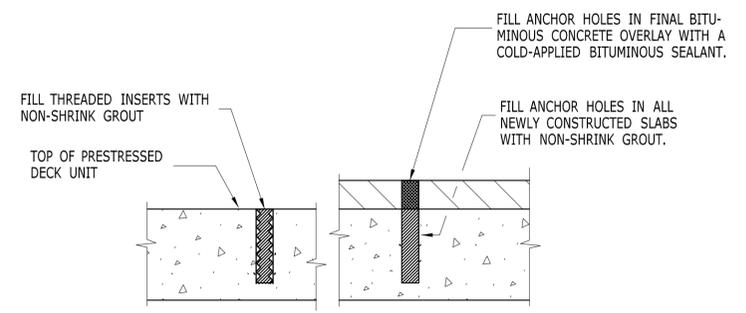
**PLAN END OF BARRIER**  
SCALE: 1" = 1'-0"

CASE	SPAN LENGTH CONTRIBUTING TO MOVEMENT AT THE EXPANSION JOINT.	METHOD OF END CONNECTION TO ABUTTING BARRIER UNIT. (WHERE MOVEMENT WILL OCCUR)	ANCHOR REQUIREMENTS FOR THE BARRIER UNIT WHICH STRADDLES THE BRIDGE JOINT
I.	UP TO 100 FEET	USE 7/8" CONNECTION ROD BUT DO NOT OVER TIGHTEN THE NUTS AND ALLOW "SLOP" AROUND THE ROD AND LOOPS.	ON ONE SIDE OF THE JOINT ONLY, INSTALL AS MANY ANCHORS AS POSSIBLE ON THE TRAFFIC SIDE OF THE BARRIER. ON THE OTHER SIDE OF THE JOINT DO NOT INSTALL ANCHORS.
II.	100 TO 420 FEET	FIELD DRILL HOLES IN ENDS OF BOTH UNITS AND CONNECT WITH 2-#25 BARS. FOR DETAILS SEE "BARRIER CONNECTION DETAILS".	ON ONE SIDE OF THE JOINT ONLY, INSTALL A TOTAL OF 10 ANCHORS. FILL THE POCKETS ON THE TRAFFIC SIDE BEFORE FILLING THE POCKETS ON THE DROP-OFF SIDE. IF THIS CANNOT BE ACHIEVED SEE III BELOW.
III.	OVER 420 FEET AND BARRIER LAYOUTS WHICH DO NOT SATISFY II.	TO BE DESIGNED BY CONTRACTOR AND REVIEWED BY ENGINEER. COST OF DESIGNING AND FURNISHING SPECIAL BARRIER UNITS OR ATTACHMENTS PAID FOR UNDER "TPCBC (STRUCTURE)".	TO BE DESIGNED BY CONTRACTOR AND REVIEWED BY ENGINEER. COST OF DESIGNING AND FURNISHING SPECIAL BARRIER UNITS OR ATTACHMENTS PAID FOR UNDER "TPCBC (STRUCTURE)".

3. THE WORK DONE ON THIS SHEET, WITH THE EXCEPTION OF THE DELINEATORS, SHALL BE PAID FOR UNDER THE ITEM "TEMPORARY PRECAST CONCRETE BARRIER CURB (STRUCTURE)"

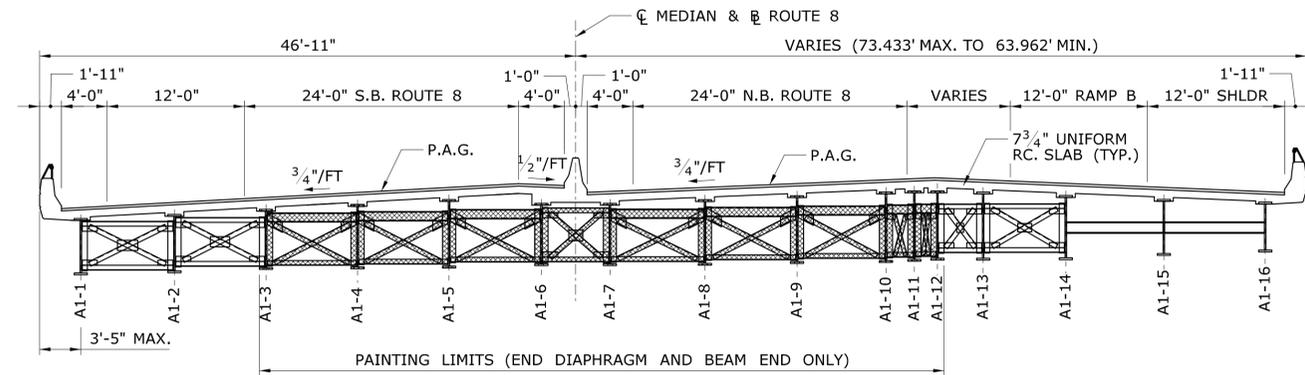
**NOTES FOR CONNECTION ROD DETAILS (SEE "ELEVATION-BARRIER CONNECTION DETAILS")**

- PLAIN STEEL WASHERS SHALL BE MANUFACTURED WITH THE FOLLOWING DIMENSIONS:  
OUTSIDE DIAMETER = 2 3/4" (+1/4", -0")  
INSIDE DIAMETER = 1 3/4" (+1/16")
- THE NUTS ON THE CONNECTION ROD SHALL BE TURNED UNTIL THE BOTTOM WASHER IS DRAWN UP AGAINST THE LOOP BAR. THE LOOP BARS SHALL NOT BE BENT DUE TO THE TIGHTENING PROCESS.
- FOR EASE OF REMOVAL OF THE NUTS, THE THREADS SHALL BE WAXED.



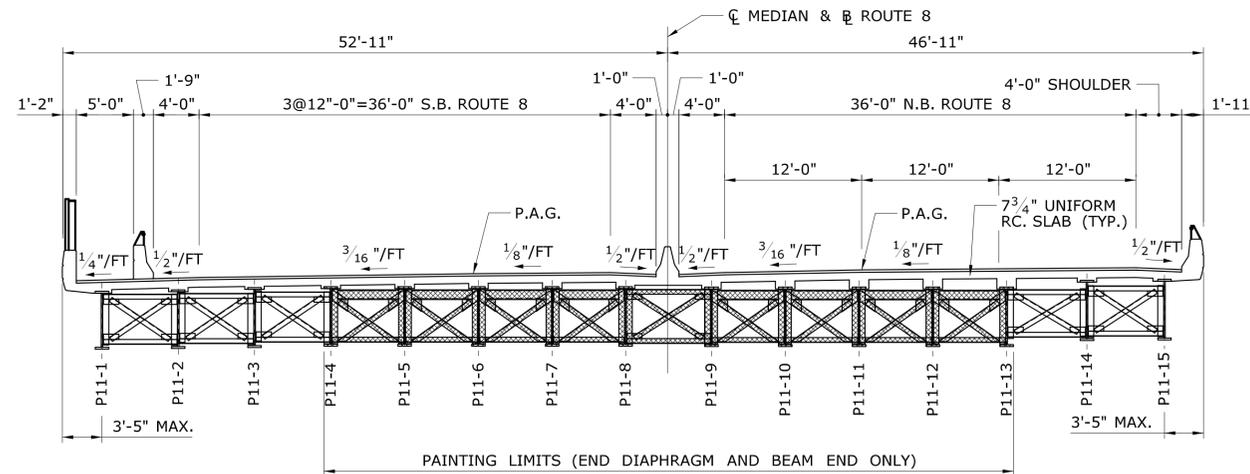
**FILLING OF ANCHOR HOLES**  
SCALE: 1 1/2" = 1'-0"

**90% PROGRESS SUBMISSION**



**TYPICAL BRIDGE SECTIONS SPANS 1 TO 5**

SCALE: 1/8"=1'-0"

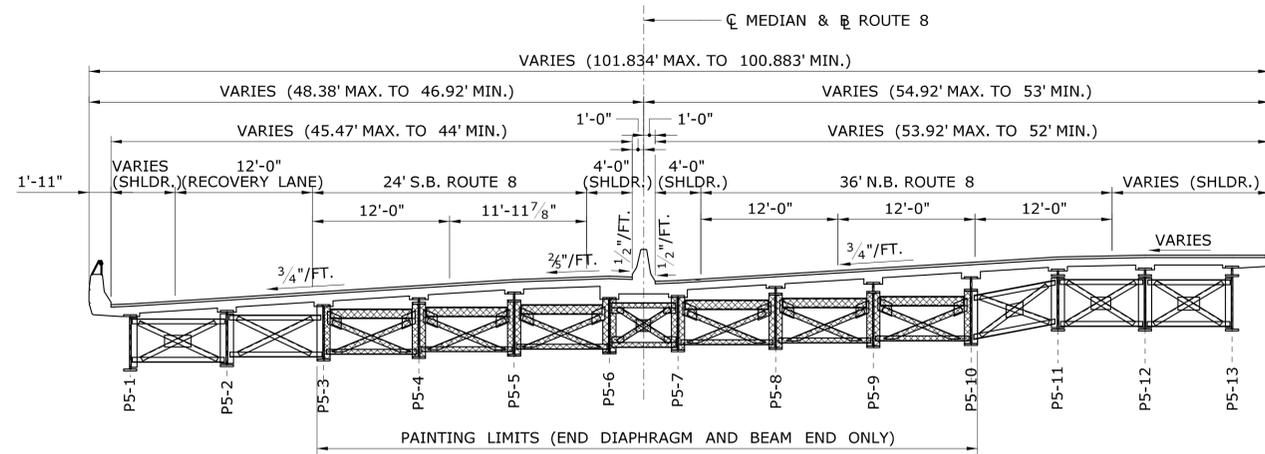


**TYPICAL BRIDGE SECTION SPAN 12**

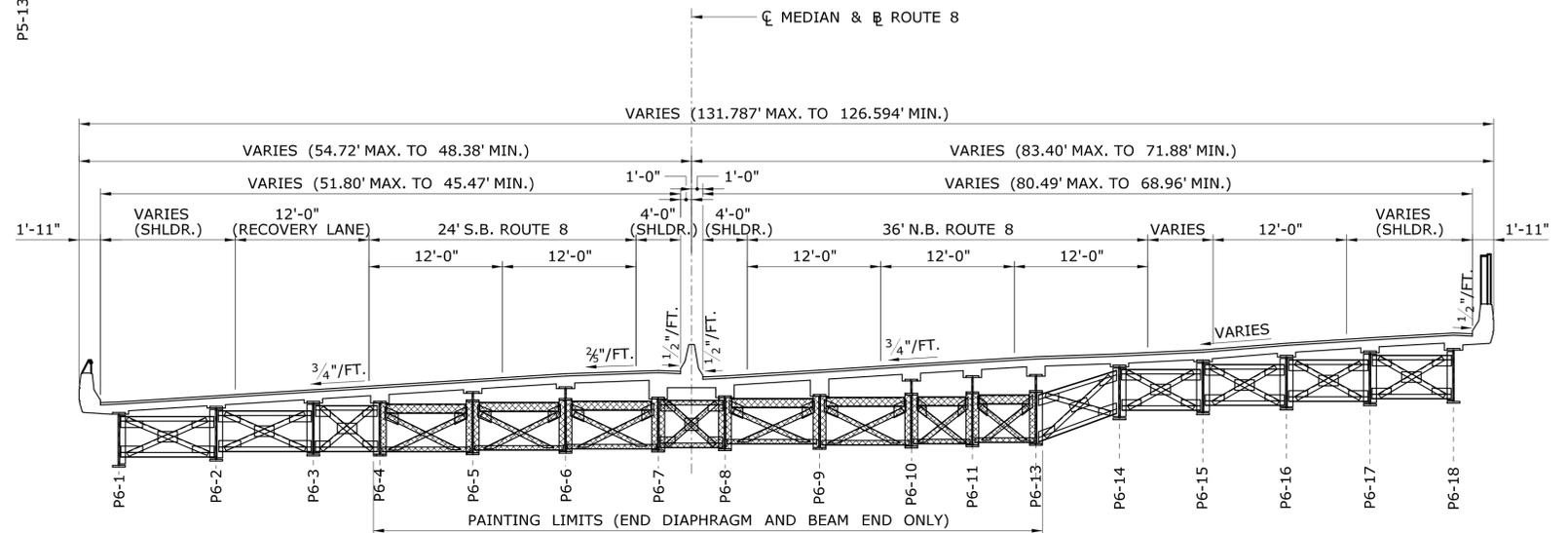
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**90% PROGRESS SUBMISSION**

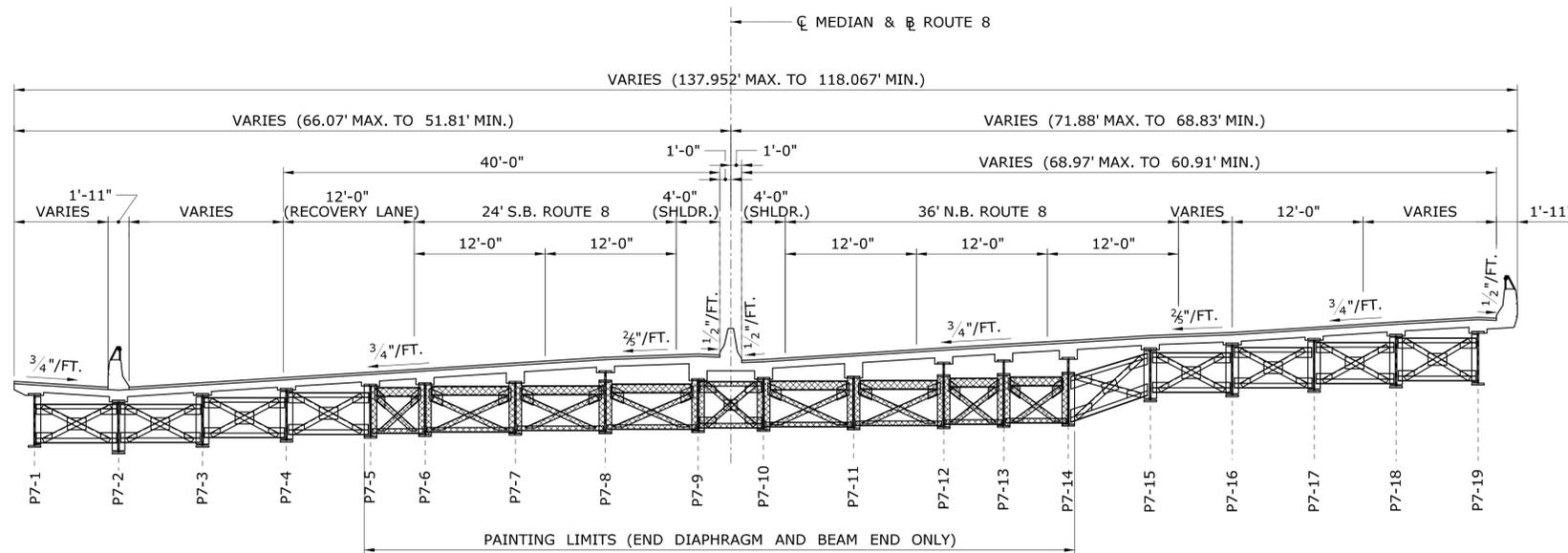
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...SB_MSH_Br00571A_126-170_XSC1.dgn	SIGNATURE/ BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE</b> <b>PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON</b> <b>DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-04</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>TYPICAL BRIDGE SECTIONS</b> <b>SPANS 1-5 &amp; 12</b>		



**TYPICAL BRIDGE SECTION SPAN 6**  
SCALE: 1/8"=1'-0"



**TYPICAL BRIDGE SECTION SPAN 7**  
SCALE: 1/8"=1'-0"



**TYPICAL BRIDGE SECTION SPAN 8**  
SCALE: 1/8"=1'-0"

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER:  
**KAB/ALH**  
CHECKED BY:  
**CC**  
SCALE AS NOTED



SIGNATURE/  
BLOCK:

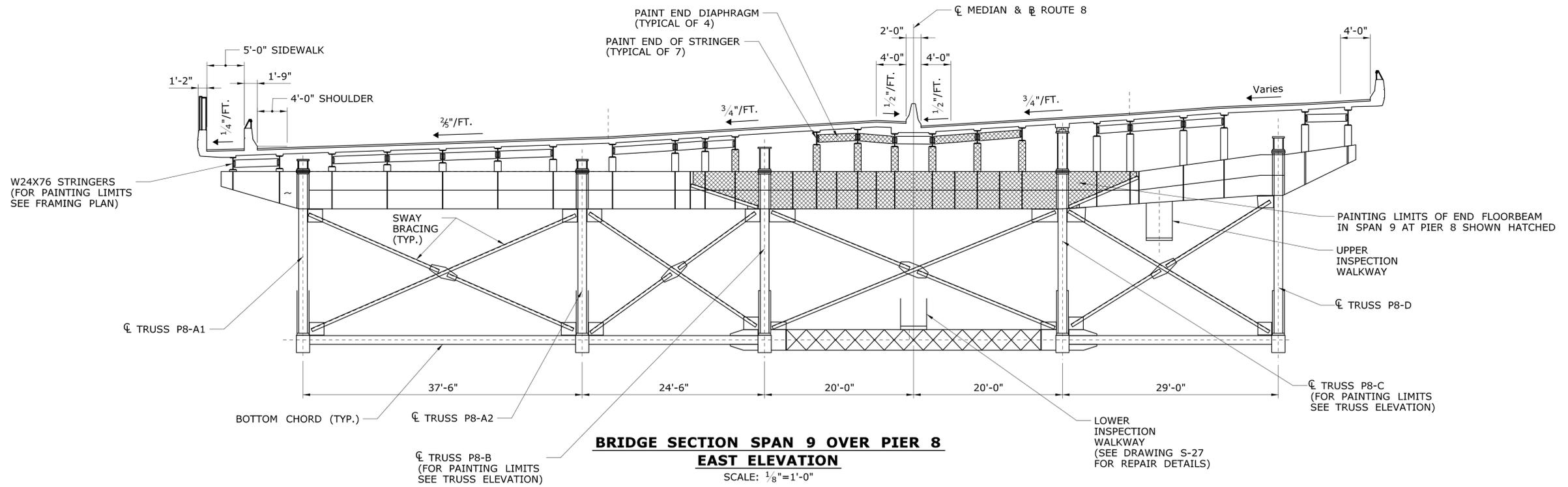
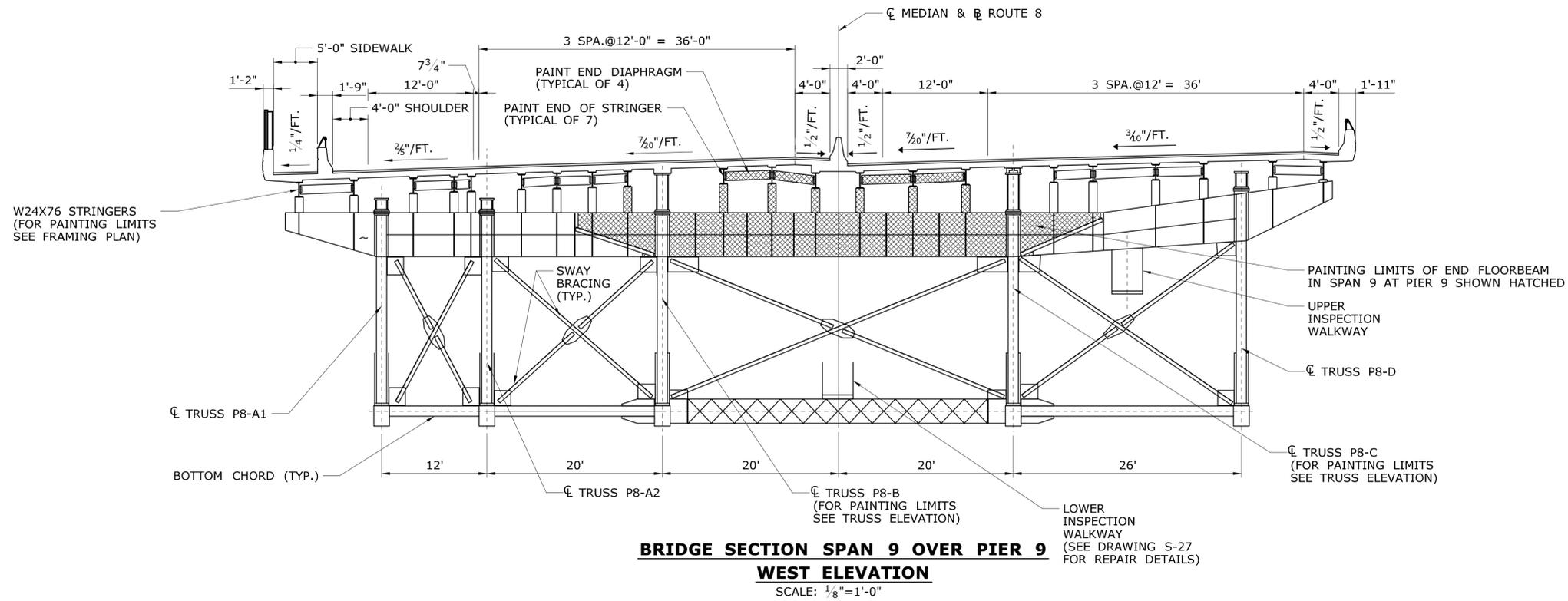
PROJECT TITLE:  
**COMMODORE HULL BRIDGE  
PAINTING AND STEEL REPAIRS**

TOWN:  
**SHELTON  
DERBY**  
DRAWING TITLE:  
**TYPICAL BRIDGE SECTIONS  
SPANS 6 THRU 8**

PROJECT NO.  
**126-170**  
DRAWING NO.  
**S-05**  
SHEET NO.

Plotted Date: 10/1/2015

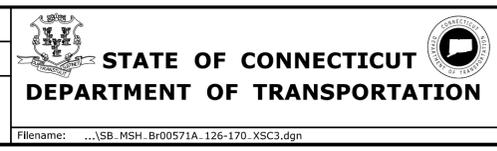
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**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015

DESIGNER/DRAFTER:  
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CHECKED BY:  
**CC**  
SCALE AS NOTED

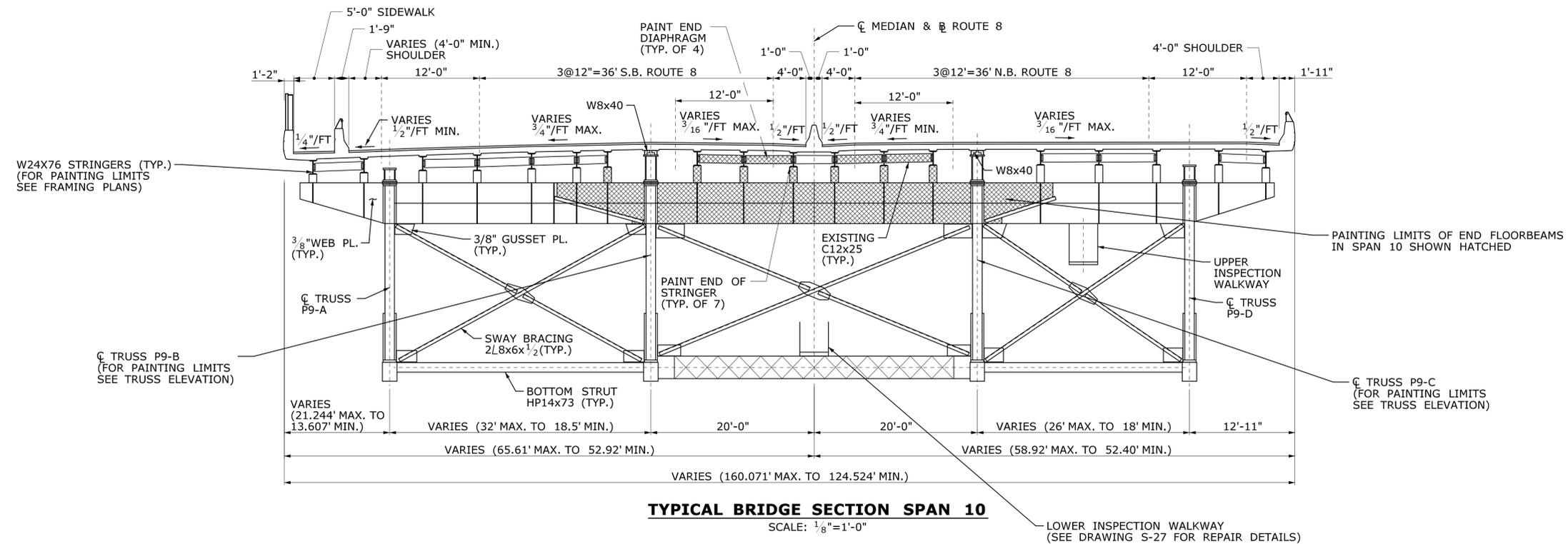


SIGNATURE/  
BLOCK:

PROJECT TITLE:  
**COMMODORE HULL BRIDGE  
PAINTING AND STEEL REPAIRS**

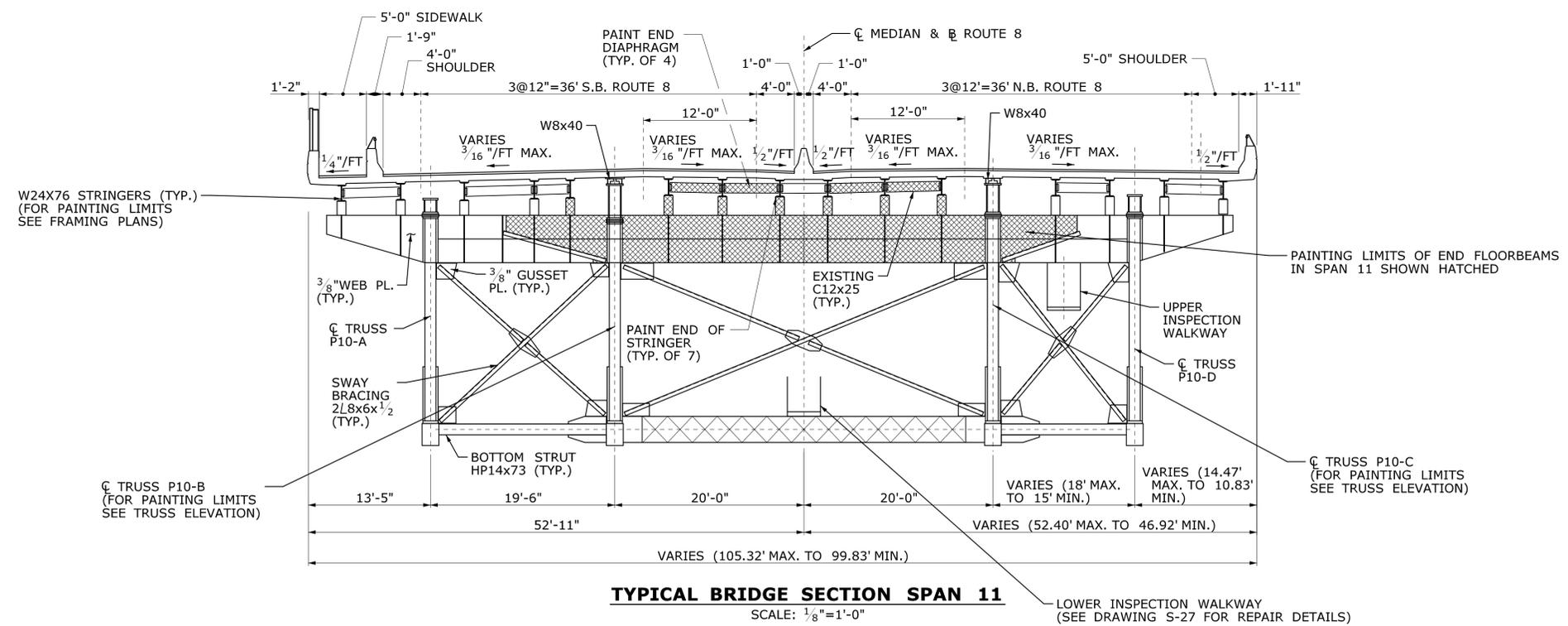
TOWN:  
**SHELTON  
DERBY**  
DRAWING TITLE:  
**TYPICAL BRIDGE SECTIONS  
SPAN 9**

PROJECT NO.  
**126-170**  
DRAWING NO.  
**S-06**  
SHEET NO.



**TYPICAL BRIDGE SECTION SPAN 10**

SCALE: 1/8"=1'-0"

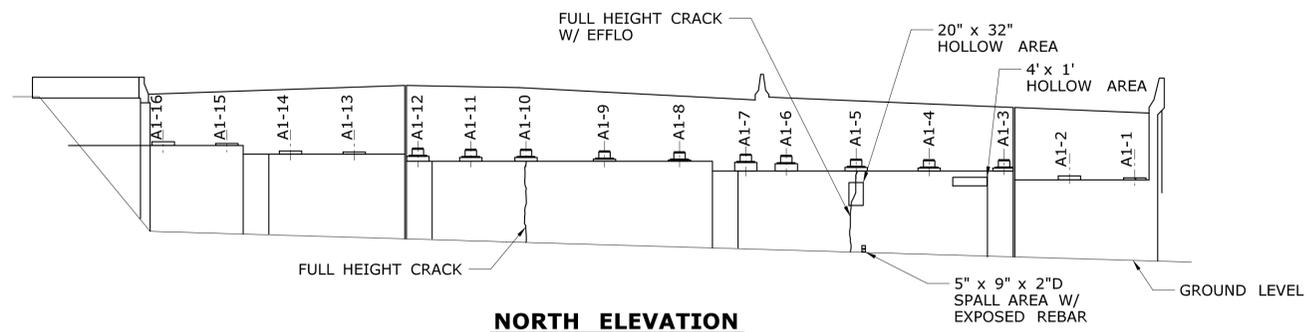


**TYPICAL BRIDGE SECTION SPAN 11**

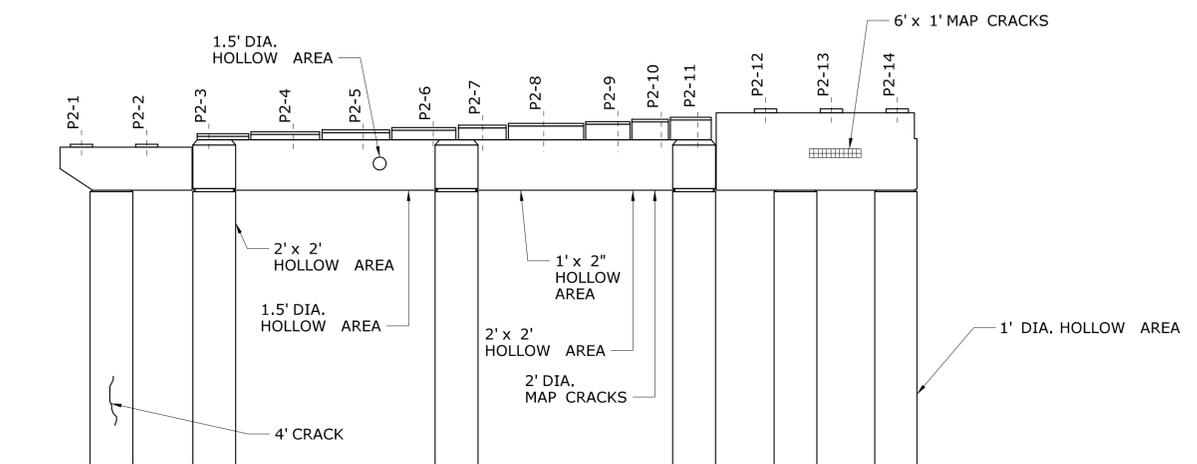
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**90% PROGRESS SUBMISSION**

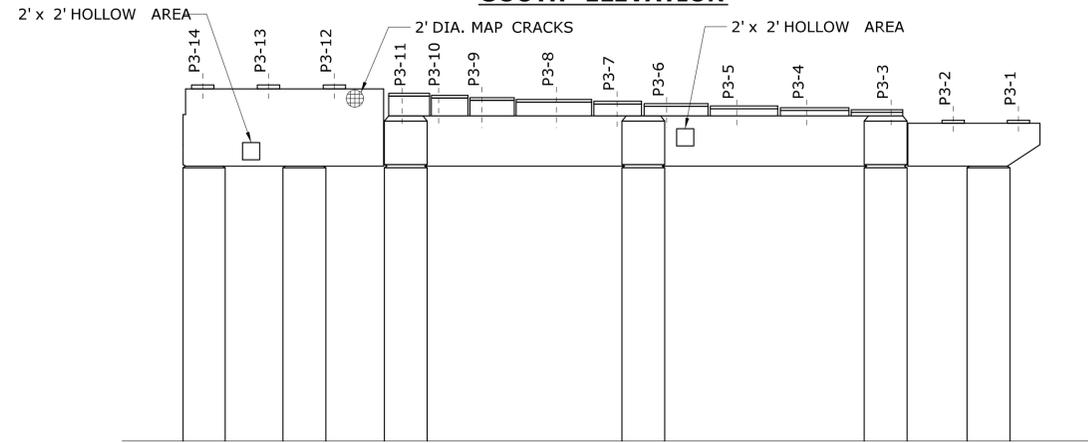
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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>TYPICAL BRIDGE SECTIONS</b> <b>SPANS 10 &amp; 11</b>		



**NORTH ELEVATION**  
**ABUTMENT NO. 1**  
 SCALE: 3/32"=1'-0"

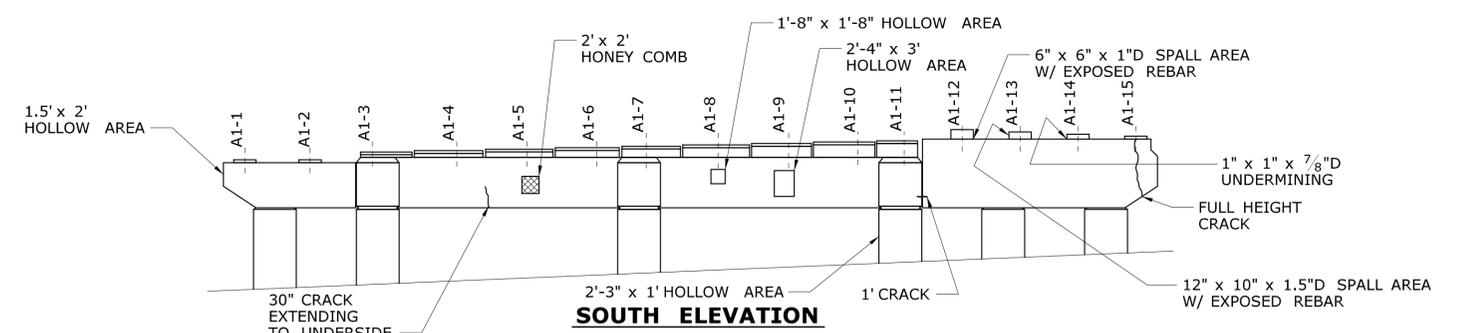


**SOUTH ELEVATION**

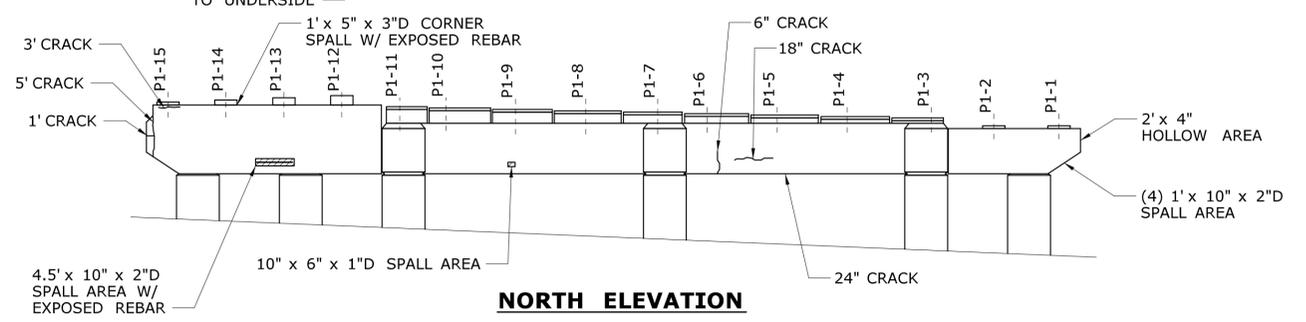


**NORTH ELEVATION**  
**PIER NO. 3**  
 SCALE: 3/32"=1'-0"

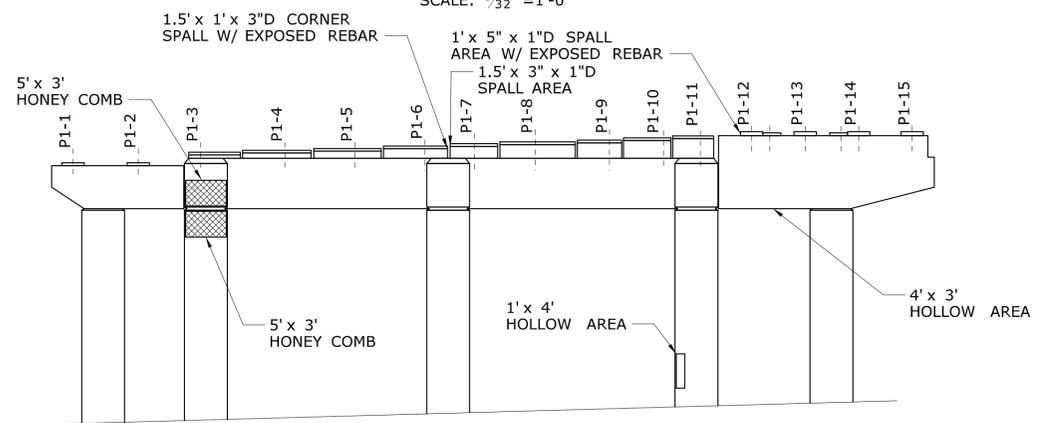
- LEGEND:**
- SPALL AREA W/ EXPOSED REBAR
  - SPALL AREA
  - HOLLOW AREA
  - MAP CRACKS
  - HONEY COMB



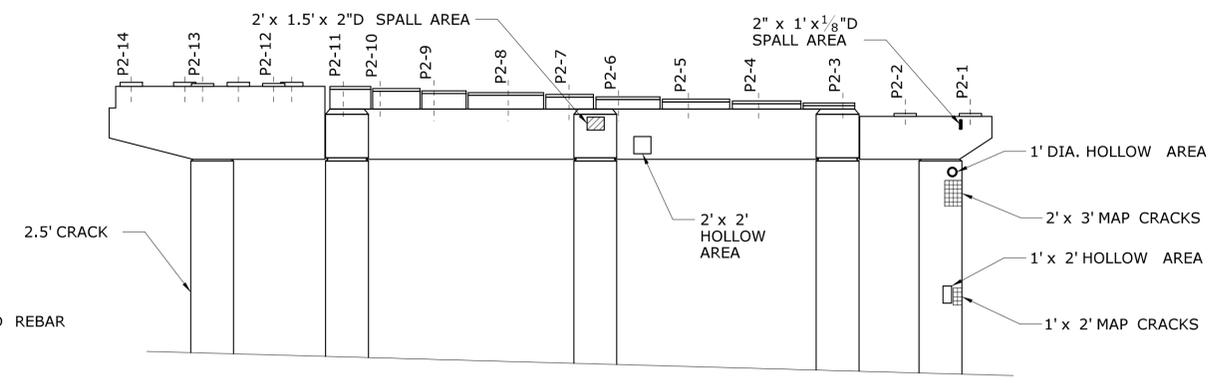
**SOUTH ELEVATION**



**NORTH ELEVATION**  
**PIER NO. 1**  
 SCALE: 3/32"=1'-0"



**SOUTH ELEVATION**



**NORTH ELEVATION**  
**PIER NO. 2**  
 SCALE: 3/32"=1'-0"

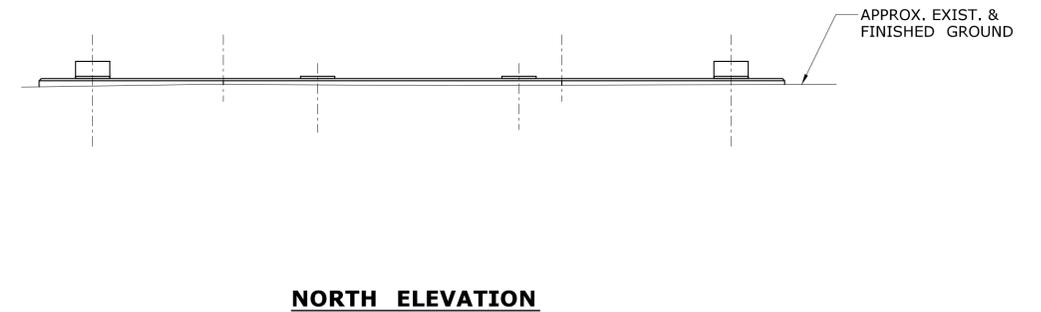
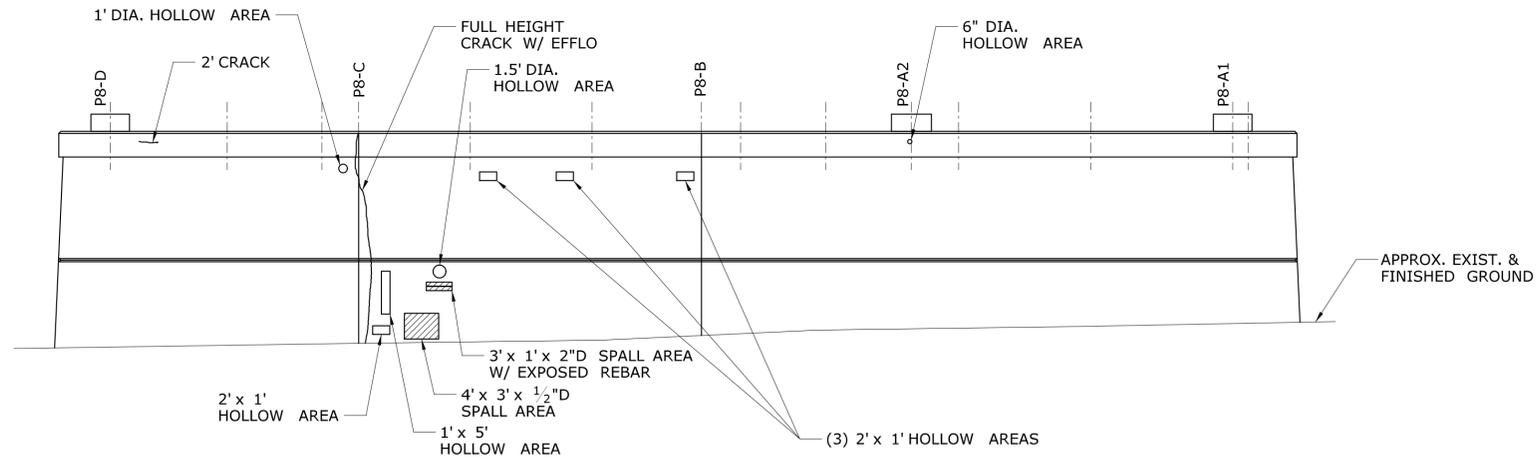
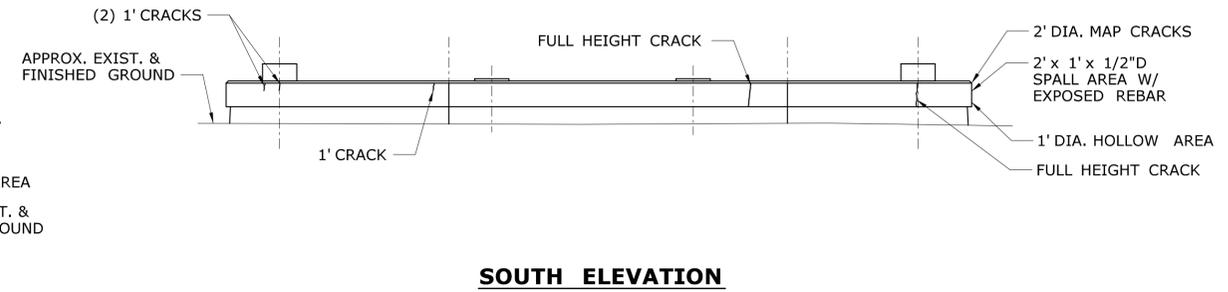
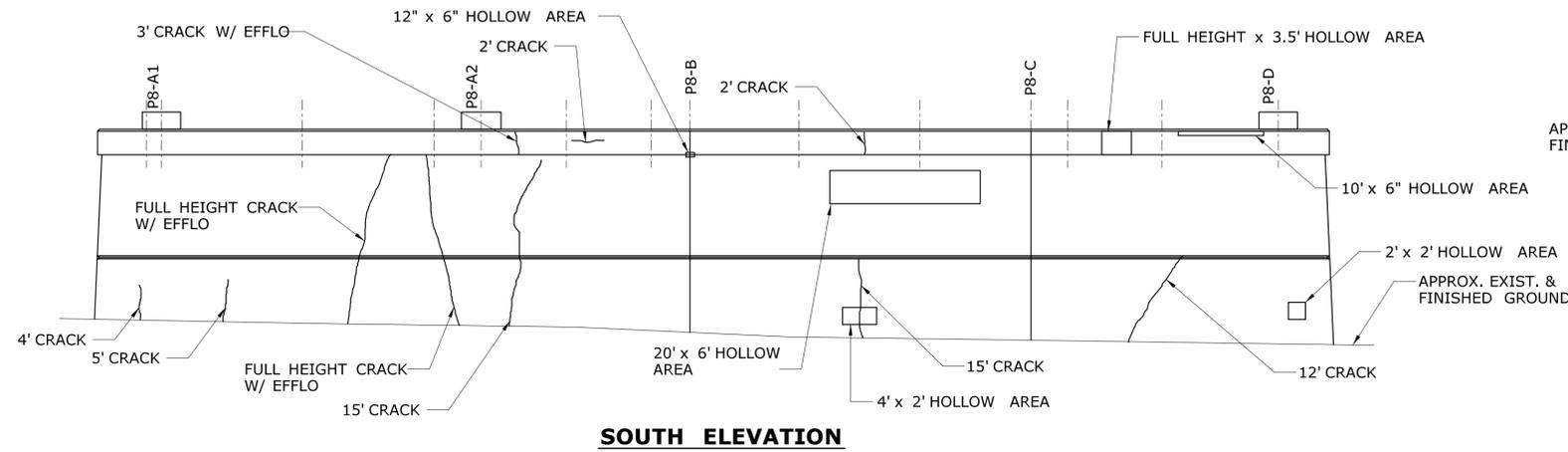
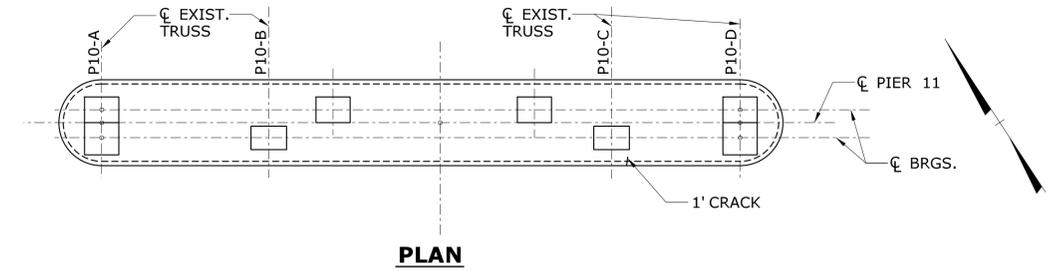
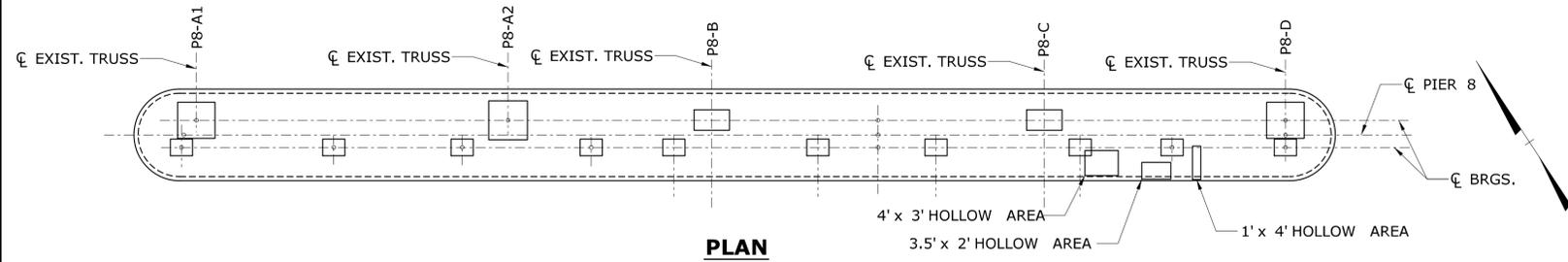
- REPAIR NOTES:**
- WORK TO BE PERFORMED SHALL INCLUDE PATCHING AND REPAIRING DETERIORATED CONCRETE.
  - FOR CONCRETE REPAIR DETAILS, SEE DWG. NO. S-13.

**90% PROGRESS SUBMISSION**

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>CAR/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...SB_MSH_Br00571A_126-170_ABT1.dgn	SIGNATURE/ BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE</b> <b>PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON</b> <b>DERBY</b> DRAWING TITLE: <b>SUBSTRUCTURE</b> <b>REPAIRS 1</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-08</b> SHEET NO.	
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015				







**PIER NO. 8**  
SCALE: 3/32"=1'-0"

**PIER NO. 11**  
SCALE: 3/32"=1'-0"

**LEGEND:**

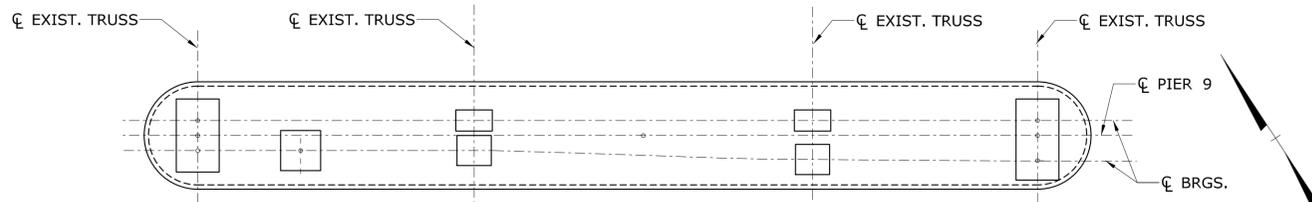
- SPALL AREA W/ EXPOSED REBAR
- SPALL AREA
- HOLLOW AREA
- MAP CRACKS
- HONEY COMB

**REPAIR NOTES:**

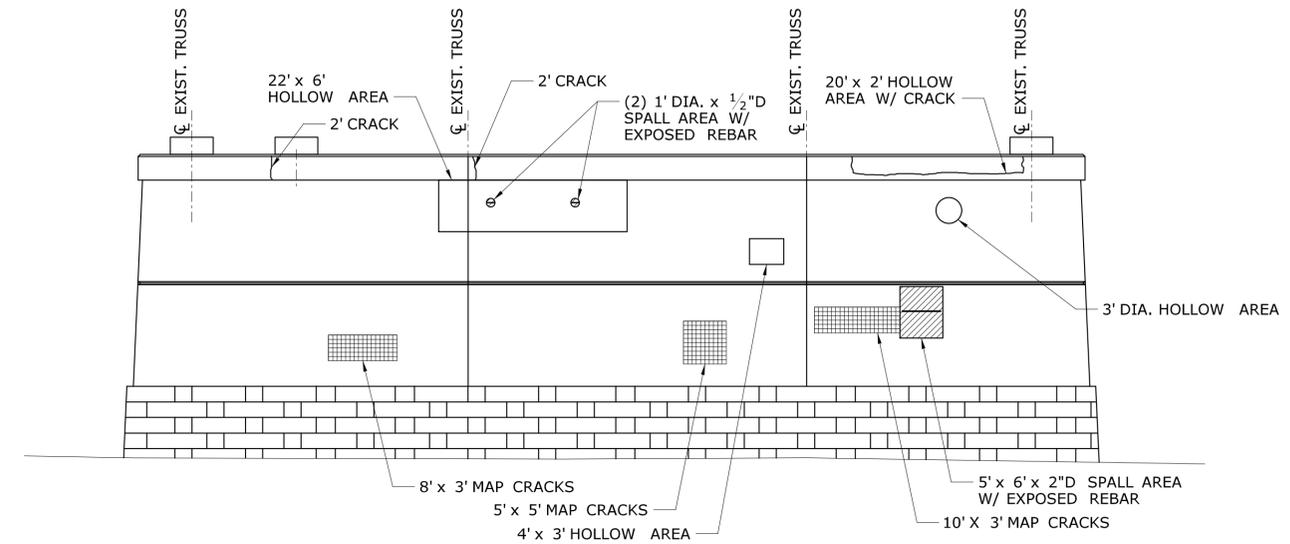
1. WORK TO BE PERFORMED SHALL INCLUDE PATCHING AND REPAIRING DETERIORATED CONCRETE.
2. FOR CONCRETE REPAIR DETAILS, SEE DWG. NO. S-13.

**90% PROGRESS SUBMISSION**

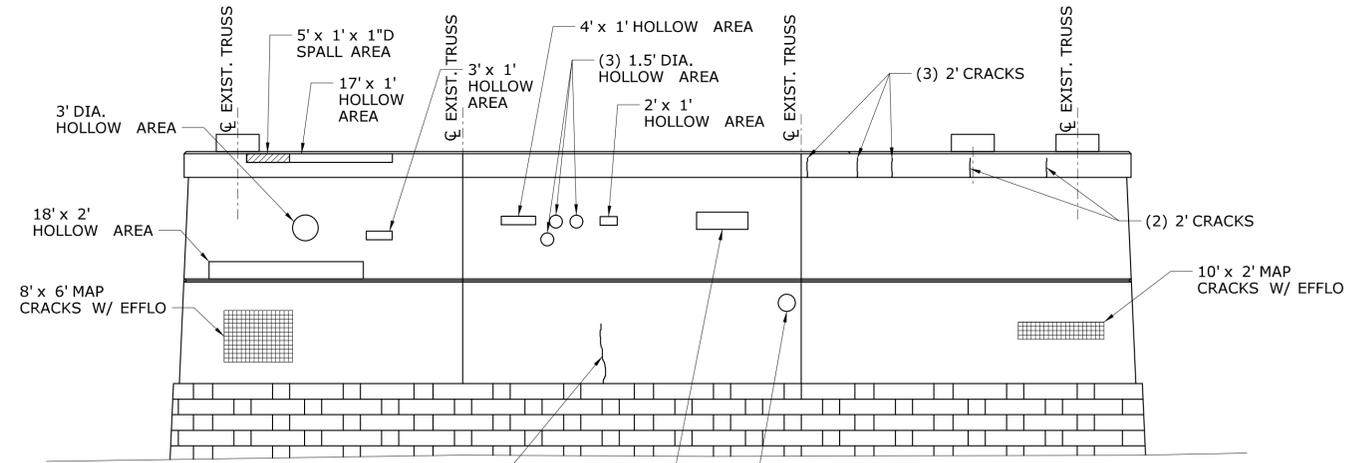
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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>SUBSTRUCTURE REPAIRS 4</b>		



**PLAN**



**SOUTH ELEVATION**

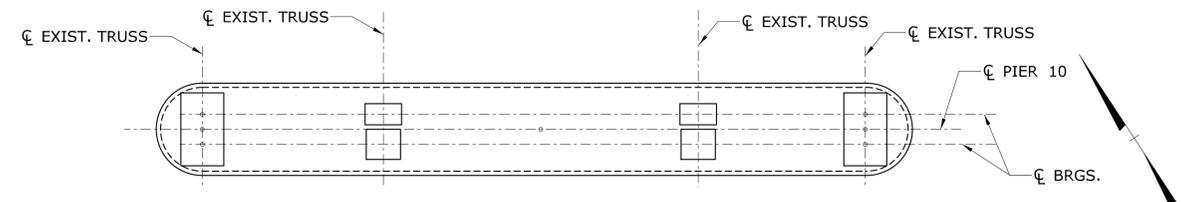


**NORTH ELEVATION**

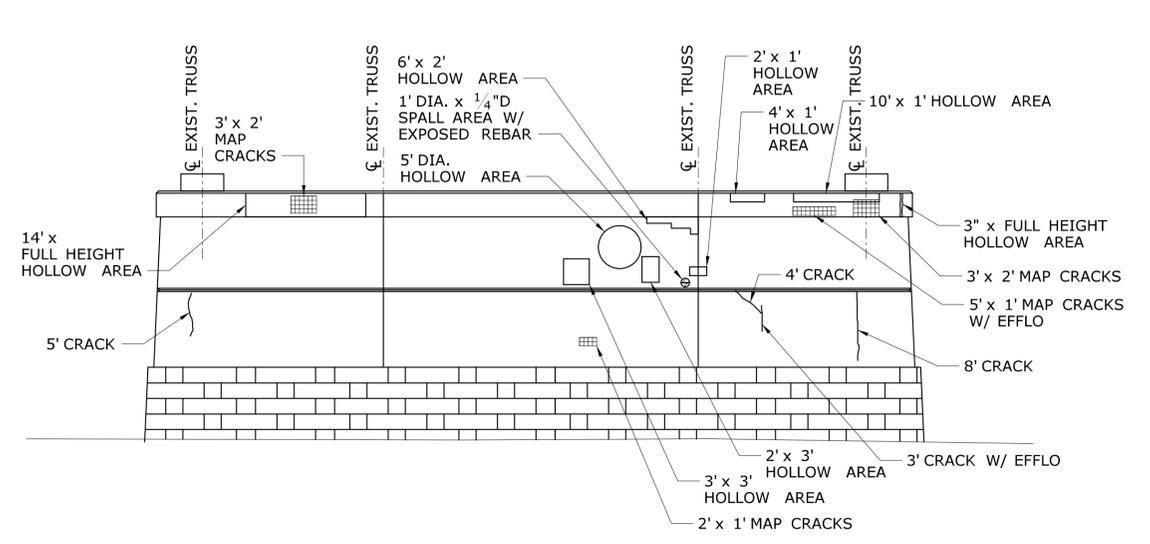
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**LEGEND:**

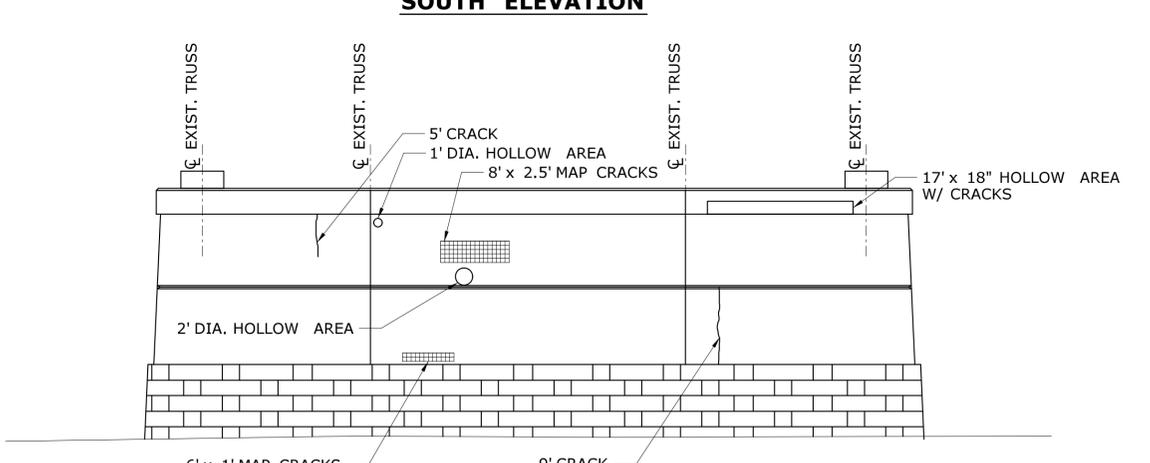
- SPALL AREA W/ EXPOSED REBAR
- SPALL AREA
- HOLLOW AREA
- MAP CRACKS
- HONEY COMB



**PLAN**



**SOUTH ELEVATION**



**NORTH ELEVATION**

**PIER NO. 10**

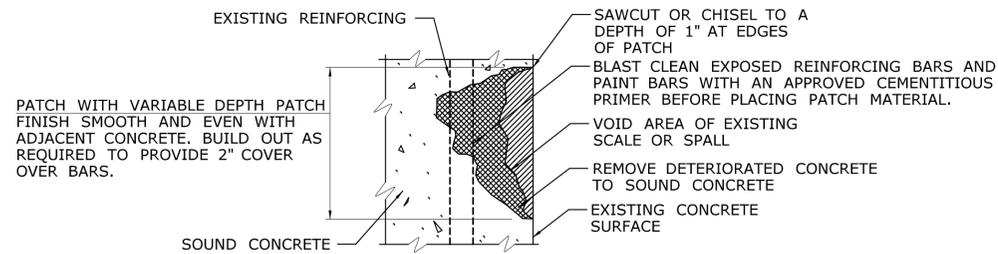
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**REPAIR NOTES:**

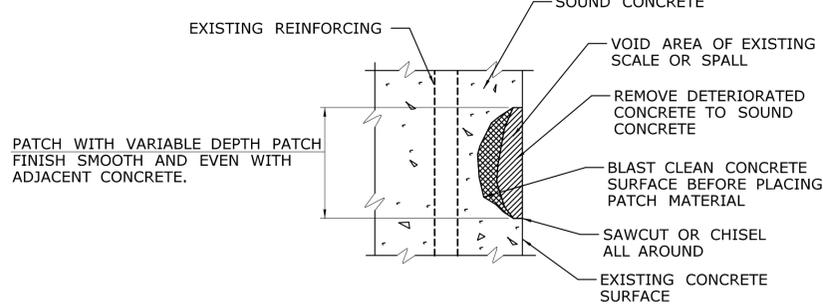
1. WORK TO BE PERFORMED SHALL INCLUDE PATCHING AND REPAIRING DETERIORATED CONCRETE.
2. FOR CONCRETE REPAIR DETAILS, SEE DWG. NO. S-13.

**90% PROGRESS SUBMISSION**

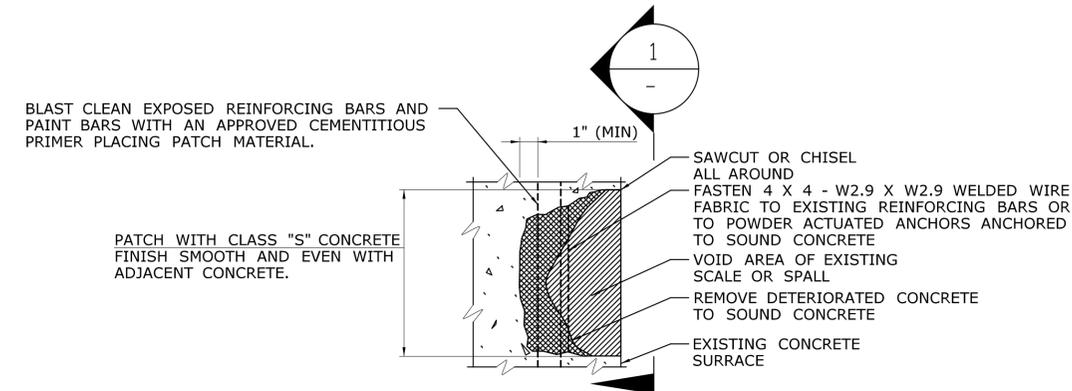
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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>COMMODORE HULL BRIDGE</b> <b>PAINTING AND STEEL REPAIRS</b> <b>PIER NO. 10</b> <b>SCALE: 3/32"=1'-0"</b> <b>REPAIR NOTES:</b> 1. WORK TO BE PERFORMED SHALL INCLUDE PATCHING AND REPAIRING DETERIORATED CONCRETE. 2. FOR CONCRETE REPAIR DETAILS, SEE DWG. NO. S-13.			<b>90% PROGRESS SUBMISSION</b> <b>SUBSTRUCTURE</b> <b>REPAIRS 5</b>



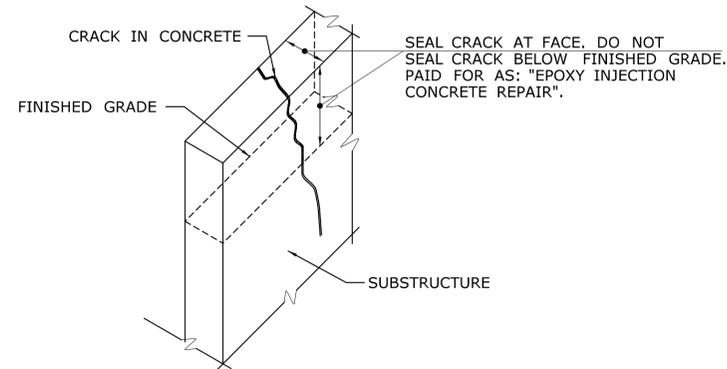
**VARIABLE DEPTH PATCH REPAIR  
(REINFORCING PARTIALLY EXPOSED)**  
NOT TO SCALE



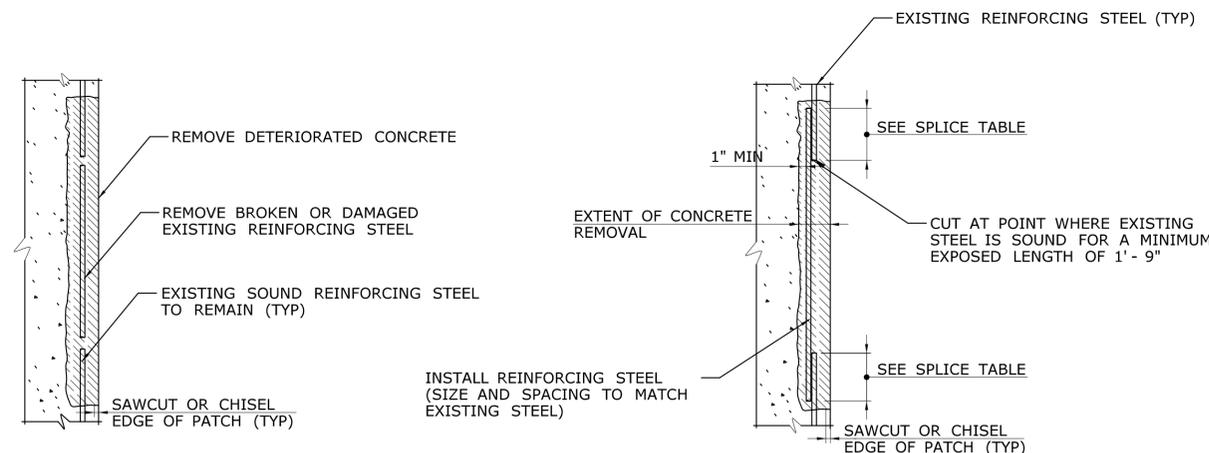
**VARIABLE DEPTH PATCH REPAIR  
(NO REINFORCING EXPOSED)**  
NOT TO SCALE



**CLASS "S" CONCRETE REPAIR**  
NOT TO SCALE



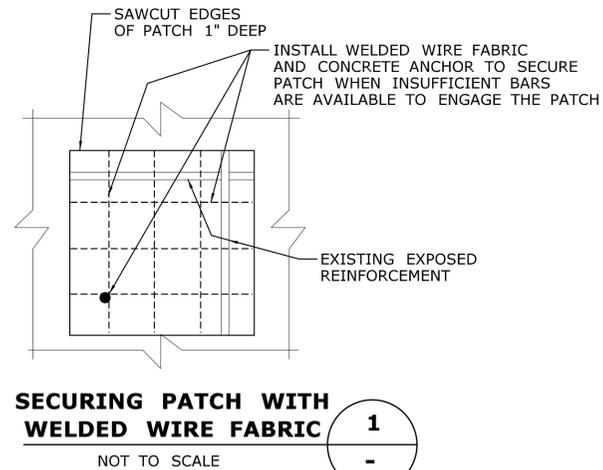
**TYPICAL LIMITS OF EPOXY INJECTION  
CONCRETE REPAIR**  
NOT TO SCALE



**TYPICAL SECTION  
DAMAGED REINFORCING**

**TYPICAL SPLICE DETAIL**

**REINFORCING REPAIR DETAIL**  
NOT TO SCALE



**SECURING PATCH WITH  
WELDED WIRE FABRIC**

TABLE OF SPLICE LENGTHS		
	BAR SIZE	MIN. SPLICE LENGTH
MAIN REINF. @ ≥ 6 O.C. **	#6	1' - 9"
	#7	2' - 4"
	#8	3' - 0"
MAIN REINF. @ ≥ 6 O.C. **	#4	1' - 4"
	#5	1' - 4"

\*\* FOR BARS SPACED < 6" O.C. MULTIPLY TABLE VALUES BY 1.25

THE ENGINEER WILL DETERMINE THE LOCATIONS OF UNSOUND OR DETERIORATED CONCRETE TO BE REPAIRED. THE METHOD USED TO DELINEATE AREAS OF CONCRETE TO BE REPAIRED SHALL NOT PERMANENTLY MARK THE CONCRETE, LEAVE ANY RESIDUE AFTER REMOVAL, OR REQUIRE HARSH CHEMICALS TO REMOVE.

THE CONTRACTOR SHALL REMOVE THE DETERIORATED CONCRETE IN ACCORDANCE WITH THE GUIDELINES SET IN THESE NOTES AND IN THE SPECIAL PROVISIONS AND THE ENGINEER WILL DETERMINE THE TYPE OF REPAIR REQUIRED FOR EACH AREA USING THE BELOW GUIDELINE FOR REPAIR TYPES.

CONCRETE REPAIR MAY BE ONE OF THE FOLLOWING TYPES:

**CLASS "S" CONCRETE**

THIS TYPE OF REPAIR SHALL BE USED WHERE THE REINFORCING BARS ARE SUFFICIENTLY EXPOSED TO ENGAGE AND ANCHOR THE PATCHING MATERIAL. CLASS "S" CONCRETE MAY BE USED FOR SMALLER AREAS LESS THAN FOUR SQUARE FEET WHERE THERE IS A SUFFICIENT TOTAL AREA TO JUSTIFY THE USE OF THIS MATERIAL AND WHERE THE PATCH CAN BE SECURELY ANCHORED BY THE REINFORCING.

OVERHEAD PATCHES SHALL BE CLASS "S" CONCRETE UNLESS OTHERWISE DIRECTED BY THE ENGINEER AND SHALL BE ANCHORED SECURELY TO THE REINFORCING BARS.

AT VERTICAL AND FLAT FACES, ONLY THE OUTER BARS OF THE OUTSIDE MAT OF REINFORCING NEED TO BE ENGAGED BY THE PATCH UNLESS THE DETERIORATION EXTENDS BEHIND THE NEXT LAYER.

BEFORE APPLYING THE PATCHING MATERIAL, THE REINFORCING BARS SHALL BE BLAST CLEANED AND PAINTED WITH AN APPROVED CEMENTITIOUS PRIMER.

**VARIABLE DEPTH PATCH**

THIS TYPE OF REPAIR SHALL BE USED TO REPAIR FLAT AND VERTICAL SURFACES WHERE THE REINFORCING BARS ARE NOT EXPOSED OR ARE ONLY PARTIALLY EXPOSED. IT SHALL GENERALLY BE USED FOR SMALLER REPAIRS WHERE CLASS "S" CONCRETE IS NOT APPROPRIATE.

THE ENGINEER MAY DIRECT THE CONTRACTOR TO REPAIR SMALL AREAS WITH REINFORCING FULLY EXPOSED AS CLASS "S" CONCRETE WHEN A SUFFICIENT VOLUME OF REPAIRS ARE IDENTIFIED.

SEE SPECIAL PROVISIONS FOR MORE INFORMATION REGARDING APPLICATION OF EACH REPAIR MATERIAL.

**REINFORCING BAR REPAIRS**

REINFORCING WHICH IS DETERMINED BY THE ENGINEER TO BE IN NEED OF REPLACEMENT, SHALL BE REMOVED TO A POINT WHERE IT IS SOUND. FOR SUBSTRUCTURES, THE PATCH SHALL EXTEND A SUFFICIENT DISTANCE BEYOND THIS POINT TO DEVELOP A SPLICE LENGTH SPECIFIED IN THE TABLE BELOW. THE ENGINEER WILL EVALUATE REINFORCING BAR REPAIR NEED FOR PIERS AFTER THE BARS HAVE BEEN EXPOSED.

REINFORCING BARS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "DEFORMED STEEL BARS".

**SEALING CRACKS IN CONCRETE**

IN GENERAL, CRACKS SHALL BE SEALED ONLY WHERE THERE IS NO EARTH FILL BEHIND THE CONCRETE FEATURE TO AVOID THE POSSIBILITY OF GROUND WATER PRESSURE BEING EXERTED ON THE SEALANT.

CRACK SEALING WILL BE PAID FOR UNDER "EPOXY INJECTION CONCRETE REPAIR"

**RESTRICTIONS**

THE REMOVAL OF DETERIORATED CONCRETE SHALL PROCEED AS DIRECTED BY THE ENGINEER. IF THE REMOVAL OF DETERIORATED CONCRETE BECOMES EXCESSIVE, THE REMOVAL WORK SHALL BE STOPPED AT THE LOCATION AND THE ENGINEER NOTIFIED IMMEDIATELY.

MAXIMUM HAMMER SIZE USED TO REMOVE DETERIORATED CONCRETE SHALL BE 15 POUNDS.

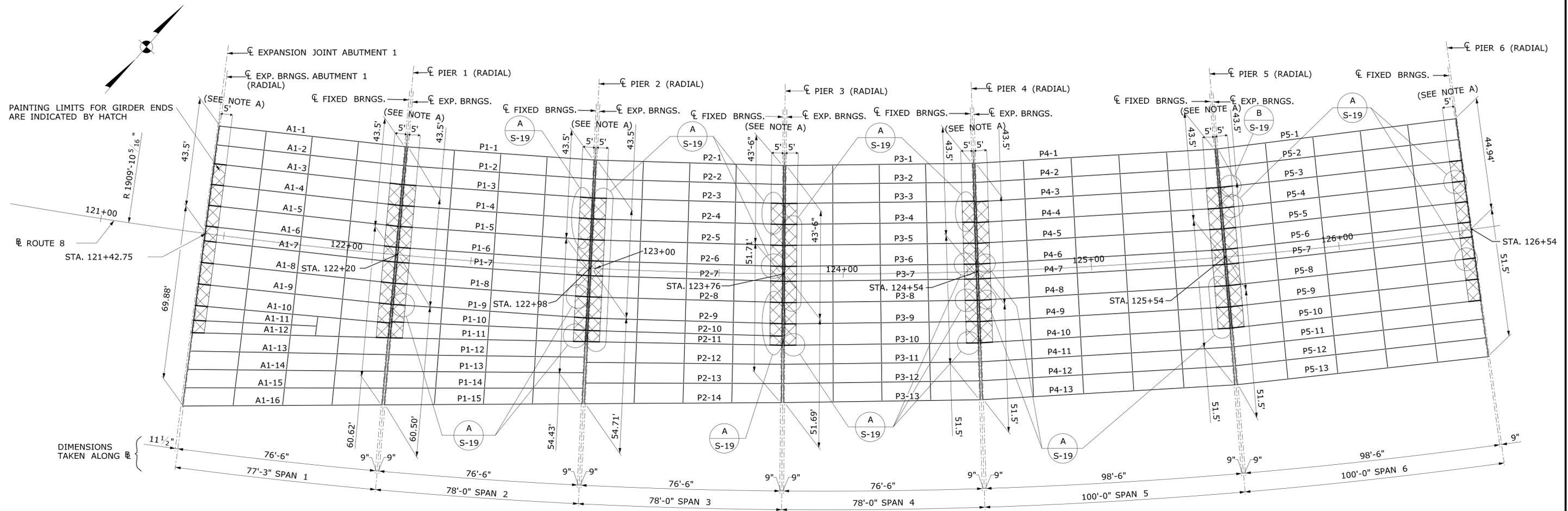
IF REMOVAL EXTENDS MORE THAN 1 1/2" BEHIND THE MAIN REINFORCING BARS, THE REMOVAL WORK SHALL BE STOPPED AND THE ENGINEER NOTIFIED IMMEDIATELY.

**NOTES**

1. IN AREAS WHERE THE REINFORCING STEEL, ON REMOVAL OF DETERIORATED CONCRETE, HAS LESS THAN HALF OF ITS SURFACE EXPOSED, THE REINFORCEMENT SHALL BE COATED WITH A CEMENTITIOUS PRIMER PRIOR TO PLACING THE PATCHING MATERIAL. THE COST OF THIS COATING IS INCLUDED IN THE APPLICABLE REPAIR PAY ITEM.
2. IF AFTER CONCRETE REMOVAL THE REINFORCING STEEL HAS AT LEAST ONE HALF OF ITS SURFACE AREA EXPOSED, THE CONCRETE SHALL BE FURTHER REMOVED TO A DEPTH OF 1" AROUND THE STEEL.
3. THE 1" DEEP SAW CUT SHALL BE INCLUDED IN THE CONTRACT BID PRICE OF THE APPLICABLE REPAIR PAY ITEM.
4. IF THE AREA OR DEPTH OF REMOVAL IS DEEMED TO BE EXCESSIVE, THE ENGINEER SHALL BE IMMEDIATELY NOTIFIED BEFORE CONTINUING REMOVAL WORK.
5. EXPOSED REINFORCEMENT TO BE THOROUGHLY CLEANED BEFORE PATCHING.

**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>CONCRETE REPAIR DETAILS</b>		



**FRAMING PLAN - SPANS 1 TO 6**  
SCALE: 1" = 20'-0"

**STRUCTURAL STEEL PAINTING NOTES**

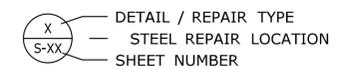
- SPAN 1:  
GIRDERS A1-3 TO A1-10, AND A1-12 ARE ORIGINAL TYPE "A" PLATE GIRDERS.  
GIRDERS A1-1, A1-2, A1-11 AND A1-13 TO A1-16 ARE NEWER PLATE GIRDERS.  
  
SPAN 2:  
GIRDERS P1-3 TO P1-11 ARE ORIGINAL TYPE "A" PLATE GIRDERS.  
GIRDERS P1-1, P1-2 AND P1-12 TO P1-15 ARE NEWER PLATE GIRDERS.  
  
SPAN 3:  
GIRDERS P2-3 TO P2-11 ARE ORIGINAL TYPE "A" PLATE GIRDERS.  
GIRDERS P2-1, P2-2 AND P2-12 TO P2-14 ARE NEWER PLATE GIRDERS.  
  
SPAN 4:  
GIRDERS P3-3 TO P3-10 ARE ORIGINAL TYPE "A" PLATE GIRDERS.  
GIRDERS P3-1, P3-2 AND P3-11 TO P3-13 ARE NEWER PLATE GIRDERS.  
  
SPAN 5:  
GIRDERS P4-3 TO P4-10 ARE ORIGINAL TYPE "B" PLATE GIRDERS.  
GIRDERS P4-1, P4-2 AND P4-11 TO P4-13 ARE NEWER PLATE GIRDERS.  
  
SPAN 6:  
GIRDERS P5-3 TO P5-10 ARE ORIGINAL TYPE "B" PLATE GIRDERS.  
GIRDERS P5-1, P5-2 AND P5-11 TO P5-13 ARE NEWER PLATE GIRDERS.

ORIGINAL PLATE GIRDER TYPE "A"  
WEB PLATE = 58" x 7/16"  
TOP FLANGE = 16" x 3/4"  
BOTTOM FLANGE VARIES FROM 16" x 3/4" TO 16" x 1 1/4" AT MIDSPAN

ORIGINAL PLATE GIRDER TYPE "B"  
WEB PLATE = 65" x 7/16"  
TOP FLANGE VARIES FROM 16" x 3/4" TO 16" x 1" AT MIDSPAN  
BOTTOM FLANGE VARIES FROM 16" x 3/4" TO 16" x 1 3/4" AT MIDSPAN

- GIRDERS A1-5, A1-9, A1-10, A1-12, P1-5, P1-8 TO P1-11, P2-5, P2-8, TO P2-11 P3-5, P3-8 TO P3-10, P4-5, P4-8 TO P4-10, P5-4, P5-5, P5-8 TO P5-10 HAVE WT SECTION WELDED TO TOP OF TOP FLANGE.
- THE BEAM END PAINTING LIMITS SHOWN ARE FROM THE END OF THE GIRDER TO THE DISTANCE 5'-0" AND SHALL INCLUDE ANY EXPOSED SURFACES OF THE GIRDER, ANY EXPOSED SURFACE OF THE CROSS FRAMING, CONNECTION PLATES, STIFFENERS, LATERAL BRACING AND STEEL BEARING ELEMENTS WITHIN THESE LIMITS.
- ALL WORK WITHIN BEAM END SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "ABRASIVE BLAST CLEANING AND FIELD PAINTING OF STRUCTURE (SITE NO. 1)".
- THE CONTAINMENT SYSTEM WITHIN BEAM END SHALL BE DESIGNED BY THE CONTRACTOR AND REVIEWED BY THE ENGINEER IN ACCORDANCE WITH THE SPECIAL PROVISION "CLASS 1 - CONTAINMENT AND COLLECTION OF SURFACE PREPARATION DEBRIS (SITE NO. 1)".
- THE PAINTING OVER STEEL REPAIR AREA OTHER THAN BEAM END SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "FIELD TOUCHUP PAINTING".

**LEGEND:**

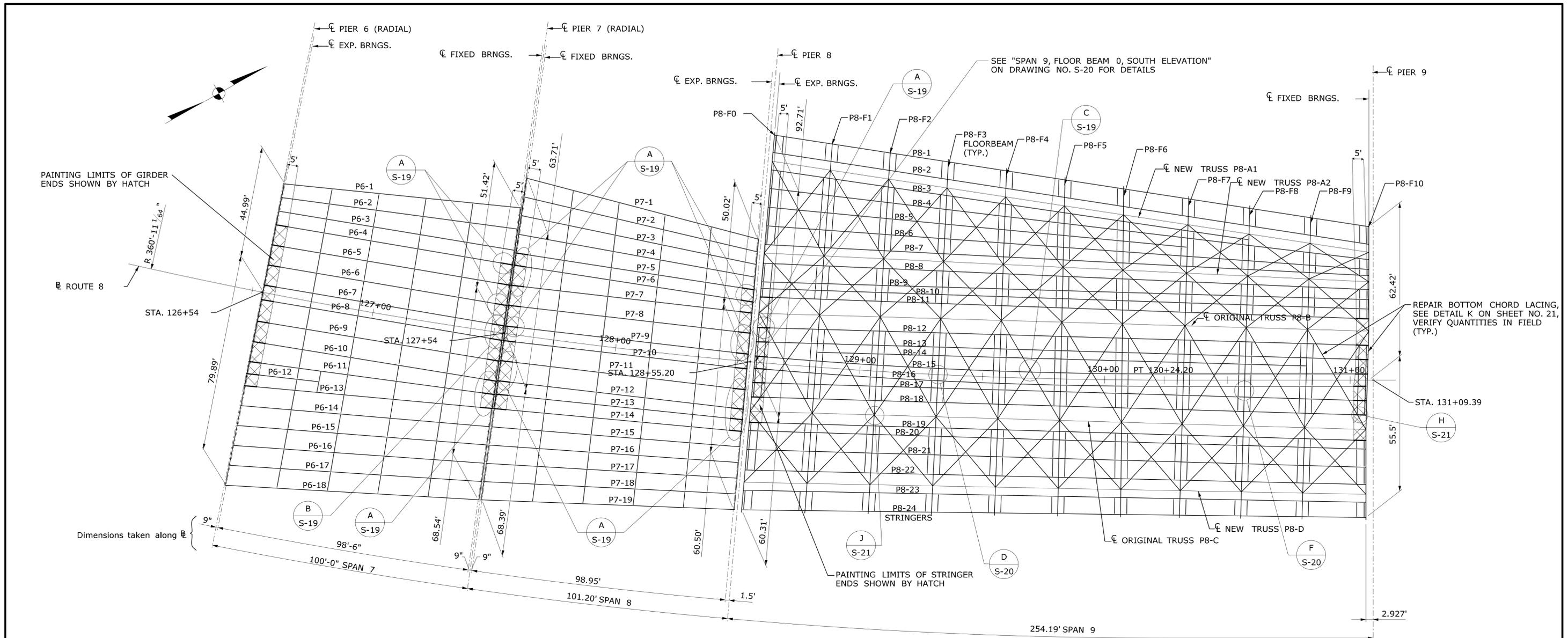


- TYPE A REPAIRS: GIRDER WEB REPAIR
- TYPE B REPAIRS: BEARING STIFFENER REPAIR
- TYPE C REPAIRS: CROSS BRACING BOTTOM CHORD REPAIR
- TYPE D REPAIRS: FLOORBEAM BOTTOM FLANGE REPAIR
- TYPE E REPAIRS: FLOORBEAM WEB REPAIR
- TYPE F REPAIRS: FLOORBEAM TOP FLANGE REPAIR
- TYPE G REPAIRS: STRINGER BOTTOM FLANGE REPAIR
- TYPE H REPAIRS: STRINGER WEB REPAIR
- TYPE I REPAIRS: FLOORBEAM STIFFENER REPAIR
- TYPE J REPAIRS: GUSSET PLATE REPAIR
- TYPE K REPAIRS: BOTTOM CHORD LACING REPAIR

FOR ADDITIONAL INFORMATION SEE PLAN SHEETS FOR CONNECTICUT DEPARTMENT OF TRANSPORTATION PROJECT NO. 36-37 (1948) "HOUSATONIC RIVER BRIDGE BETWEEN SHELTON AND DERBY", AND CTDOT PROJECT NO. 126-119 (1983) "COMMODORE HULL BRIDGE ROUTE 8 OVER THE HOUSATONIC RIVER". NOTE THAT STRUCTURAL STEEL IDENTIFICATION CALLOUTS SHOWN IN THE FRAMING PLANS ARE THOSE USED IN THE STEEL FRAMING PLANS OF PROJECT NO. 126-119.

**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>FRAMING PLAN 1 OF 3</b>		



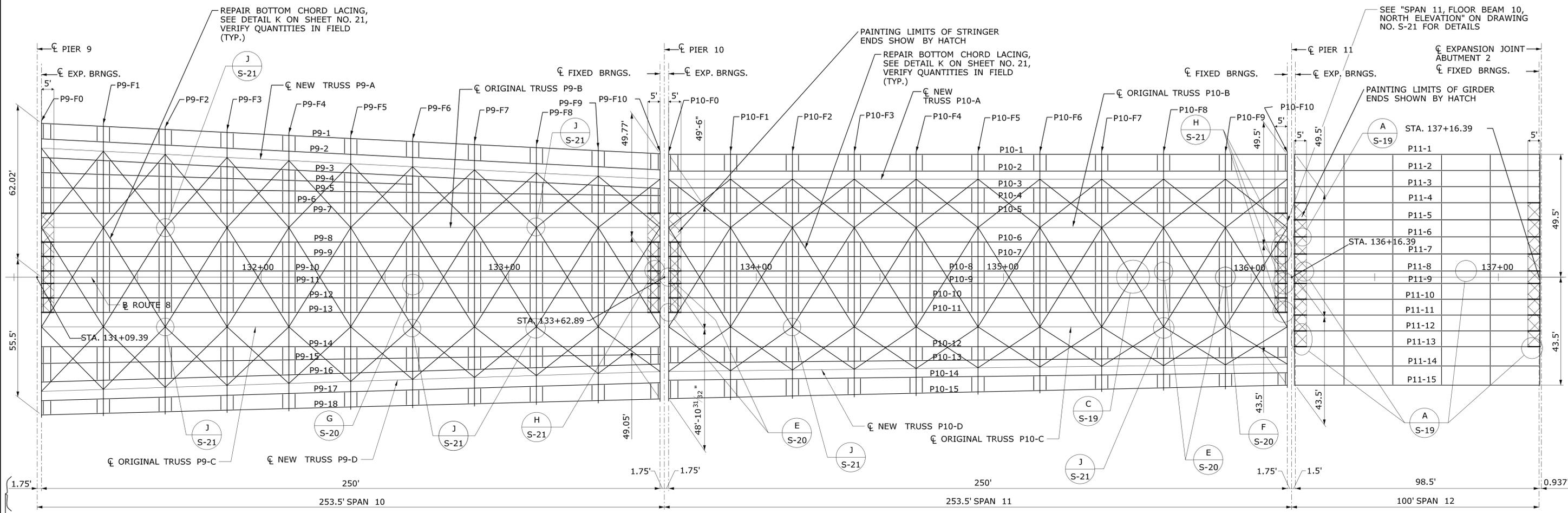
**FRAMING PLAN - SPANS 7, 8 & 9**  
SCALE: 1" = 20'-0"

**STRUCTURAL STEEL PAINTING NOTES**

1. SPAN 7:  
GIRDERS P6-4 TO P6-13, ARE ORIGINAL TYPE "B" PLATE GIRDERS.  
GIRDERS P6-1 TO P6-3, P6-14 TO P6-18 ARE NEWER PLATE GIRDERS.  
  
SPAN 8:  
GIRDERS P7-5 TO P7-14, ARE ORIGINAL TYPE "B" PLATE GIRDERS.  
GIRDERS P7-1 TO P7-4, P7-15 TO P7-19 ARE NEWER PLATE GIRDERS.  
  
ORIGINAL PLATE GIRDER TYPE "B"  
WEB PLATE = 65" x 7/16"  
TOP FLANGE VARIES FROM 16" x 3/4" TO 16" x 1" AT MIDSPAN  
BOTTOM FLANGE VARIES FROM 16" x 3/4" TO 16" x 1 3/4" AT MIDSPAN
2. GIRDERS P6-5, P6-6, P6-10 TO P6-13, P7-7, P7-8, P7-12 TO P7-14  
HAVE WT SECTION WELDED TO TOP OF TOP FLANGE. SEE DETAIL ON  
STEEL DETAIL SHEET.
3. ALL STRINGERS ARE W24 x 76 SECTIONS. ONLY THE ENDS OF ORIGINAL  
STRINGERS WILL REQUIRE ABRASIVE BLAST CLEANING AND FIELD PAINTING.  
SEE SPECIAL PROVISIONS "CLASS 3 - CONTAINMENT AND COLLECTION OF  
SURFACE PREPARATION DEBRIS (SITE NO. 1)" AND "LOCALIZED PAINT  
REMOVAL AND FIELD PAINTING OF BEAM ENDS"
4. FOR PAINTING LIMITS ON FLOORBEAMS IN TRUSS SPANS AND FLOORBEAM SIZES  
SEE TYPICAL CROSS SECTION ON DRAWING NO. S-06.
5. THE BEAM END PAINTING LIMITS SHOWN ARE FROM THE END OF THE GIRDER OR  
TRUSS TO THE DISTANCE 5'-0" AND SHALL INCLUDE ANY EXPOSED SURFACES OF  
THE GIRDER, TRUSS CHORD, FLOORBEAM AND STRINGER, ANY EXPOSED SURFACE OF  
THE CROSS FRAMING, CONNECTION PLATES, STIFFENERS, LATERAL BRACING, LACING  
AND STEEL BEARING ELEMENTS WITHIN THESE LIMITS.
6. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION  
"ABRASIVE BLAST CLEANING AND FIELD PAINTING OF STRUCTURE (SITE NO. 1)".
7. THE CONTAINMENT SYSTEM SHALL BE DESIGNED BY THE CONTRACTOR AND  
REVIEWED BY THE ENGINEER IN ACCORDANCE WITH THE SPECIAL PROVISION "CLASS  
1 - CONTAINMENT AND COLLECTION OF SURFACE PREPARATION DEBRIS (SITE NO.  
1)".
8. THE PAINTING OVER STEEL REPAIR AREA OTHER THAN BEAM END SHALL BE  
PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "FIELD TOUCHUP  
PAINTING".
9. SEE TRUSS ELEVATION SHEETS FOR PAINTING LIMITS AND INDICATION OF MEMBER  
SIZES.

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	DESIGNER/DRAFTER: <b>KAB/ALH</b>	CHECKED BY: <b>CC</b>	SCALE AS NOTED
				THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>		SIGNATURE/ BLOCK:
				PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b>	DRAWING NO. <b>S-15</b>
				FRAMING PLAN 2 OF 3			
				Filename: ...SB_MSH_Br00571A_126-170_FRM2.dgn			



**FRAMING PLAN - SPANS 10, 11 & 12**

SCALE: 1" = 20'-0"

**STRUCTURAL STEEL PAINTING NOTES**

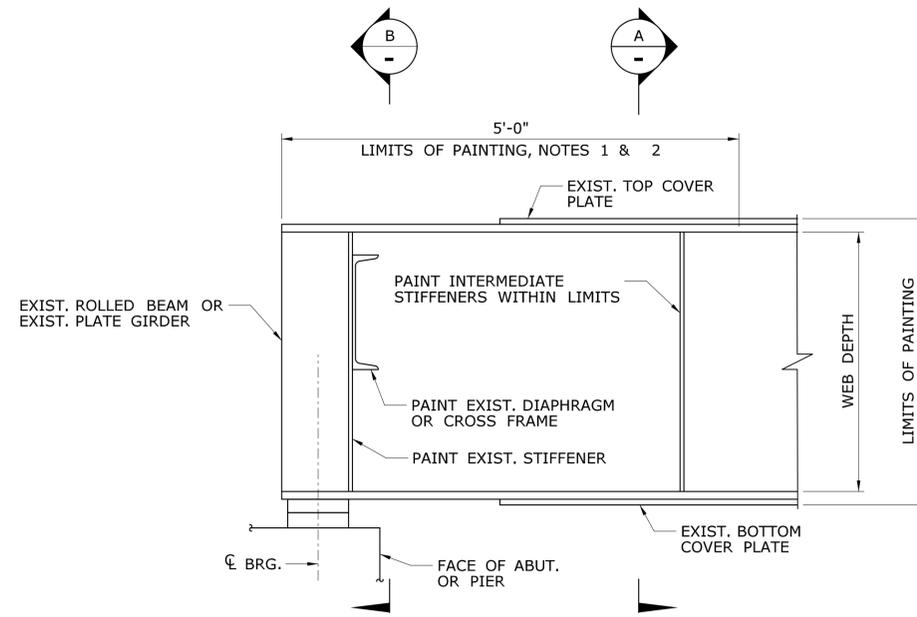
- SPAN 12: GIRDERS P11-4 TO P11-13, ARE ORIGINAL TYPE "B" PLATE GIRDERS. GIRDERS P11-1 TO P11-3, P11-14 AND P11-15 ARE NEWER PLATE GIRDERS.  
  
ORIGINAL PLATE GIRDER TYPE "B"  
WEB PLATE = 65" x 7/16"  
TOP FLANGE VARIES FROM 16" x 3/4" TO 16" x 1" AT MIDSPAN  
BOTTOM FLANGE VARIES FROM 16" x 3/4" TO 16" x 1 3/4" AT MIDSPAN
- GIRDERS P11-11, P11-12, AND P11-13 HAVE WT SECTION WELDED TO TOP OF TOP FLANGE.
- ALL STRINGERS ARE W24 x 76 SECTIONS. ONLY THE ENDS OF ORIGINAL STRINGERS WILL REQUIRE ABRASIVE BLAST CLEANING AND FIELD PAINTING. SEE SPECIAL PROVISIONS "CLASS 3 - CONTAINMENT AND COLLECTION OF SURFACE PREPARATION DEBRIS (SITE NO. 1)" AND "LOCALIZED PAINT REMOVAL AND FIELD PAINTING OF BEAM ENDS".
- FOR PAINTING LIMITS AND SIZE DESIGNATIONS ON FLOORBEAMS IN TRUSS SPAN SEE TYPICAL CROSS SECTION DWG. NO. S-07.
- THE BEAM END PAINTING LIMITS SHOWN ARE FROM THE END OF THE GIRDER OR TRUSS TO THE DISTANCE 5'-0" AND SHALL INCLUDE ANY EXPOSED SURFACES OF THE GIRDER, TRUSS CHORD, FLOORBEAM AND STRINGER, ANY EXPOSED SURFACE OF THE CROSS FRAMING, CONNECTION PLATES, STIFFENERS, LATERAL BRACING, LACING AND STEEL BEARING ELEMENTS WITHIN THESE LIMITS.
- ALL WORK WITHIN BEAM END SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "ABRASIVE BLAST CLEANING AND FIELD PAINTING OF STRUCTURE (SITE NO. 1)".
- THE CONTAINMENT SYSTEM WITHIN BEAM END SHALL BE DESIGNED BY THE CONTRACTOR AND REVIEWED BY THE ENGINEER IN ACCORDANCE WITH THE SPECIAL PROVISION "CLASS 1 - CONTAINMENT AND COLLECTION OF SURFACE PREPARATION DEBRIS (SITE NO. 1)".
- THE PAINTING OVER STEEL REPAIR AREA OTHER THAN BEAM END SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "FIELD TOUCHUP PAINTING".

**90% PROGRESS SUBMISSION**

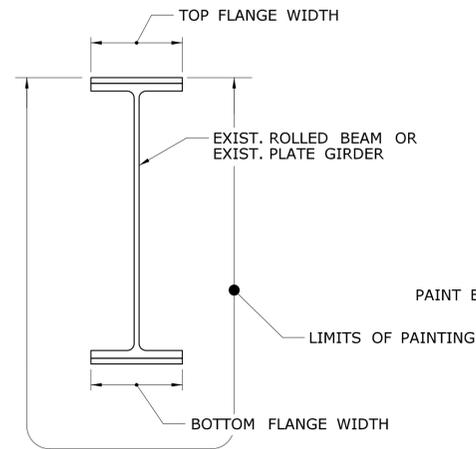
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...\\SB_MSH_Br00571A_126-170_FRM3.dgn	SIGNATURE/BLOCK: 	PROJECT TITLE: <b>COMMODORE HULL BRIDGE</b> <b>PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON</b> <b>DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-16</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>FRAMING PLAN</b> <b>3 OF 3</b>		

**PAINTING NOTES**

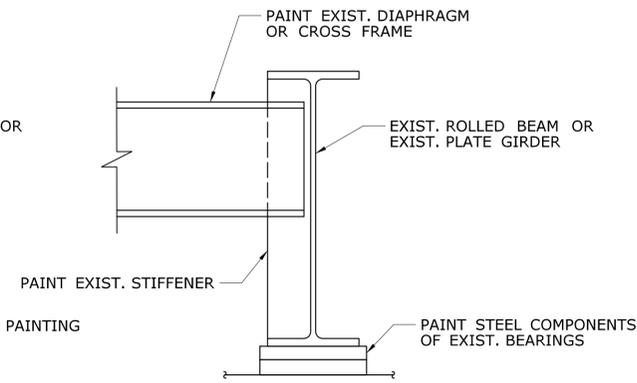
1. THE BEAM END PAINTING LIMITS SHOWN ARE FROM THE END OF THE GIRDER TO THE DISTANCE 5'-0" AND SHALL INCLUDE ANY EXPOSED SURFACES OF THE GIRDER, ANY EXPOSED SURFACE OF THE CROSS FRAMING, CONNECTION PLATES, STIFFENERS, LATERAL BRACING AND STEEL BEARING ELEMENTS WITHIN THESE LIMITS.
2. ALL WORK WITHIN BEAM END SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "ABRASIVE BLAST CLEANING AND FIELD PAINTING OF STRUCTURE (SITE NO. 1)".
3. THE CONTAINMENT SYSTEM WITHIN BEAM END SHALL BE DESIGNED BY THE CONTRACTOR AND REVIEWED BY THE ENGINEER IN ACCORDANCE WITH THE SPECIAL PROVISION "CLASS 1 - CONTAINMENT AND COLLECTION OF SURFACE PREPARATION DEBRIS (SITE NO. 1)".
4. THE PAINTING OVER STEEL REPAIR AREA OTHER THAN BEAM END SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "FIELD TOUCHUP PAINTING".



**TYPICAL BEAM/GIRDER ELEVATION**  
NOT TO SCALE



**SECTION A**  
NOT TO SCALE



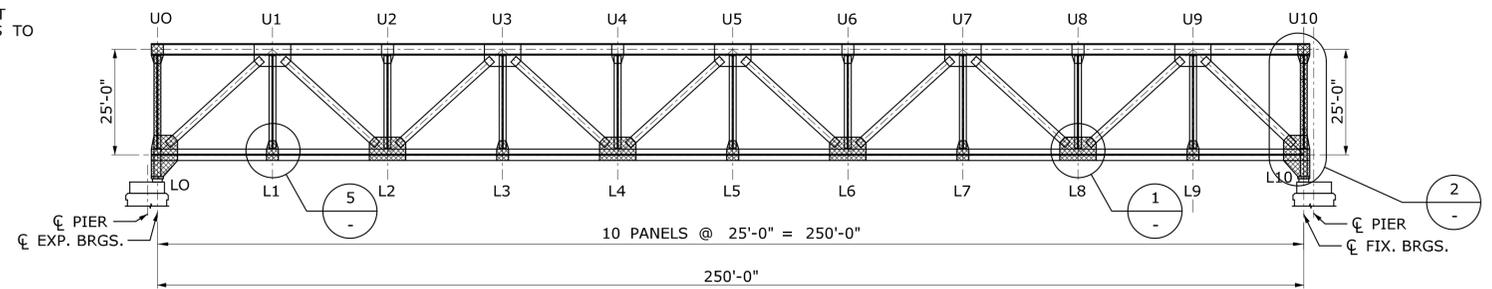
**SECTION B**  
NOT TO SCALE

**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>APPROACH SPANS PAINTING DETAILS</b>			

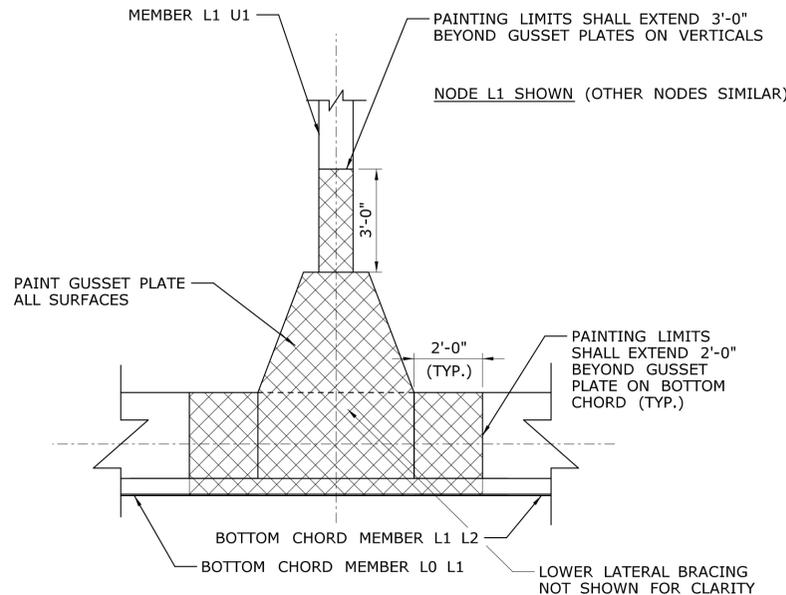
**STRUCTURAL STEEL PAINTING NOTES**

1. PAINT END NODES TO BE PAID UNDER THE ITEM "LOCALIZED PAINT REMOVAL AND FIELD PAINTING OF BEAM ENDS". PAINT ALL OTHER NODES TO BE PAID UNDER THE ITEM "FIELD TOUCH-UP PAINTING".



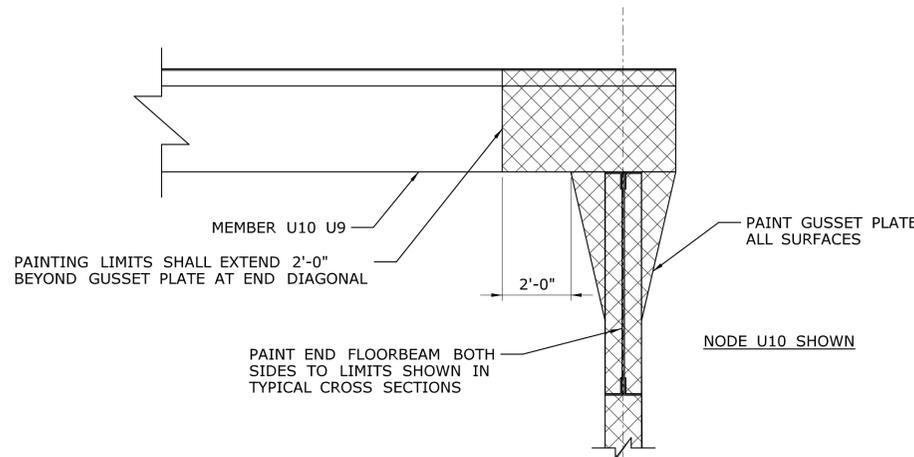
**ELEVATION (SPAN 9-TRUSS B & C, SPAN 10-TRUSS B & C, SPAN 11-TRUSS B & C)**

SCALE: 1"=20'-0"



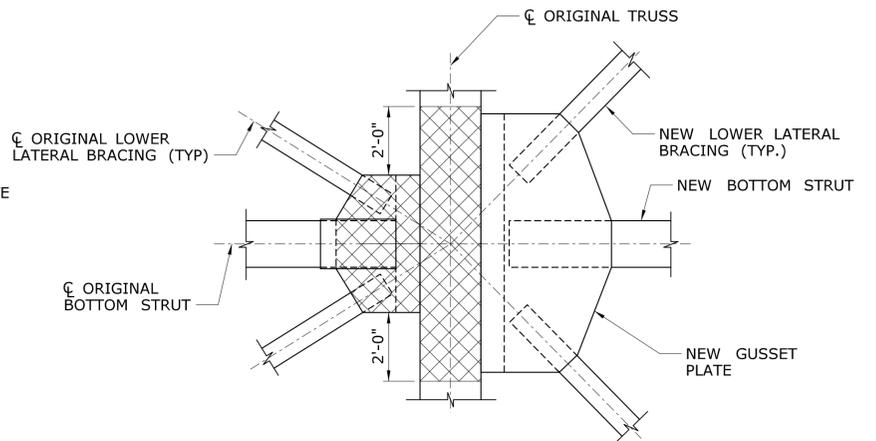
**5 TYPICAL BOTTOM NODE PAINTING LIMITS**

SCALE: 3/8" = 1'-0"



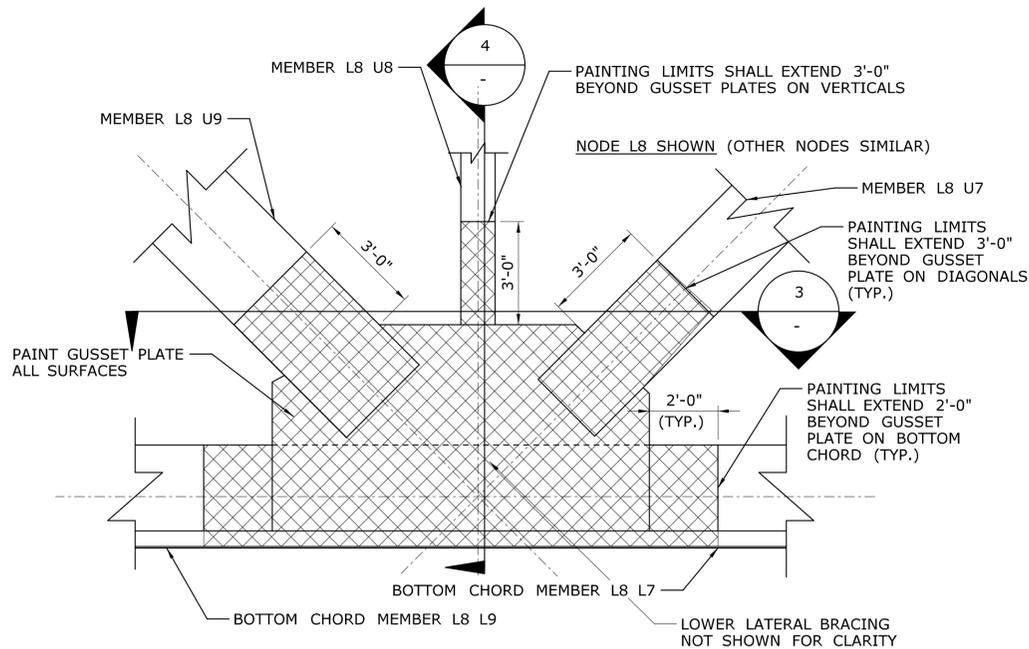
**2 TYPICAL END NODE PAINTING LIMITS**

SCALE: 3/8" = 1'-0"



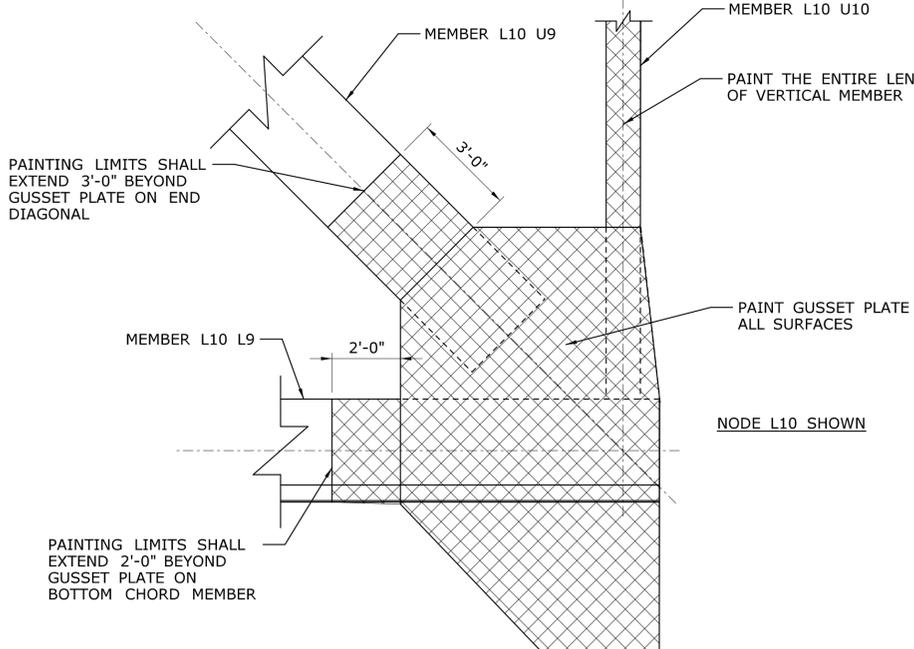
**3 TYPICAL BOTTOM NODE PAINTING LIMITS**

SCALE: 3/8" = 1'-0"



**1 TYPICAL BOTTOM NODE PAINTING LIMITS**

SCALE: 3/8" = 1'-0"

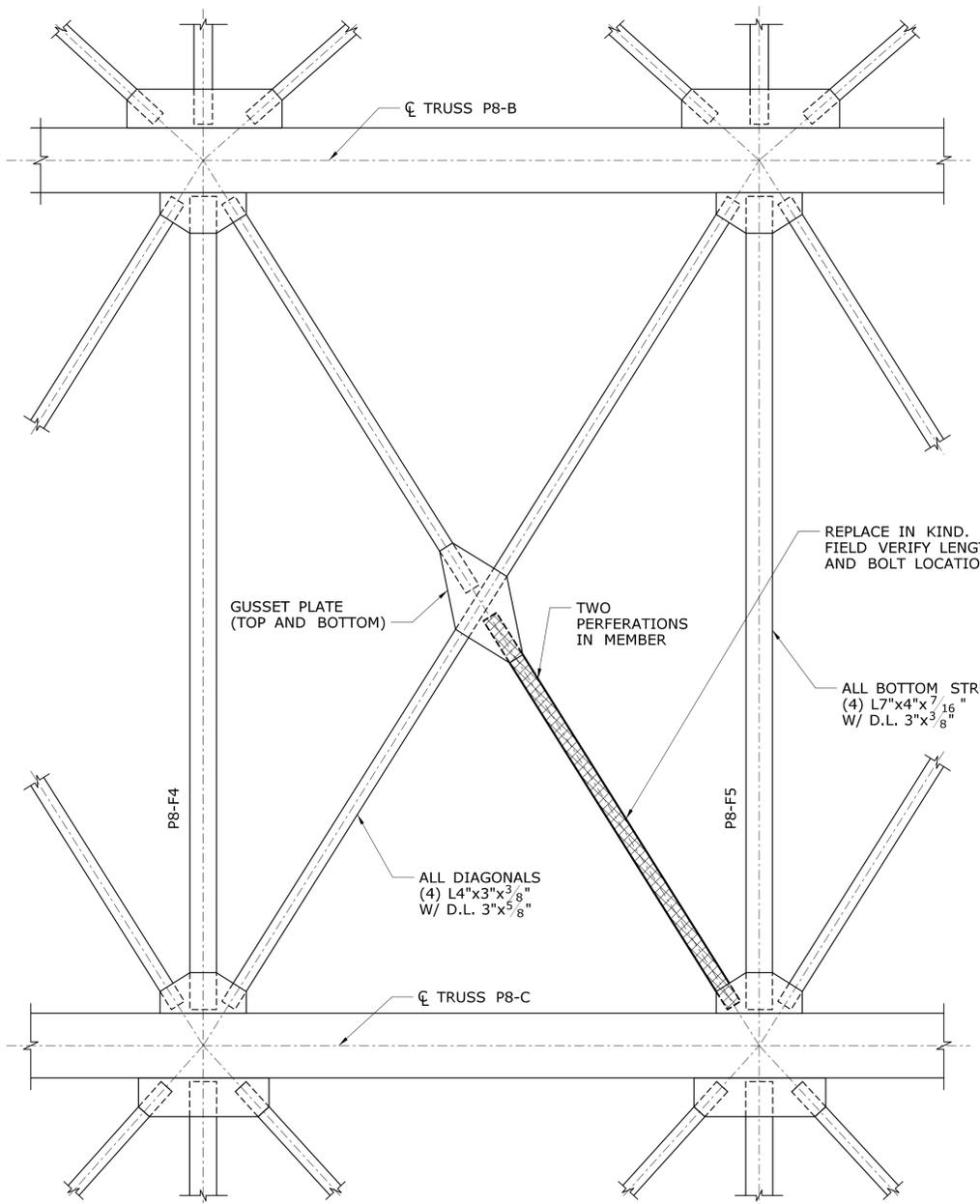


**4 TYPICAL BOTTOM NODE PAINTING LIMITS**

SCALE: 3/8" = 1'-0"

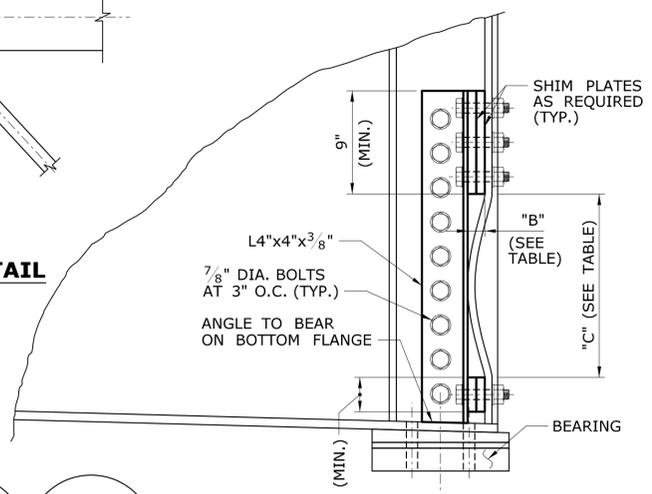
**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		



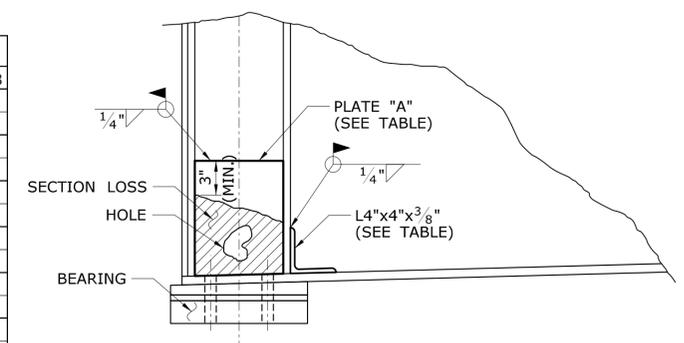
**C CROSS BRACING BOTTOM CHORD REPAIR DETAIL**  
 SCALE: 1/4" = 1'-0"

REPAIR LOCATION		DIMENSION "B"	DIMENSION "C"
GIRDER	SPAN LOCATION		
3	6 PIER 5 - WEST SIDE	3/4"	1'-0"
7	7 PIER 7 - EAST SIDE	1 1/2"	1'-4"
8	7 PIER 7 - WEST SIDE	3/4"	1'-2"



**B BEARING STIFFENER REPAIR DETAIL**  
 SCALE: 1 1/2" = 1'-0"

GIRDER	SPAN	REPAIR LOCATION		PLATE "A"		
		LOCATION	HEIGHT	WIDTH	THICKNESS	LENGTH
9	2	PIER 1	8"	7 1/2"	7/16"	7"
3	2	PIER 2	9"	7 1/2"	7/16"	7"
4	2	PIER 2	7"	7 1/2"	7/16"	7"
5	2	PIER 2	9"	7 1/2"	7/16"	7"
8	2	PIER 2	9"	7 1/2"	7/16"	7"
10	2	PIER 2	8"	7 1/2"	7/16"	7"
11	2	PIER 2	6"	7 1/2"	7/16"	7"
3	3	PIER 2	6"	7 1/2"	7/16"	7"
4	3	PIER 2	9"	7 1/2"	7/16"	7"
5	3	PIER 2	6"	7 1/2"	7/16"	7"
6	3	PIER 2	6"	7 1/2"	7/16"	7"
7	3	PIER 2	7"	7 1/2"	7/16"	7"
8	3	PIER 2	9"	7 1/2"	7/16"	7"
9	3	PIER 2	7"	7 1/2"	7/16"	7"
10	3	PIER 2	9"	7 1/2"	7/16"	7"
11	3	PIER 2	7"	7 1/2"	7/16"	7"
3	3	PIER 3	7"	7 1/2"	7/16"	7"
4	3	PIER 3	7"	7 1/2"	7/16"	7"
6	3	PIER 3	10"	7 1/2"	7/16"	7"
8	3	PIER 3	7"	7 1/2"	7/16"	7"
9	3	PIER 3	7"	7 1/2"	7/16"	7"
10	3	PIER 3	6"	7 1/2"	7/16"	7"
11	3	PIER 3	9"	7 1/2"	7/16"	7"
4	4	PIER 3	1'-0"	7 1/2"	7/16"	7"
5	4	PIER 3	7"	7 1/2"	7/16"	7"
6	4	PIER 3	7"	7 1/2"	7/16"	7"
7	4	PIER 3	9"	7 1/2"	7/16"	7"
8	4	PIER 3	7"	7 1/2"	7/16"	7"
10	4	PIER 3	8"	7 1/2"	7/16"	7"
3	4	PIER 4	7"	7 1/2"	7/16"	7"
4	4	PIER 4	7"	7 1/2"	7/16"	7"
6	4	PIER 4	1'-0"	7 1/2"	7/16"	7"
8	4	PIER 4	8"	7 1/2"	7/16"	7"
10	4	PIER 4	9"	7 1/2"	7/16"	7"
6	5	PIER 4	8"	7 1/2"	7/16"	7"
8	5	PIER 4	8"	7 1/2"	7/16"	7"
8	5	PIER 5	7"	7 1/2"	7/16"	7"
9	5	PIER 5	8"	7 1/2"	7/16"	7"
10	5	PIER 5	8"	7 1/2"	7/16"	7"
4	6	PIER 5	9"	7 1/2"	7/16"	7"
4	6	PIER 6	8"	7 1/2"	7/16"	7"
8	6	PIER 6	9"	7 1/2"	7/16"	7"
4	7	PIER 7	9"	7 1/2"	7/16"	7"
6	7	PIER 7	11"	7 1/2"	7/16"	7"
10	7	PIER 7	9"	7 1/2"	7/16"	7"
11	7	PIER 7	10"	7 1/2"	7/16"	7"
13	7	PIER 7	1'-0"	7 1/2"	7/16"	7"
5	8	PIER 7	10"	7 1/2"	7/16"	7"
6	8	PIER 7	10"	7 1/2"	7/16"	7"
7	8	PIER 7	10"	7 1/2"	7/16"	7"
9	8	PIER 7	10"	7 1/2"	7/16"	7"
11	8	PIER 7	8"	7 1/2"	7/16"	7"
12	8	PIER 7	11"	7 1/2"	7/16"	7"
5	8	PIER 8	N/A	N/A	N/A	7"
6	8	PIER 8	9"	7 1/2"	7/16"	7"
8	8	PIER 8	9"	7 1/2"	7/16"	7"
9	8	PIER 8	N/A	N/A	N/A	7"
11	8	PIER 8	N/A	N/A	N/A	7"
12	8	PIER 8	N/A	N/A	N/A	7"
13	8	PIER 8	10"	7 1/2"	7/16"	7"
14	8	PIER 8	N/A	N/A	N/A	7"
8	12	MIDSPAN	2'-0"	2'-0"	7/16"	N/A
13	12	N. ABUT	7"	7 1/2"	7/16"	7"



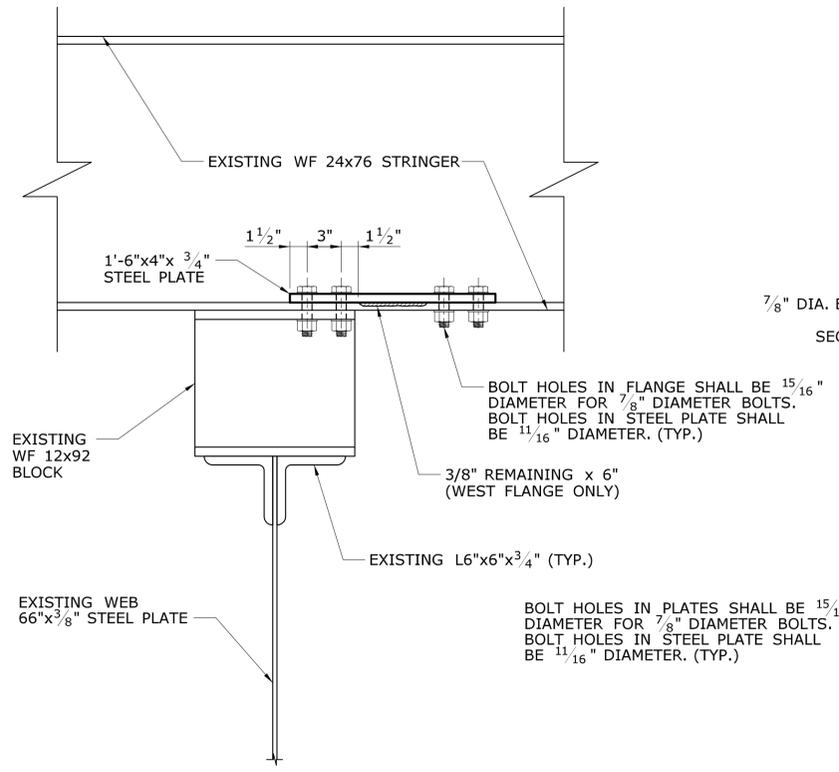
**A GIRDER WEB REPAIR DETAIL**  
 SCALE: 1 1/2" = 1'-0"

- STRUCTURAL STEEL NOTES**
- STRUCTURAL STEEL FOR BOLTED REPAIRS SHALL CONFORM TO AASHTO M270, GRADE 50 T2, AND BE GALVANIZED IN ACCORDANCE WITH ASTM A123.
  - STRUCTURAL STEEL FOR WEB REPAIRS AND STEEL BENTS SHALL CONFORM TO AASHTO M270, GRADE 50 T2.
  - WELDING DETAILS, PROCEDURES AND TESTING METHODS SHALL CONFORM TO THE AASHTO/AWS D1.5 BRIDGE WELDING CODE, UNLESS OTHERWISE NOTED ON THE PLANS.
  - COST OF BOLT REMOVAL FOR STEEL REPAIR INCLUDED IN THE ITEM "STRUCTURAL STEEL"
  - THE STRUCTURAL STEEL FABRICATORS SHALL BE CERTIFIED UNDER THE AISC QUALITY CONTROL PROGRAM AS "CATEGORY Sbr - SIMPLE STEEL BRIDGES".

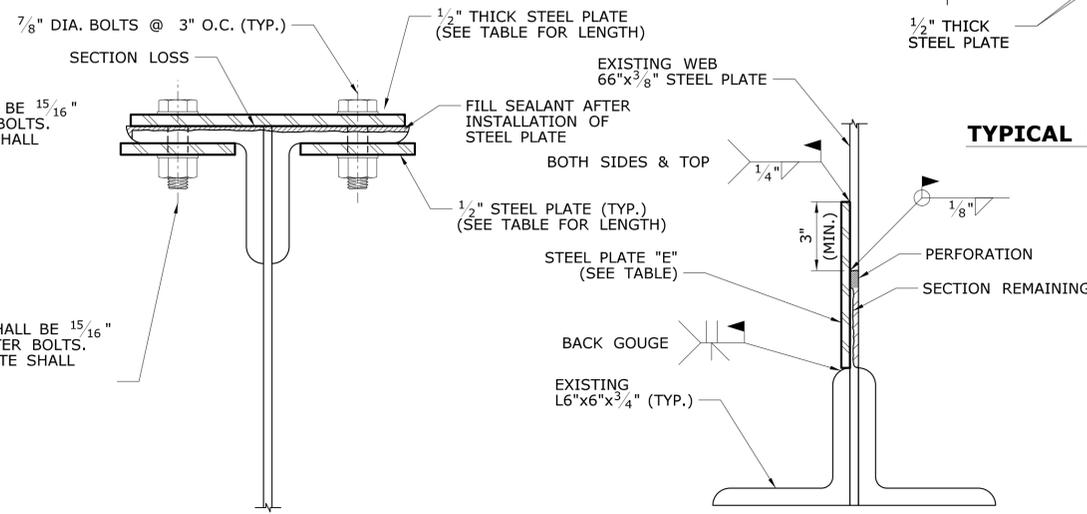
- STEEL REPAIR NOTES**
- DETERIORATED PORTIONS OF THE GIRDER WEB WITHIN THE LIMITS OF PAINTING SHALL BE REPAIRED IN ACCORDANCE WITH THESE DETAILS AND AS ORDERED BY THE ENGINEER. THE DEFINITION OF DETERIORATED IS OBVIOUS PERFORATIONS, SLIVER CONDITIONS, AND EXTREME SECTION LOSS OF GREATER THAN 1/3 OF ORIGINAL STEEL WEB THICKNESS.
  - PRIOR TO STARTING ANY OF THE STEEL REPAIR WORK, THE DETERIORATED AREAS SHALL BE ABRASIVE BLAST CLEANED ACCORDING TO THE LIMITS SHOWN ON THESE PLANS, SPECIFICATIONS, OR AS ORDERED BY THE ENGINEER. SEE SPECIAL PROVISION "ABRASIVE BLAST CLEANING AND FIELD PAINTING" OR "NTC - LOCALIZED PAINT REMOVAL" FOR MORE INFORMATION
  - THE SIZE OF REPAIR PLATES AND THE LENGTH OF REPAIR ANGLES SHALL BE DETERMINED BY THE ENGINEER BASED ON FIELD MEASUREMENTS TAKEN AT THE DETERIORATED LOCATIONS INDICATED ON THE FRAMING PLANS PRIOR TO SHOP DRAWING PREPARATION AND FABRICATION.
  - WHERE POSSIBLE, WEB REPAIR PLATES SHALL EXTEND 3" BEYOND THE LIMIT OF THE MAJOR SECTION LOSSES. PLATE AND SHAPE SIZES ARE FOR ESTIMATING PURPOSES. FINAL DIMENSIONS TO BE CONFIRMED BY FIELD MEASUREMENTS AND WITH ENGINEER PRIOR TO SHOP DRAWING PREPARATION AND FABRICATION.
  - THE NEW PLATE(S) SHALL BE WELDED OR BOLTED TO THE CLEANED STEEL.
  - ALL NEW PLATES AND ANGLES, SHALL RECEIVE THE NEPCOAT APPROVED PAINT SYSTEM. SEE SPECIAL PROVISION "STRUCTURAL STEEL" FOR MORE INFORMATION.
  - PLATE DIMENSIONS SHALL BE IN ACCORDANCE WITH THE FOLLOWING:
    - WHERE CALLED OUT ON THE FRAMING PLANS, THE ENGINEER SHALL TAKE FIELD MEASUREMENTS AT DETERIORATED BEAM ENDS. WELD SIZES WILL ALSO BE MEASURED TO INSURE THE INDICATED PLATE CHAMFERS WILL BE ADEQUATE TO AVOID CONFLICTS WITH THE EXISTING WELDS.
    - THE ENGINEER SHALL DEVELOP ACCURATE DRAWINGS OF THE EXISTING CONDITIONS BASED ON THESE MEASUREMENTS AND INCLUDE SUGGESTED PLATE DIMENSIONS.
    - CONTRACTOR SHALL THEN OBTAIN THE NEW PLATES IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS FOR "STRUCTURAL STEEL".
  - MULTIPLE PASS WELDS, INSPECTED BY THE MAGNETIC PARTICLE METHOD SHALL HAVE EACH PASS OR LAYER INSPECTED AND ACCEPTED BEFORE PROCEEDING TO THE NEXT PASS OR LAYER, AS DETERMINED BY THE ENGINEER.
  - ALL STEEL REPAIRS INCLUDE SEALANT SHALL BE PAID FOR UNDER THE ITEM "STRUCTURAL STEEL".

**90% PROGRESS SUBMISSION**

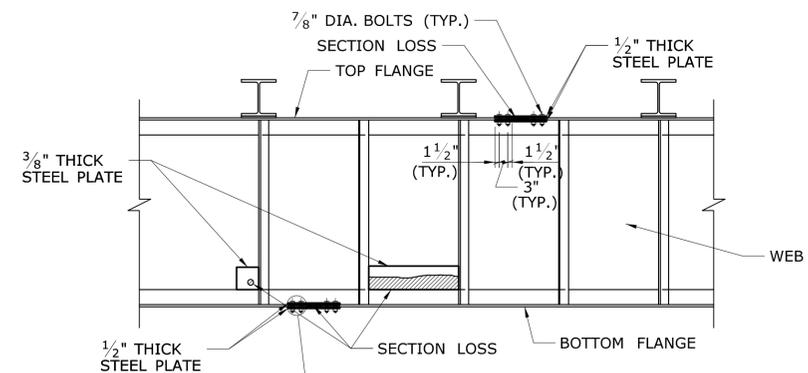
FLOORBEAM TOP FLANGE REPAIR TABLE			
REPAIR LOCATION		LOCATION	REPAIR LENGTH
FLOORBEAM	SPAN		
8	9	BETWEEN S16 & S17	3'-0"
9	11	BETWEEN S8 & S9	3'-0"



**G**  
**S-16**  
**STRINGER BOTTOM FLANGE REPAIR DETAIL**  
SCALE: 1 1/2" = 1'-0"



**F**  
**S-15**  
**F**  
**S-16**  
**FLOORBEAM TOP FLANGE REPAIR DETAIL**  
SCALE: 3" = 1'-0"



**TYPICAL FLOORBEAM REPAIR ELEVATION**  
N.T.S.

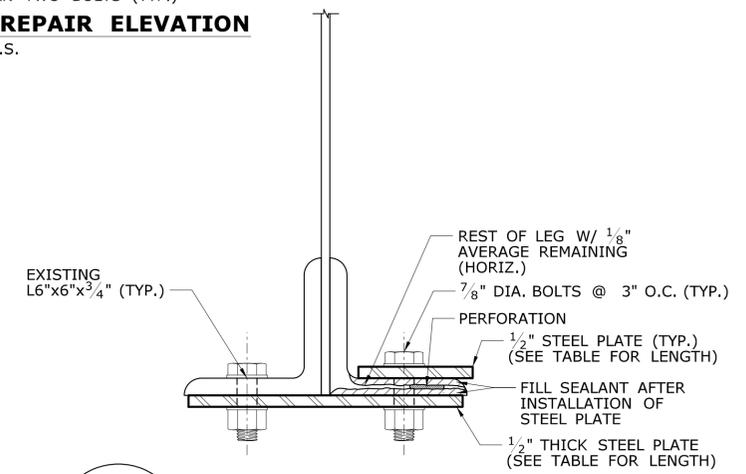
PERFORATION

SECTION REMAINING

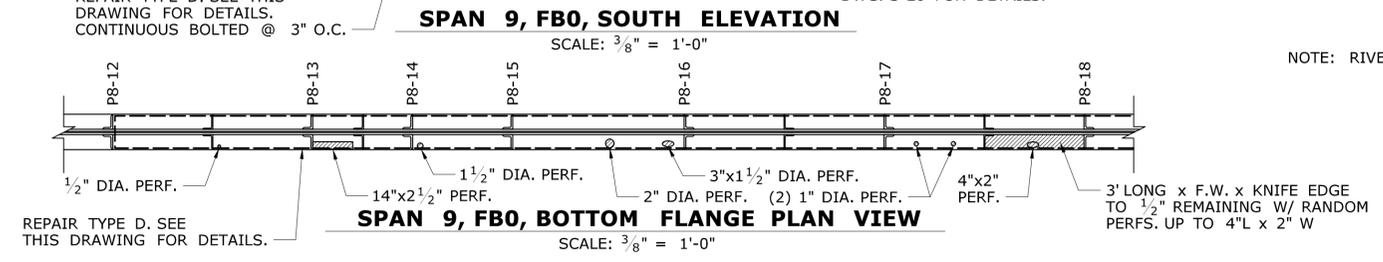
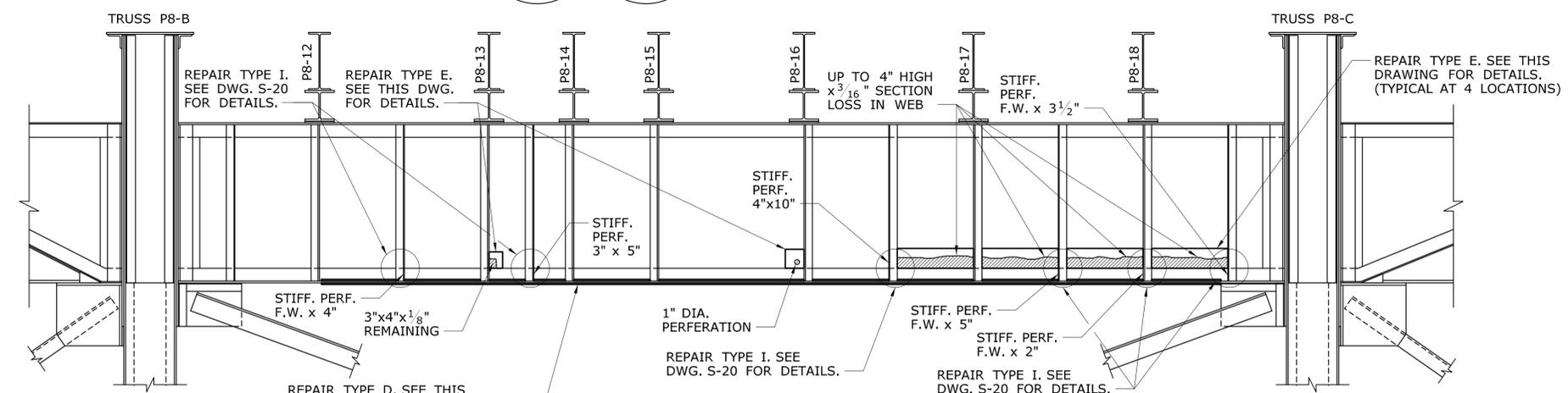
3" (MIN.)

1/8"

**E**  
**S-16**  
**FLOORBEAM WEB REPAIR DETAIL**  
SCALE: 3" = 1'-0"



**D**  
**S-15**  
**FLOORBEAM BOTTOM FLANGE REPAIR DETAIL**  
SCALE: 3" = 1'-0"



FLOORBEAM BOTTOM FLANGE REPAIR TABLE			
REPAIR LOCATION		LOCATION	REPAIR LENGTH
FLOORBEAM	SPAN		
0	9	BETWEEN P8-12 & P8-C	50'
3	9	BETWEEN S15 & S16	6'
10	11	BETWEEN P10-8 & P10-B	10'

FLOORBEAM WEB REPAIR TABLE					
REPAIR LOCATION			PLATE "E"		
FLOORBEAM	SPAN	LOCATION	HEIGHT	WIDTH	THICKNESS
0	9	P8-13	7"	6"	3/8"
0	9	P8-16	7"	6"	3/8"
0	9	BETWEEN P8-16 & P8-17	7"	2'-6"	3/8"
0	9	BETWEEN P8-17 & P8-18	7"	2'-6"	3/8"
0	9	BETWEEN P8-17 & P8-18	7"	2'-6"	3/8"
0	9	BETWEEN P8-18 & P8-C	7"	2'-6"	3/8"
0	11	BETWEEN S8 & S9	5"	4'-6"	3/8"
0	11	S11	5"	5"	3/8"
8	11	S8	5"	6"	3/8"
9	11	BETWEEN S8 & S9	1'-0"	1'-0"	3/8"

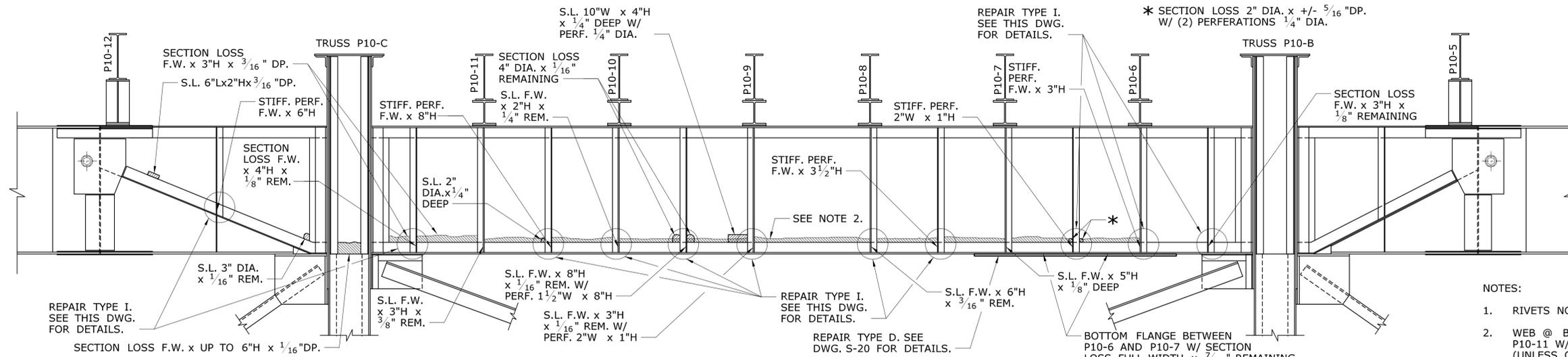
**90% PROGRESS SUBMISSION**

<p>DESIGNER/DRAFTER: <b>KAB/ALH</b></p> <p>CHECKED BY: <b>CC</b></p> <p>SCALE AS NOTED</p>	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> <p>Signature/Block:</p>	<p>PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b></p>	<p>TOWN: <b>SHELTON DERBY</b></p> <p>DRAWING TITLE: <b>STEEL REPAIR DETAILS 2</b></p>	<p>PROJECT NO. <b>126-170</b></p> <p>DRAWING NO. <b>S-20</b></p> <p>SHEET NO.</p>
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THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 10/1/2015

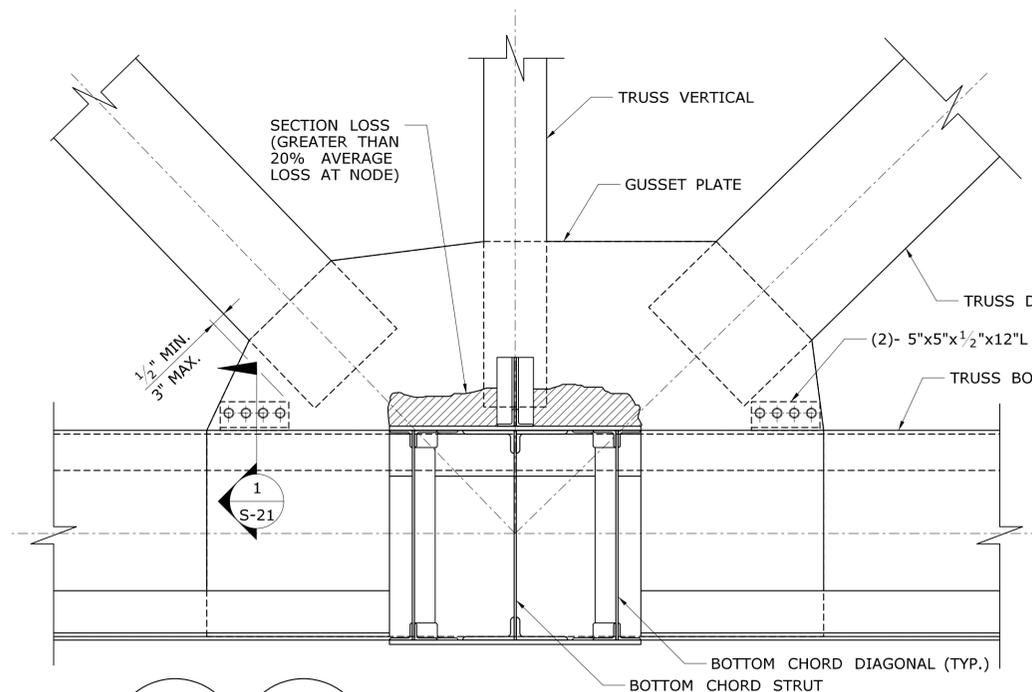
Filename: ...SB\_MSH\_Br00571A\_126-170\_STD2.dgn



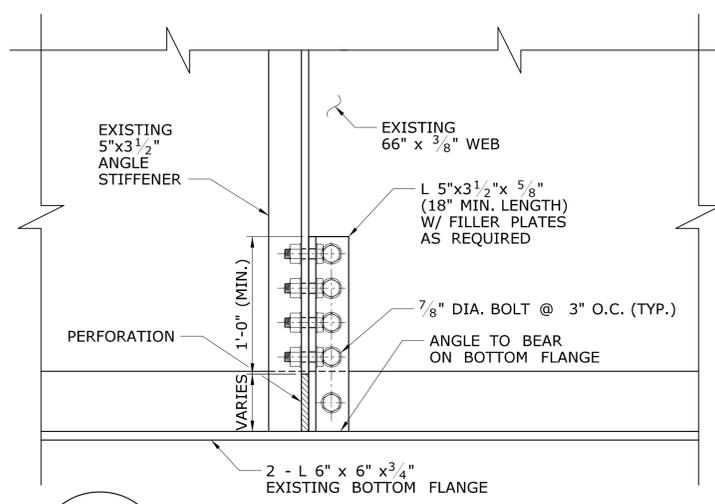
**SPAN 11, FB10, NORTH ELEVATION**  
SCALE: 3/8" = 1'-0"

GUSSET PLATE REPAIR LOCATIONS		
SPAN	TRUSS	NODE
9	P8-C	L2
10	P9-B	L2
10	P9-B	L8
10	P9-C	L2
10	P9-C	L6
10	P9-C	L8
11	P10-C	L2
11	P10-C	L8

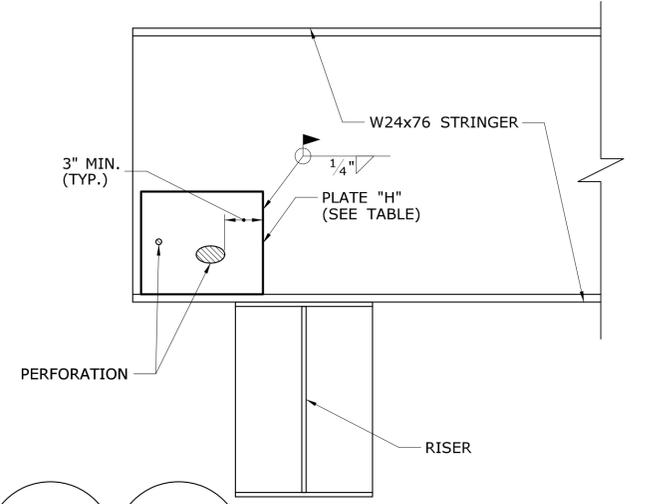
STRINGER WEB REPAIR TABLE					
REPAIR LOCATION			PLATE "H"		
STRINGER	SPAN	LOCATION	HEIGHT	WIDTH	THICKNESS
18	9	PIER 9	1'-0"	1'-0"	3/8"
10	10	PIER 10	6"	7"	3/8"
6	11	PIER 11	1'-6"	2'-0"	3/8"
7	11	PIER 11	9"	9"	3/8"
9	11	PIER 11	9"	1'-6"	3/8"
11	11	PIER 11	9"	1'-6"	3/8"



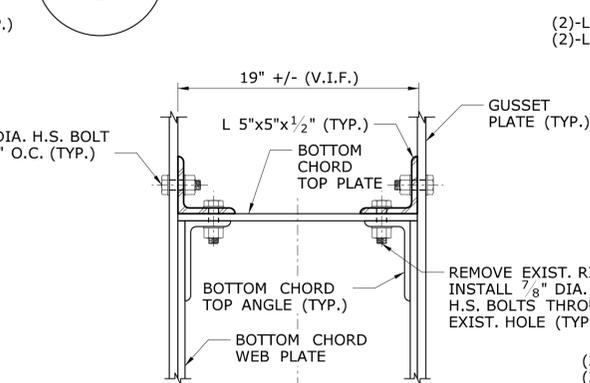
**GUSSET PLATE REPAIR DETAIL**  
SCALE: 3/4" = 1'-0"



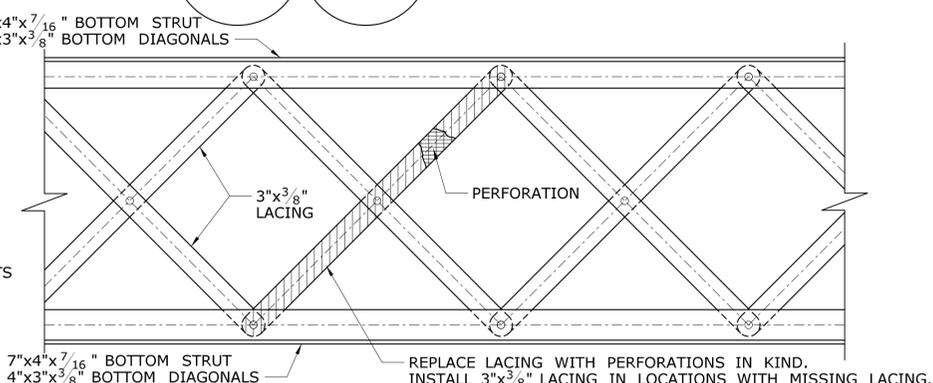
**FLOORBEAM STIFFENER REPAIR DETAIL**  
SCALE: 1 1/2" = 1'-0"



**STRINGER WEB REPAIR DETAIL**  
SCALE: 1 1/2" = 1'-0"



**GUSSET PLATE REPAIR SECTION**  
SCALE: 1 1/2" = 1'-0"



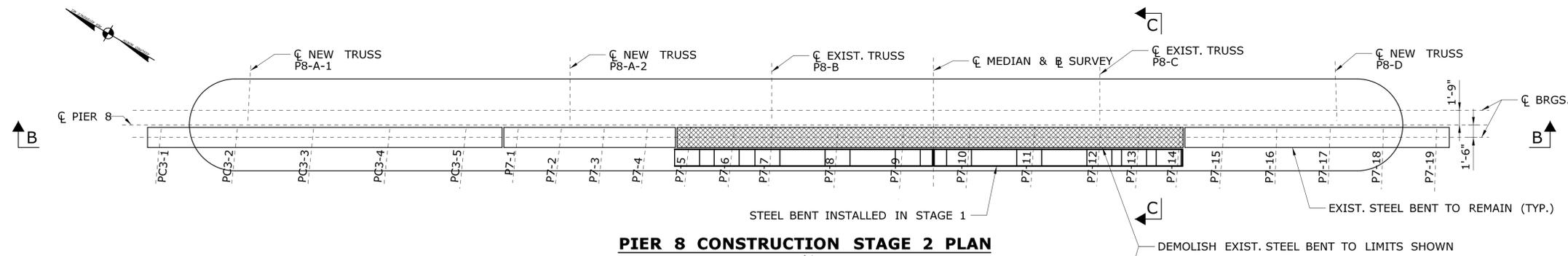
**BOTTOM CHORD LACING REPAIR DETAIL**  
SCALE: 1" = 1'-0"

**90% PROGRESS SUBMISSION**

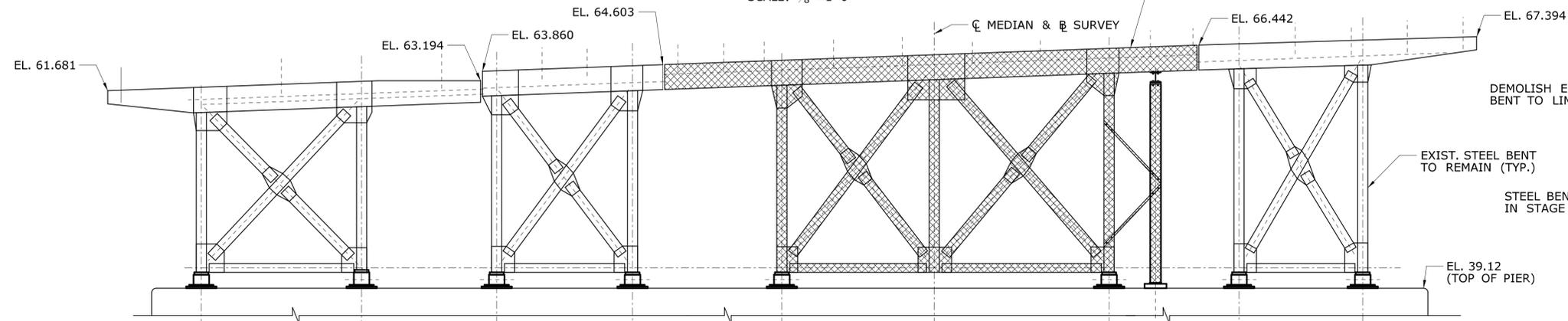
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION Filename: ...SB_MSH_Br00571A_126-170_STD3.dgn	SIGNATURE/BLOCK:  PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b> DRAWING TITLE: <b>STEEL REPAIR DETAILS 3</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-21</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		

**SUGGESTED CONSTRUCTION STAGE**

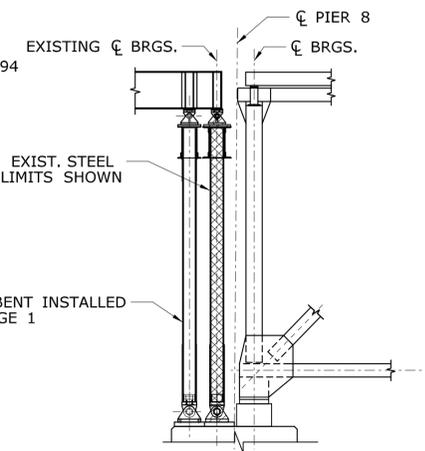
1. INSTALL STEEL BENT AT PIER 8 WHILE MAINTAIN EXISTING BENT.
2. DEMOLISH EXISTING STEEL BENT.



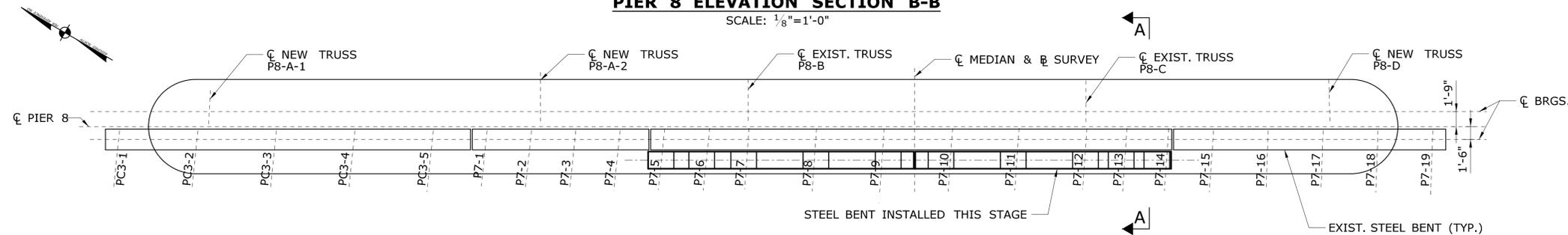
**PIER 8 CONSTRUCTION STAGE 2 PLAN**  
SCALE: 1/8"=1'-0"



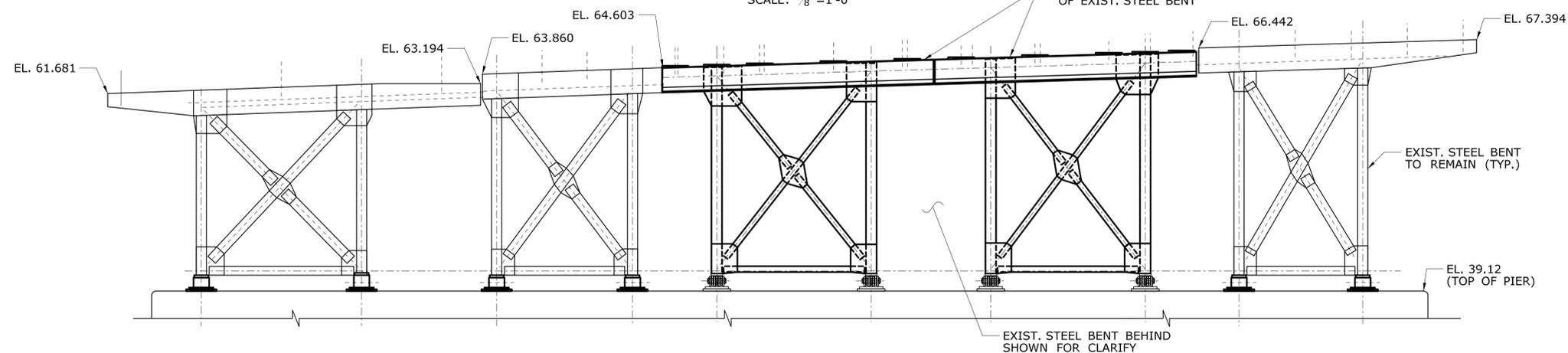
**PIER 8 ELEVATION SECTION B-B**  
SCALE: 1/8"=1'-0"



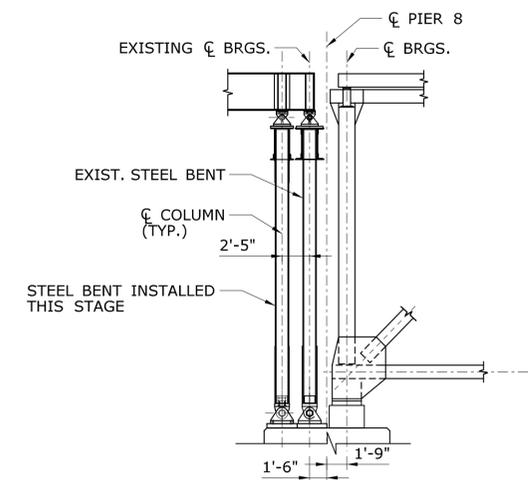
**SECTION C-C**  
SCALE: 1/8"=1'-0"



**PIER 8 CONSTRUCTION STAGE 1 PLAN**  
SCALE: 1/8"=1'-0"



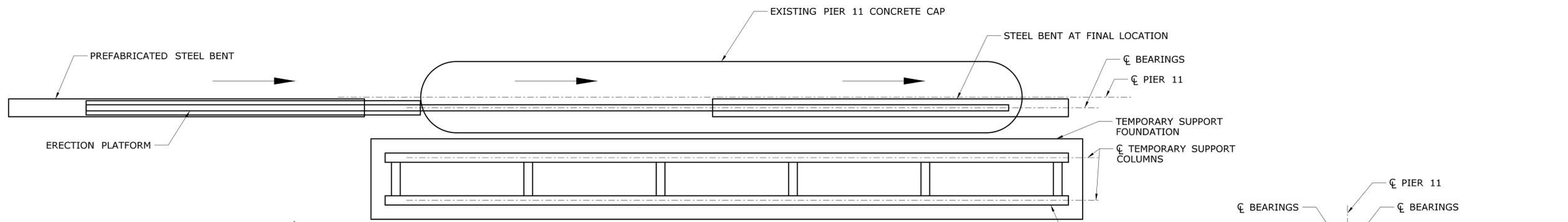
**PIER 8 ELEVATION LOOKING NORTH**  
SCALE: 1/8"=1'-0"



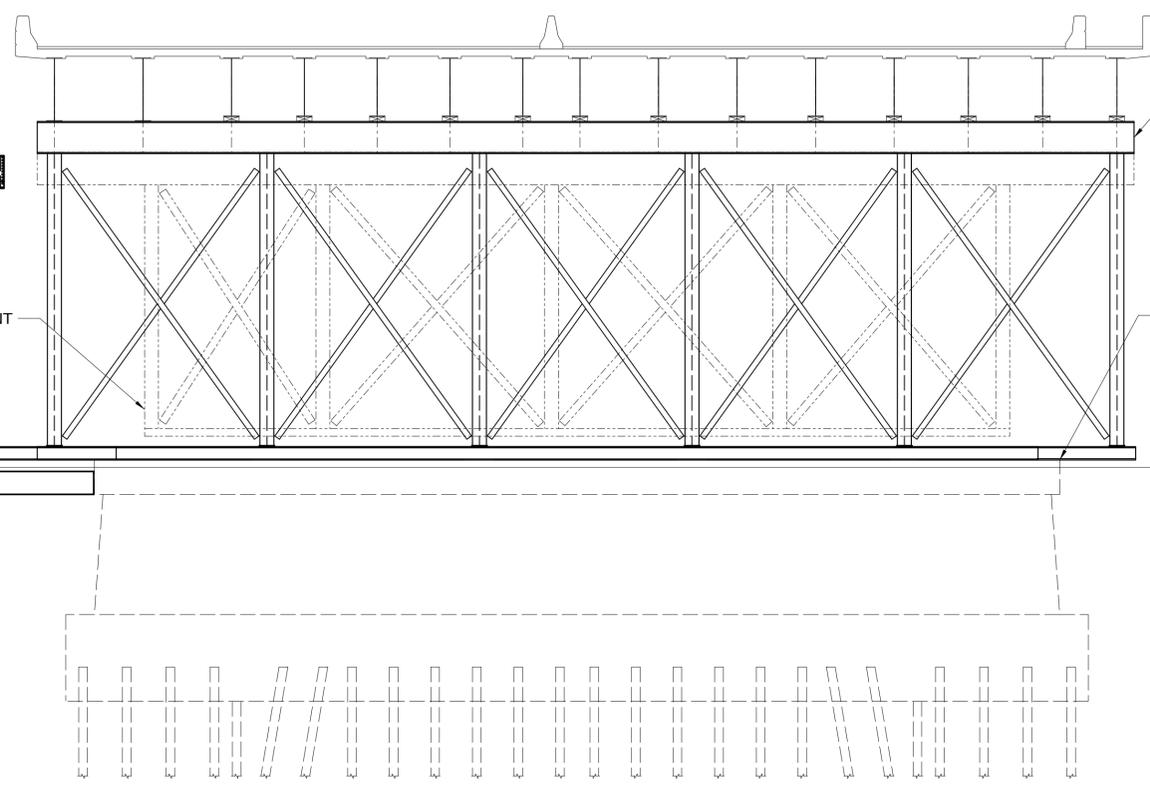
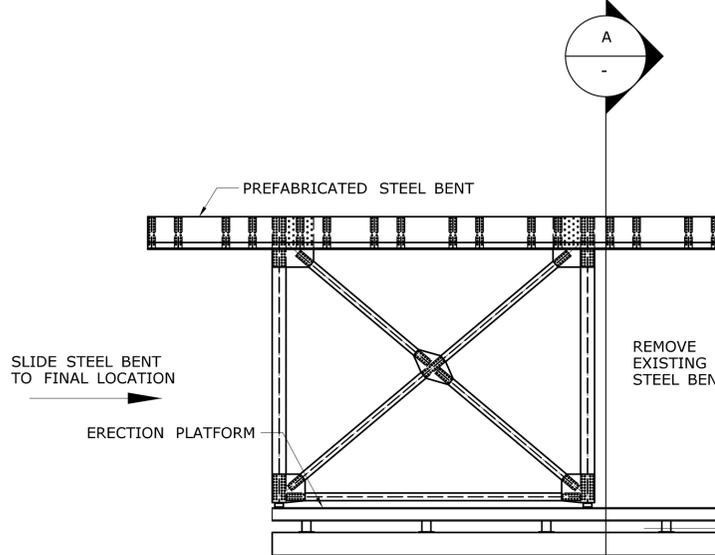
**SECTION A-A**  
SCALE: 1/8"=1'-0"

**90% PROGRESS SUBMISSION**

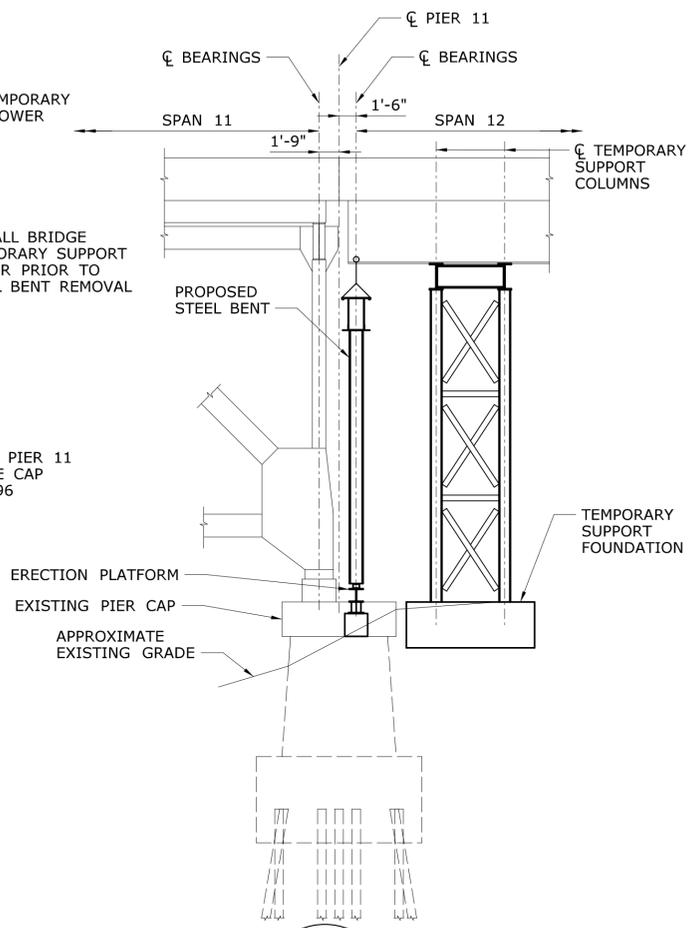
<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p>		<p>DESIGNER/DRAFTER: <b>AST/ALH</b></p> <p>CHECKED BY: <b>CC</b></p> <p>SCALE AS NOTED</p>	<p><b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION</p> <p>Filename: ...\\SB_MSH_Br00571A_126-170 STD8.dgn</p>	<p>SIGNATURE/BLOCK:</p>	<p>PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b></p>	<p>TOWN: <b>SHELTON DERBY</b></p>	<p>PROJECT NO. <b>126-170</b></p> <p>DRAWING NO. <b>S-22</b></p> <p>SHEET NO.</p>	
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<p>DRAWING TITLE: <b>CONSTRUCTION STAGE STEEL BENT AT PIER 8</b></p>			



**PIER 11 CONSTRUCTION STAGE 3 PLAN**  
SCALE: 1/8" = 1'-0"



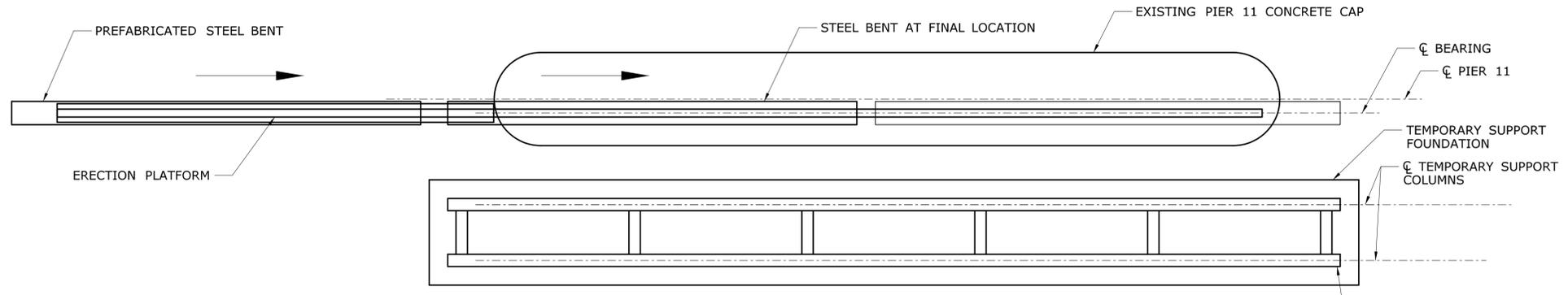
**PIER 11 ELEVATION LOOKING SOUTH**  
SCALE: 1/8" = 1'-0"



**SECTION A**  
SCALE: 1/8" = 1'-0"

**SUGGESTED CONSTRUCTION STAGE**

1. INSTALL BRIDGE TEMPORARY SUPPORT TOWER AND FOUNDATION.
2. DEMOLISH STEEL BENT AT PIER.
3. INSTALL ERECTION PLATFORM, PREFABRICATED WESTERN STEEL BENT, SLIDE STEEL BENT TO FINAL LOCATION.
4. PREFABRICATED EASTERN STEEL BENT, SLIDE STEEL BENT TO FINAL LOCATION.
5. REMOVE ERECTION PLATFORM AND INSTALL ALL BEARINGS.
6. REMOVE BRIDGE TEMPORARY SUPPORT TOWER AND FOUNDATION.

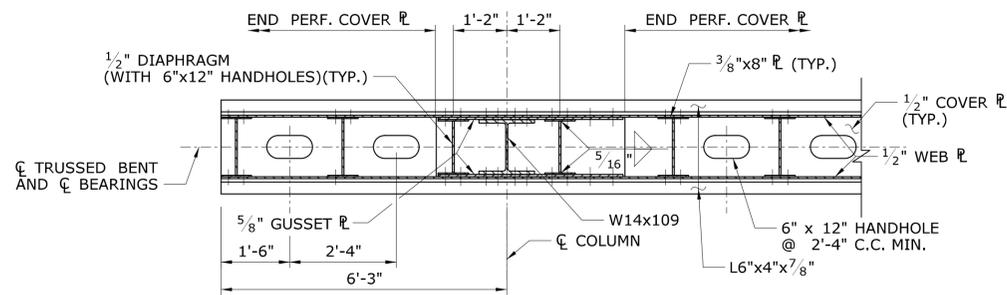


**PIER 11 CONSTRUCTION STAGE 4 PLAN**  
SCALE: 1/8" = 1'-0"

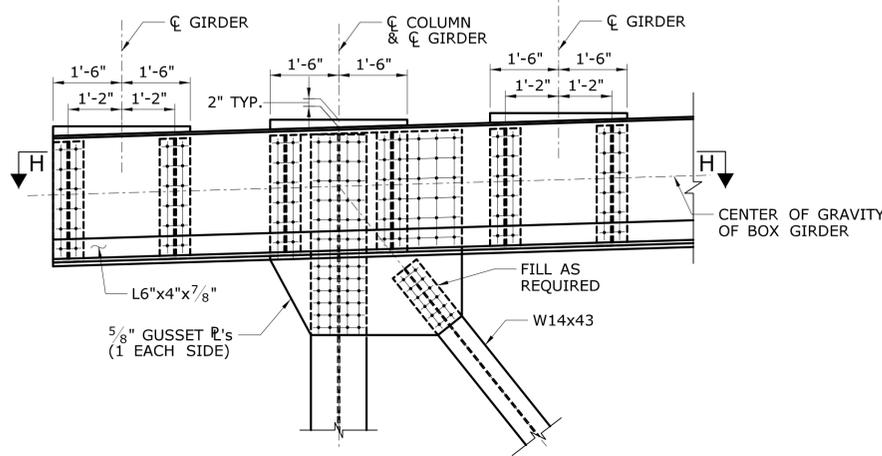
**NOTE:**  
REMOVE TIMBER FENCE AND PEDESTRIAN WALKWAY PRIOR TO CONSTRUCTION. RESTORE SITE TO EXISTING CONDITIONS ONCE CONSTRUCTION COMPLETE.

**90% PROGRESS SUBMISSION**

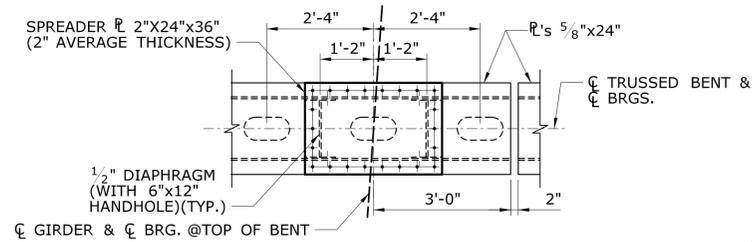
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>ALH</b> CHECKED BY: <b>MPE</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION Filename: ...SB_MSH_Br00571A_126-170_STD11.dgn	SIGNATURE/BLOCK:  PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-23</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>CONSTRUCTION STAGE STEEL BENT AT PIER 11</b>	



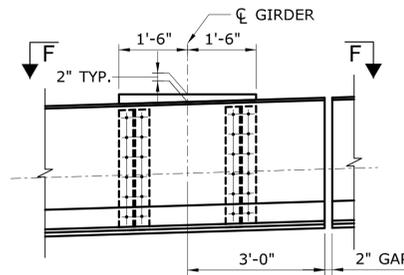
**SECTION H-H**  
SCALE: 1/2" = 1'-0"



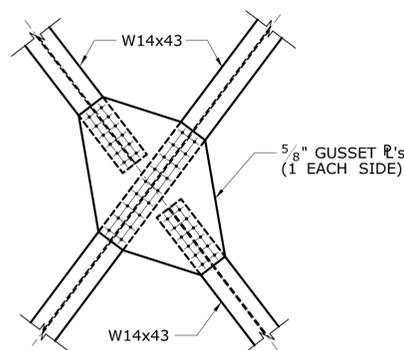
**DETAIL G**  
SCALE: 1/2" = 1'-0"



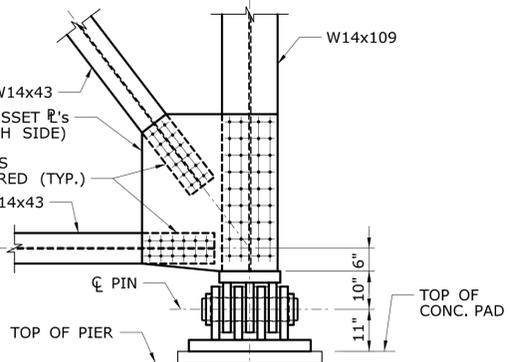
**PLAN VIEW F-F**  
SCALE: 1/2" = 1'-0"



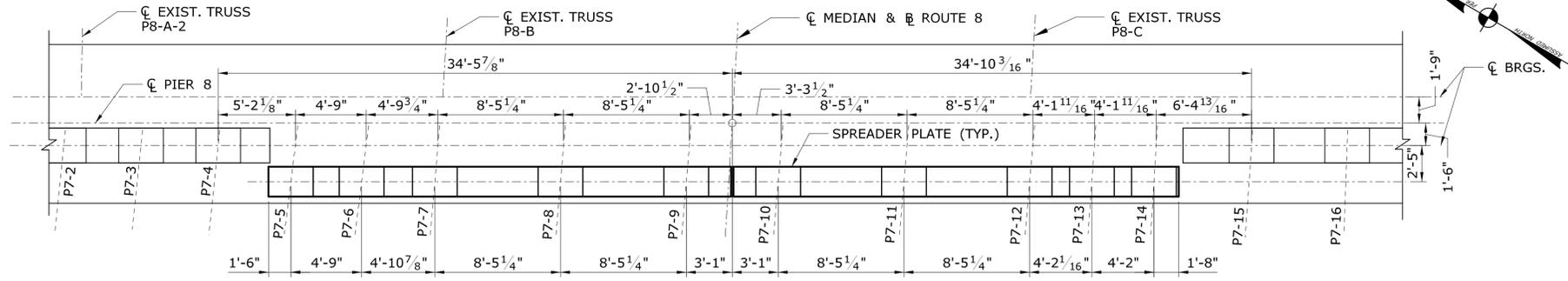
**DETAIL E**  
SCALE: 1/2" = 1'-0"



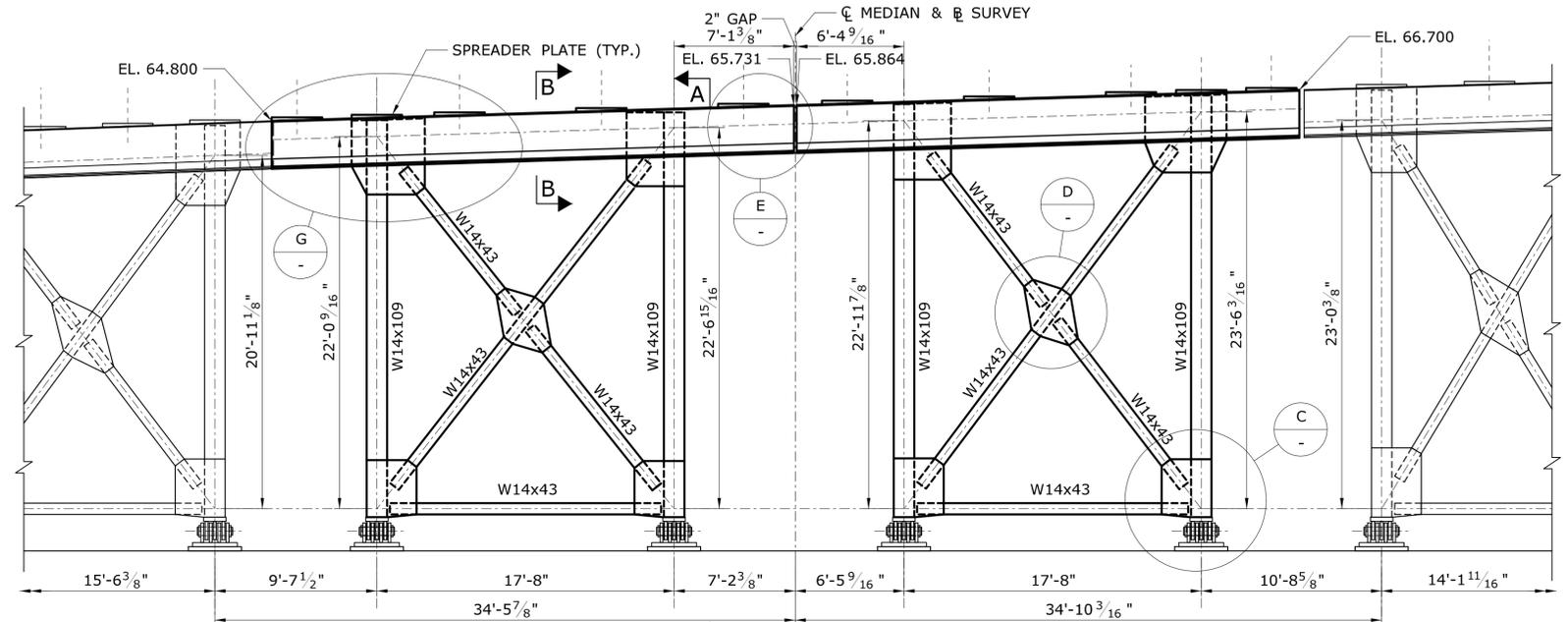
**DETAIL D**  
SCALE: 1/2" = 1'-0"



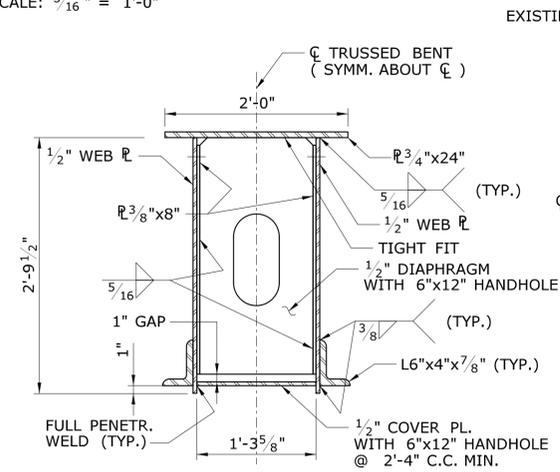
**DETAIL C**  
SCALE: 1/2" = 1'-0"



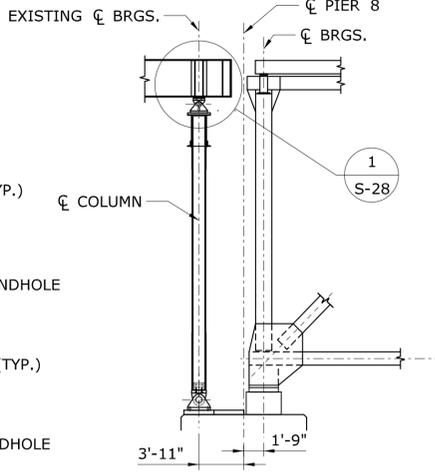
**PLAN**  
SCALE: 3/16" = 1'-0"



**ELEVATION A**  
SCALE: 3/16" = 1'-0"



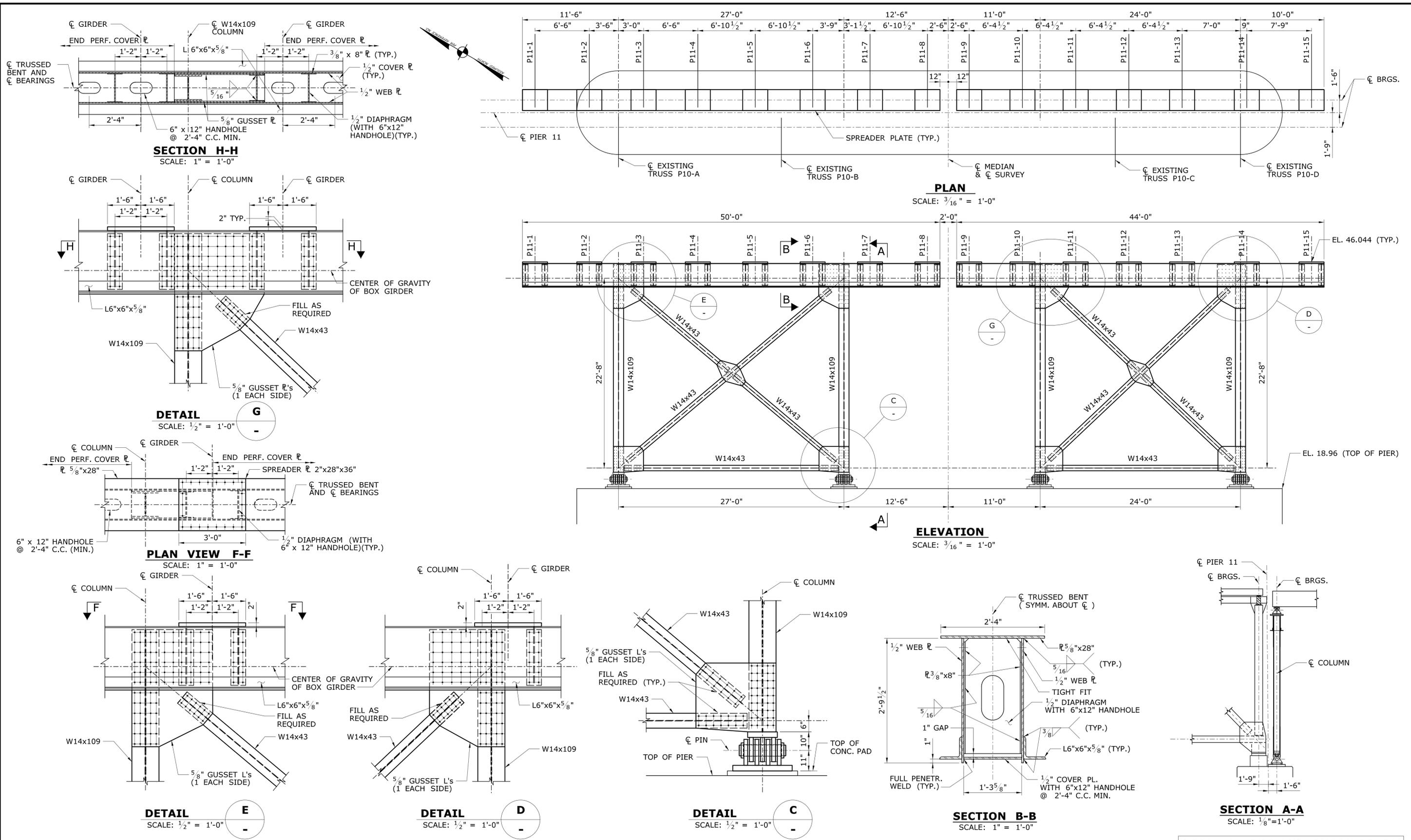
**SECTION B-B**  
SCALE: 1" = 1'-0"



**SECTION A-A**  
SCALE: 1/8" = 1'-0"

**90% PROGRESS SUBMISSION**

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>CAR/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION Filename: ...SB_MSH_Br00571A_126-170_STD6.dgn	SIGNATURE/BLOCK:  PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b> DRAWING TITLE: <b>REPLACEMENT OF STEEL BENTS AT PIER 8</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-24</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		



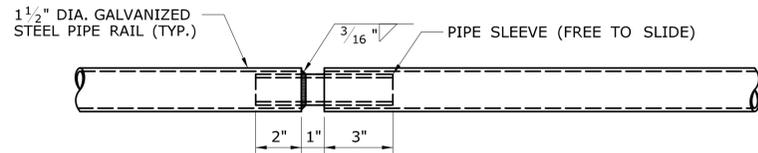
**90% PROGRESS SUBMISSION**

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...\\SB_MSH_Br00571A_126-170_STD7.dgn	SIGNATURE/BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE</b> <b>PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON</b> <b>DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-25</b> SHEET NO.
REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 10/1/2015	<b>REPLACEMENT OF STEEL BENTS AT PIER 11</b>						

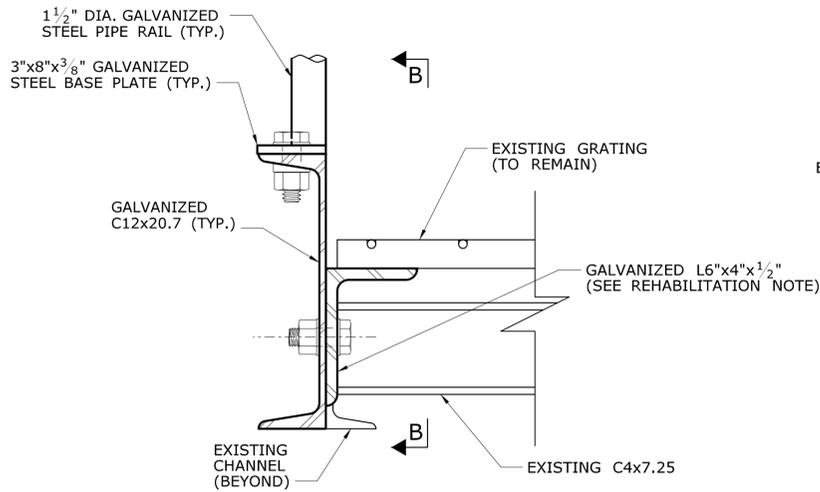


**NOTES:**

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS "WALKWAY MODIFICATIONS".

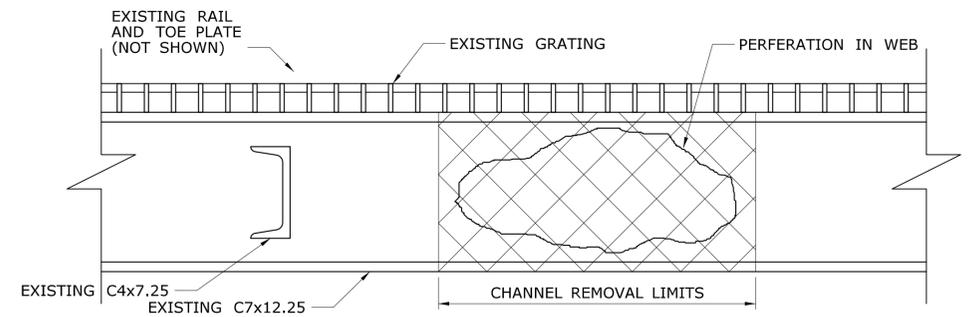


**PIPE RAIL EXPANSION JOINT DETAIL**  
SCALE: 3" = 1'-0"

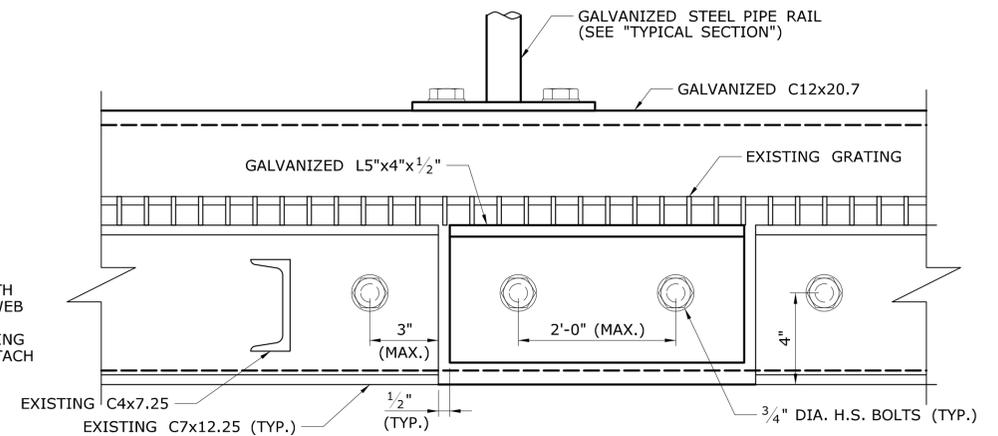


**TYPICAL REHABILITATION SECTION**  
SCALE: 3" = 1'-0"

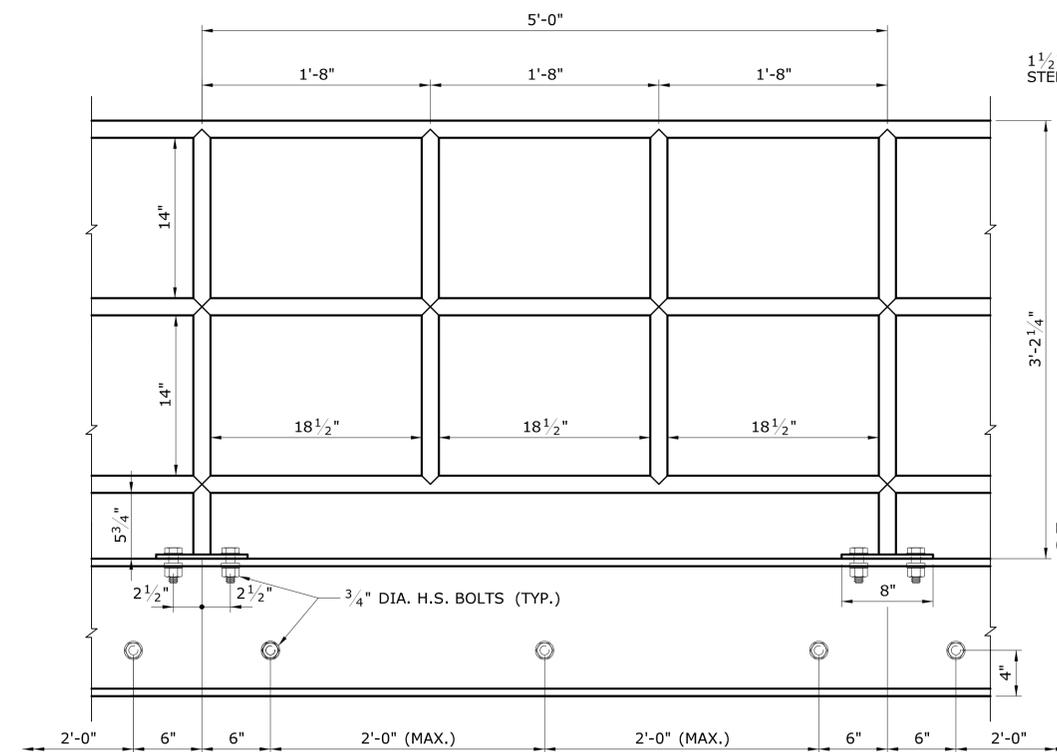
**REHABILITATION NOTE:**  
REMOVE EXISTING C7x12.25 IN LOCATIONS WITH SEVERE SECTION LOSS OR PERFORATIONS IN WEB AS DETERMINED BY ENGINEER. TEMPORALLY SUPPORT C4x7.25 BRACING AND GRATING DURING MEMBER REPLACEMENT. WHERE REQUIRED, REATTACH C4x7.25 TO PROPOSED ANGLE WITH 1/2" CONNECTION PLATE WELDED TO ANGLE.



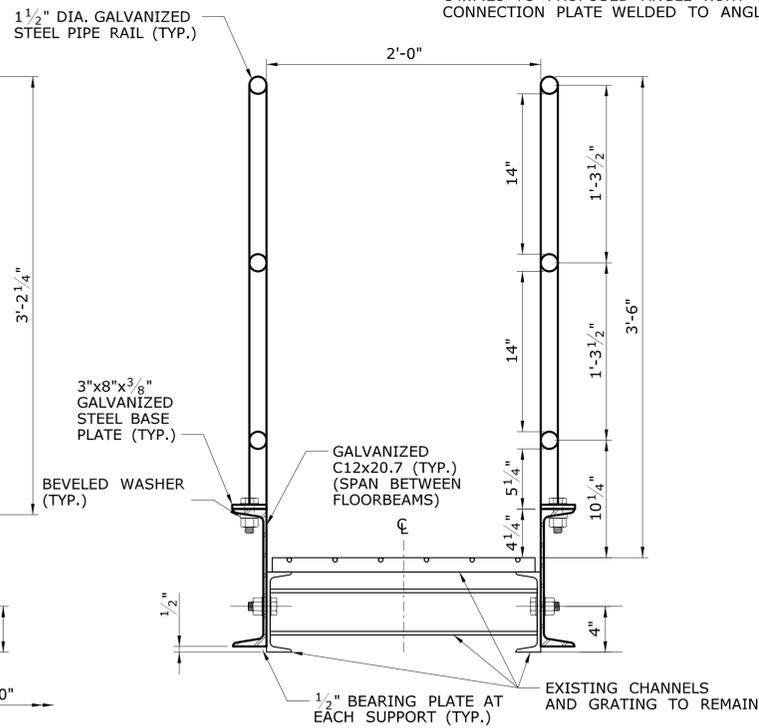
**EXISTING ELEVATION B-B**  
SCALE: 3" = 1'-0"



**REHABILITATION ELEVATION B-B**  
SCALE: 3" = 1'-0"

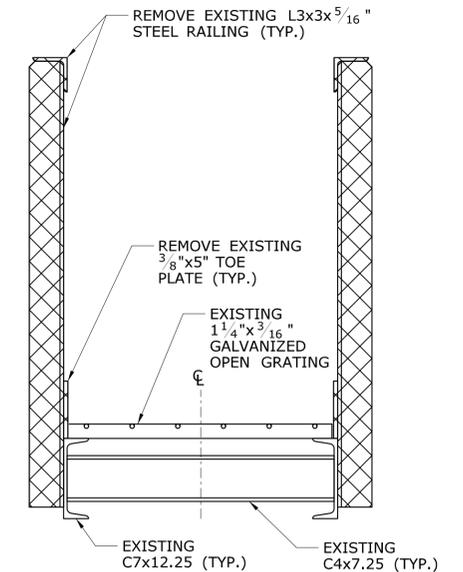


**ELEVATION**



**TYPICAL SECTION**

**LEGEND**  
[Cross-hatched symbol] : REMOVAL LIMITS FOR EXISTING STEEL



**DEMOLITION SECTION**  
**EXISTING INSPECTION WALKWAY**  
SCALE: 1 1/2" = 1'-0"

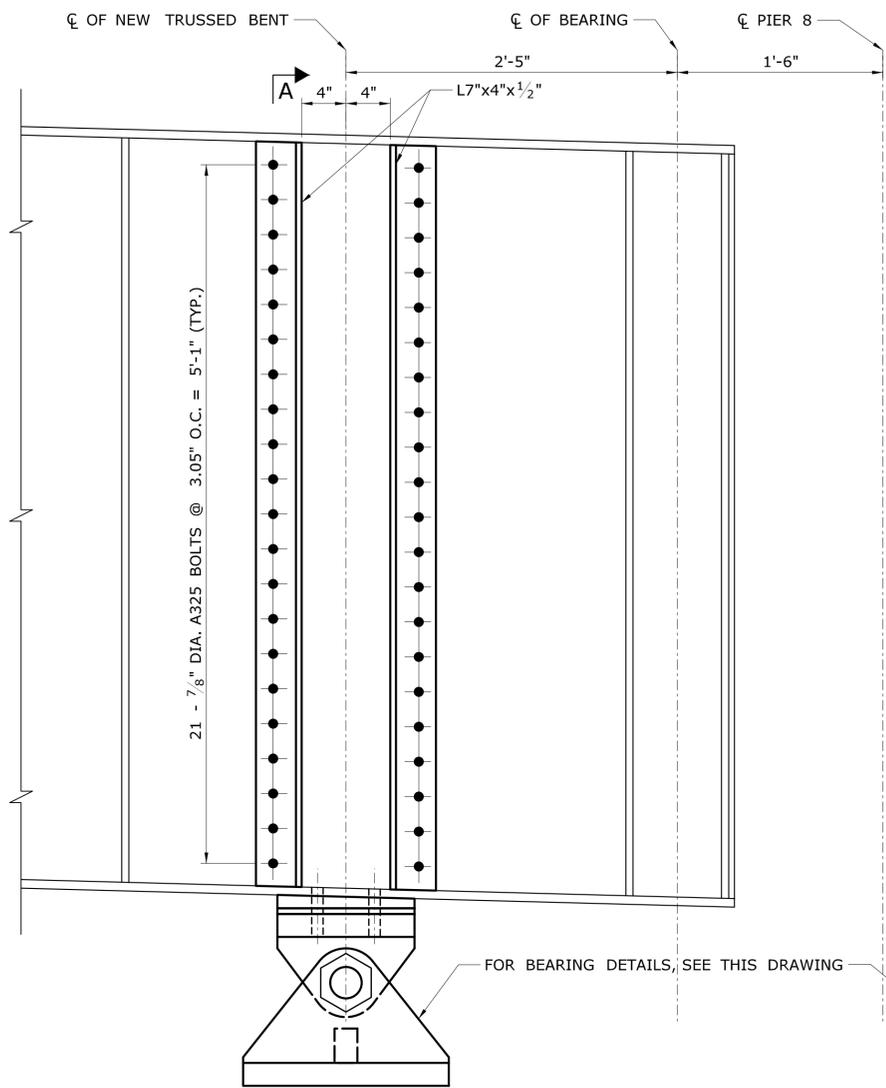
**NOTE:**  
ADJUST BOLT SPACING AS REQUIRED TO ATTACH TO COMPETENT WEB OF EXISTING C7x12.25. SEE "TYPICAL REHABILITATION SECTION" FOR LOCATIONS WITHOUT COMPETENT WEB.

**REPAIR WORK AT EXISTING INSPECTION WALKWAY DETAILS**

SCALE: 1 1/2" = 1'-0"

**90% PROGRESS SUBMISSION**

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	CHECKED BY: <b>CC</b>					
REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 10/1/2015	Filename: ...SB_MSH_Br00571A_126-170_STD10.dgn		DRAWING TITLE: <b>INSPECTION WALKWAY MODIFICATION DETAILS</b>		SHEET NO.	SHEET NO.

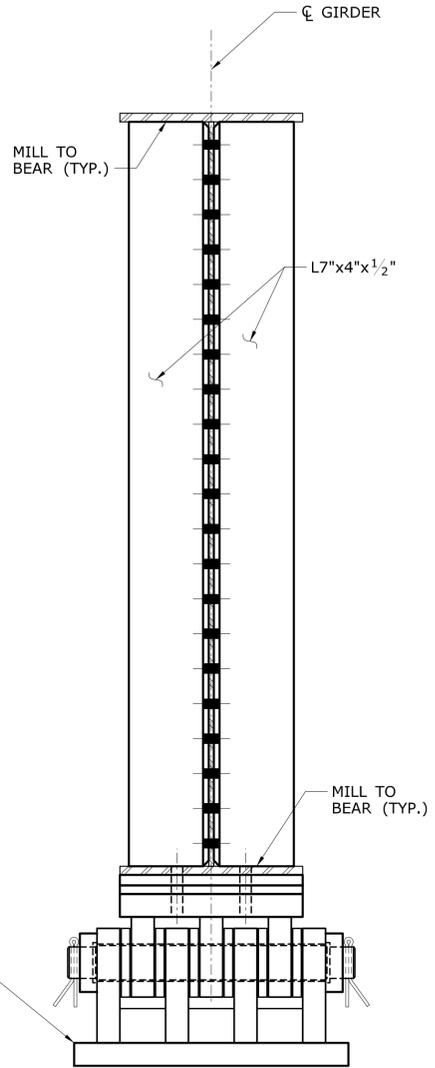


**ELEVATION**

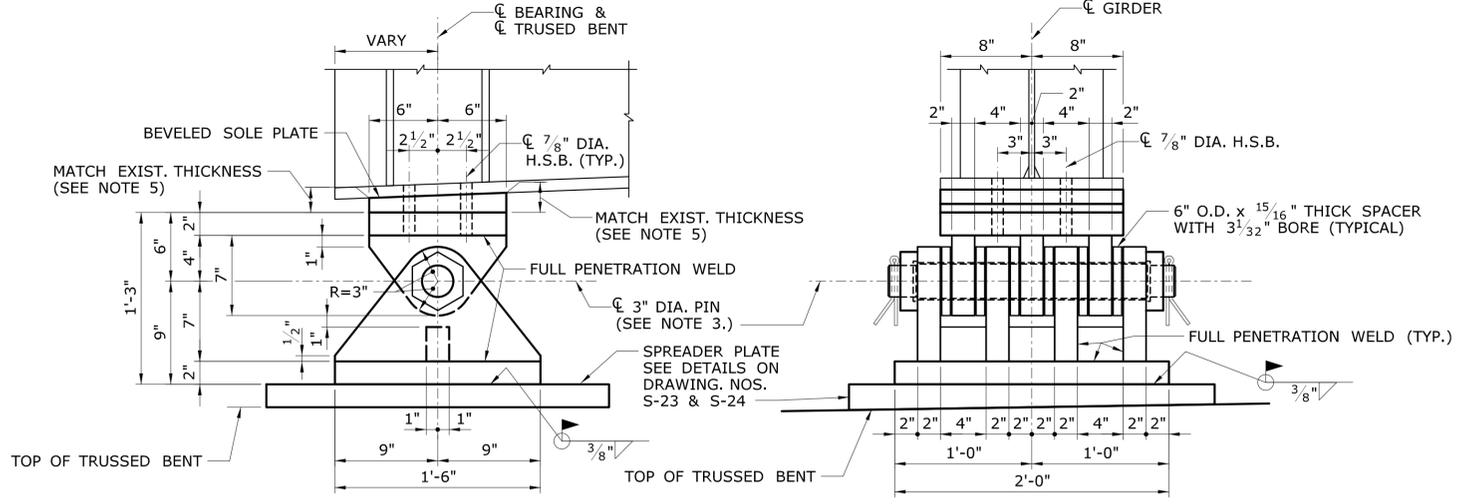
**1**  
**S-24**

**BEARING STIFFENERS FOR REPLACEMENT BENTS AT PIER 8**

SCALE: 1 1/2" = 1'-0"

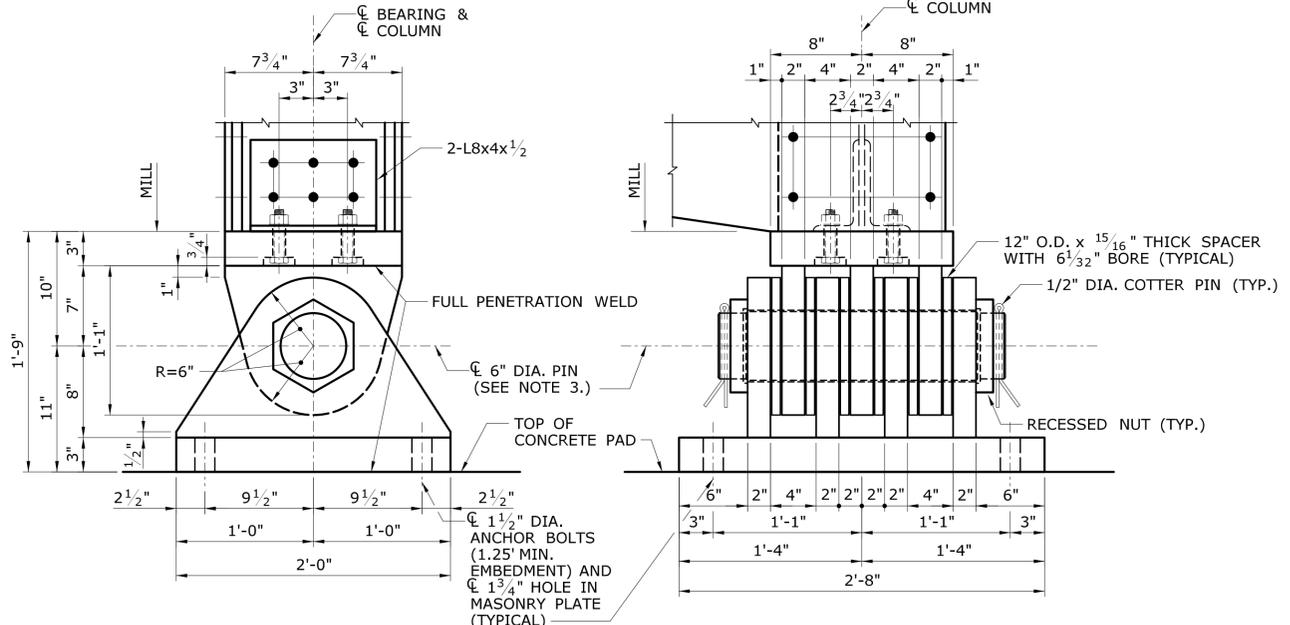


**SECTION A-A**



**EXPANSION STEEL BEARING SHOE-E2  
TOP OF TRUSSED BENT AT PIERS 8 & 11**

SCALE: 1 1/2" = 1'-0"



**EXPANSION STEEL BEARING SHOE-E3  
BOTTOM OF TRUSSED BENT AT PIERS 8 & 11**

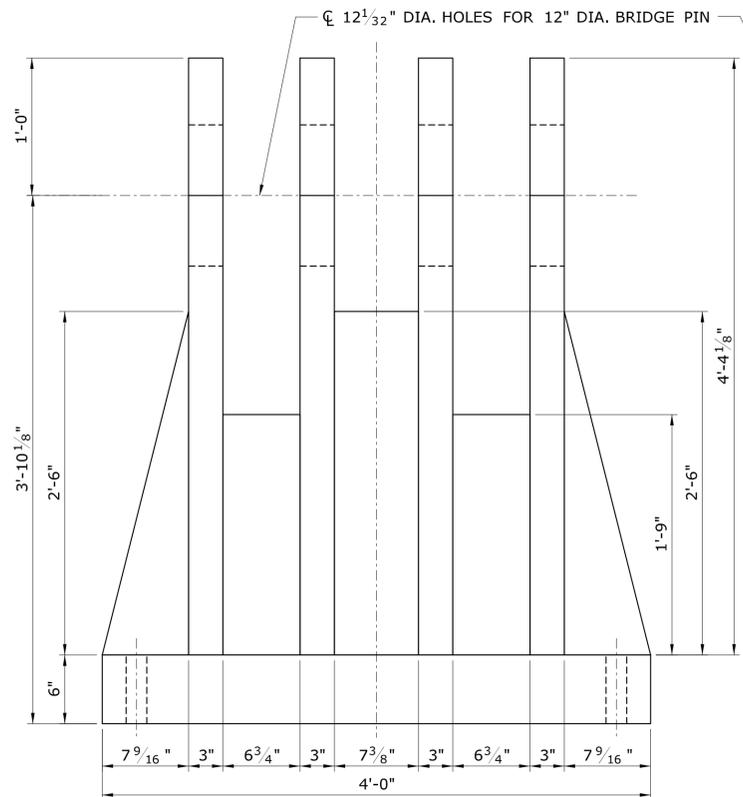
SCALE: 1 1/2" = 1'-0"

**BEARING NOTES:**

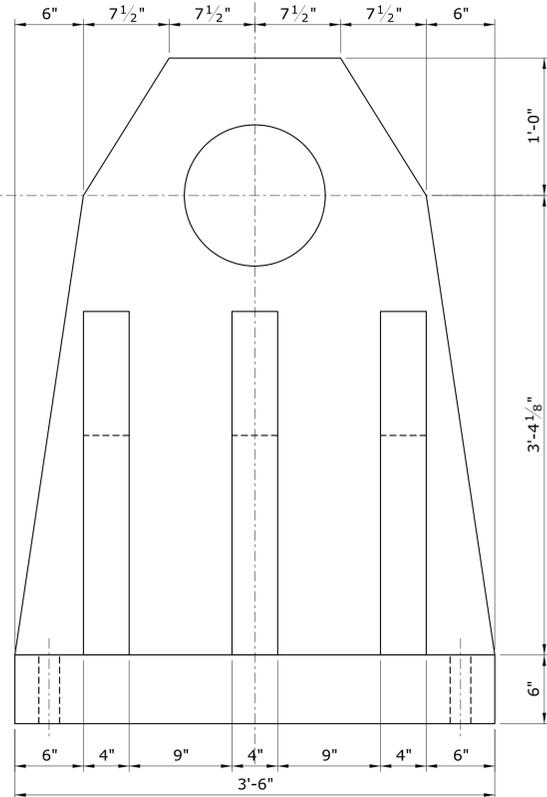
1. FURNISHING AND INSTALLING STEEL BEARING SHALL BE PAID FOR UNDER THE ITEM "STRUCTURAL STEEL (SITE NO. 8)" AND "STRUCTURAL STEEL (SITE NO. 11)".
2. ALL STEEL PLATE SHALL CONFORM TO AASHTO M270, GRADE 50 T2.
3. PIN MATERIAL SHALL CONFORM TO ASTM A-668 CLASS F AND SHALL HAVE A FINISHED SURFACE OF AN ANSI STANDARD FINISH OF 125.
4. WELDING DETAILS, PROCEDURES AND TESTING METHODS SHALL CONFORM TO THE AASHTO/AWS D1.5 BRIDGE WELDING CODE, UNLESS OTHERWISE NOTED ON THE PLANS.
4. CONTRACTOR SHALL GET FIELD MEASUREMENT PRIOR TO BEARING FABRICATION.

**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		



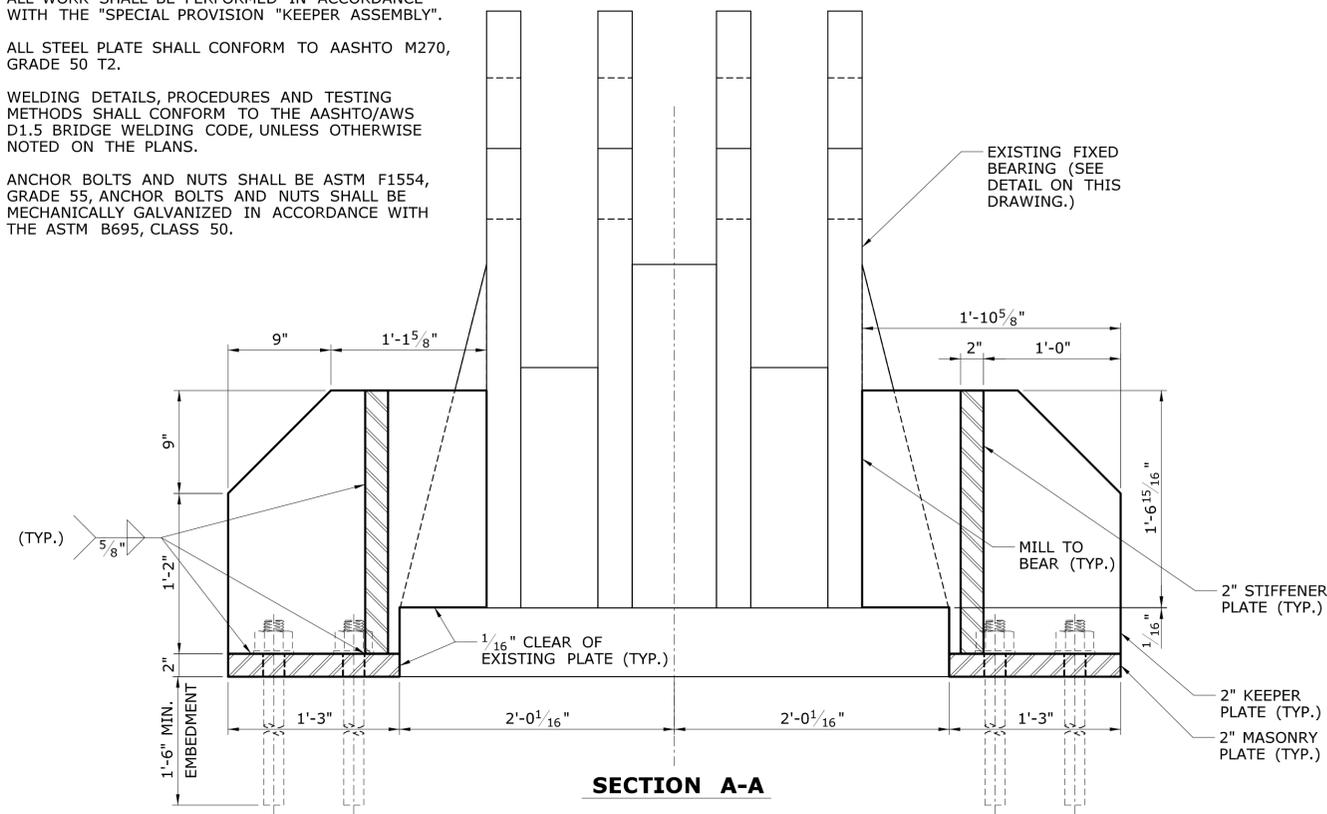
**FRONT VIEW**



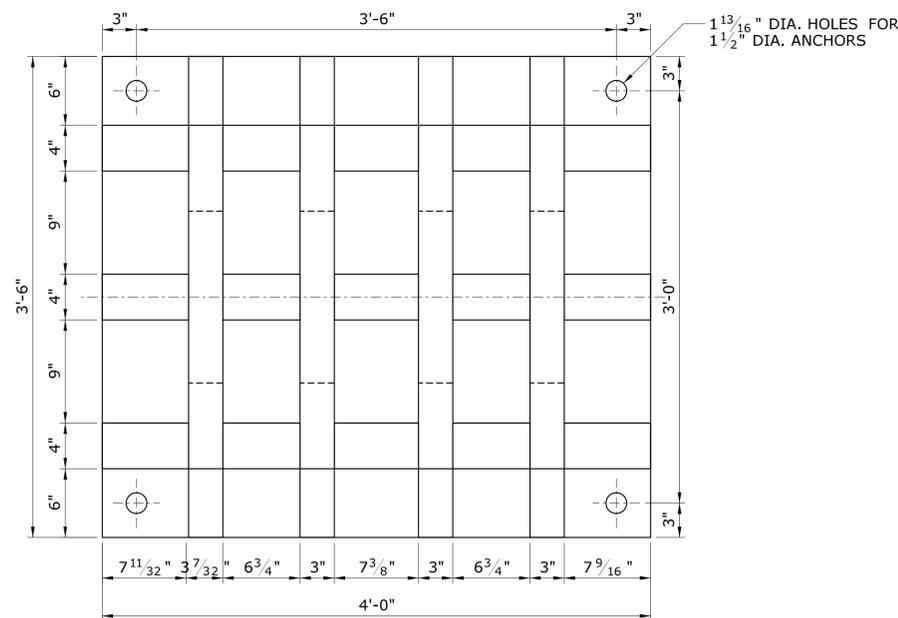
**SIDE VIEW**

**NOTES:**

1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE "SPECIAL PROVISION "KEEPER ASSEMBLY".
2. ALL STEEL PLATE SHALL CONFORM TO AASHTO M270, GRADE 50 T2.
3. WELDING DETAILS, PROCEDURES AND TESTING METHODS SHALL CONFORM TO THE AASHTO/AWS D1.5 BRIDGE WELDING CODE, UNLESS OTHERWISE NOTED ON THE PLANS.
4. ANCHOR BOLTS AND NUTS SHALL BE ASTM F1554, GRADE 55, ANCHOR BOLTS AND NUTS SHALL BE MECHANICALLY GALVANIZED IN ACCORDANCE WITH THE ASTM B695, CLASS 50.



**SECTION A-A**

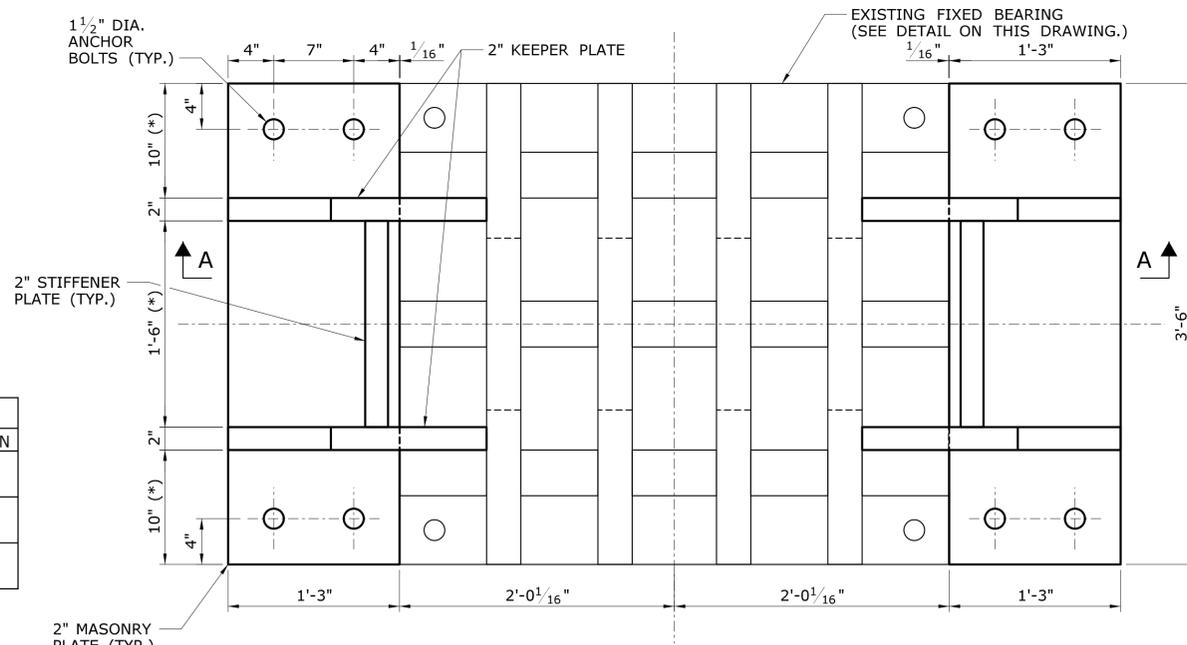


**PLAN VIEW**

**EXISTING FIXED BEARING**

SCALE: 1 1/2" = 1'-0"

BEARING LOCATIONS		
SPAN	TRUSS	LOCATION
9	P8-B	PIER 9
9	P8-C	
10	P9-B	PIER 10
10	P9-C	
11	P10-B	PIER 11
11	P10-C	



**PLAN VIEW**

**FIXED BEARING RESTRAINERS**

SCALE: 1 1/2" = 1'-0"

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015

DESIGNER/DRAFTER: **KAB/ALH**  
 CHECKED BY: **CC**  
 SCALE AS NOTED

**STATE OF CONNECTICUT**  
**DEPARTMENT OF TRANSPORTATION**

Filename: ...SB\_MSH\_Br00571A\_126-170\_BRS1.dgn

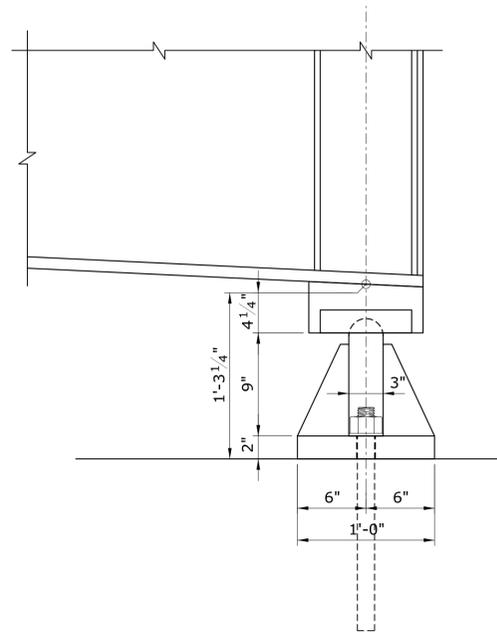
SIGNATURE/BLOCK:

PROJECT TITLE:  
**COMMODORE HULL BRIDGE**  
**PAINTING AND STEEL REPAIRS**

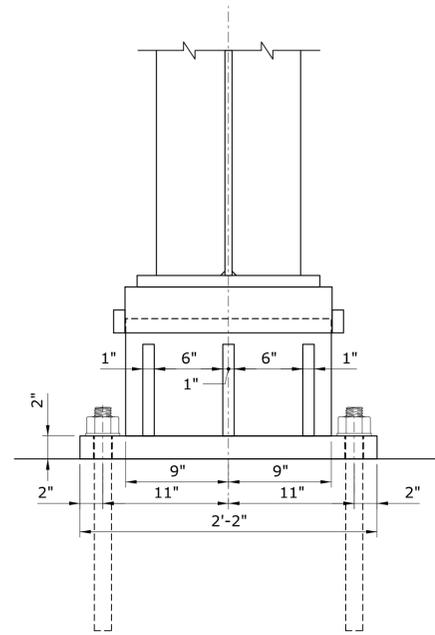
TOWN: **SHELTON**  
**DERBY**

DRAWING TITLE:  
**FIXED BEARING RESTRAINTS**  
**AT TRUSS SPANS**

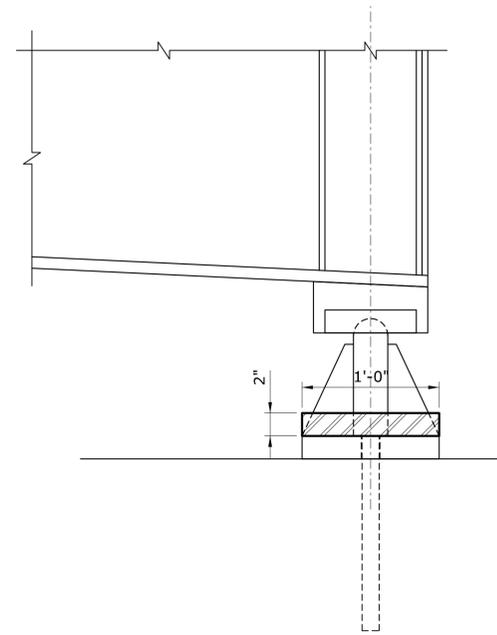
PROJECT NO. **126-170**  
 DRAWING NO. **S-29**  
 SHEET NO.



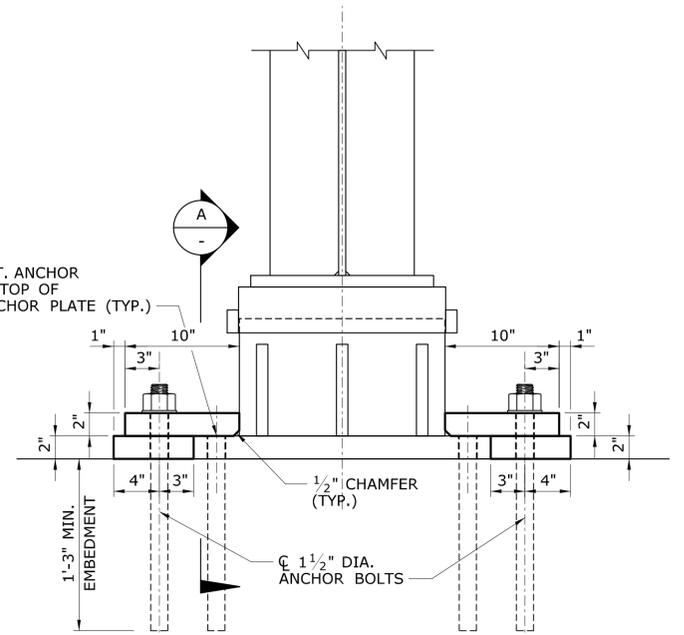
**SIDE VIEW**



**END ELEVATION**



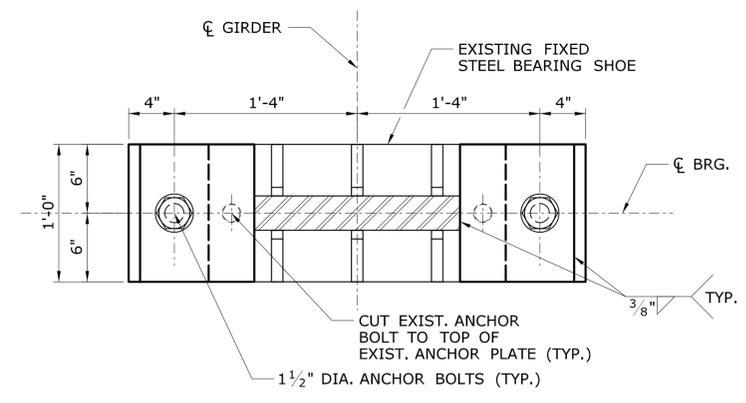
**SECTION A**



**END ELEVATION**

**EXISTING FIXED STEEL BEARING SHOE DETAILS**

SCALE: 1 1/2" = 1'-0"



**PLAN**

BEARING LOCATIONS			
SPAN	GIRDERS	QUANTITY	LOCATION
1	6	1	PIER 1
3	3-11	9	PIER 2
5	3-11	9	PIER 5
6	6 & 7	2	PIER 6
7	7 & 8	2	PIER 7
8	5, 7, 9 & 11	4	PIER 7
12	4-10	7	ABUT. 2

**FIXED BEARING RESTRAINER DETAILS**

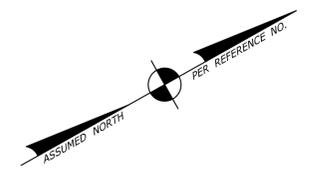
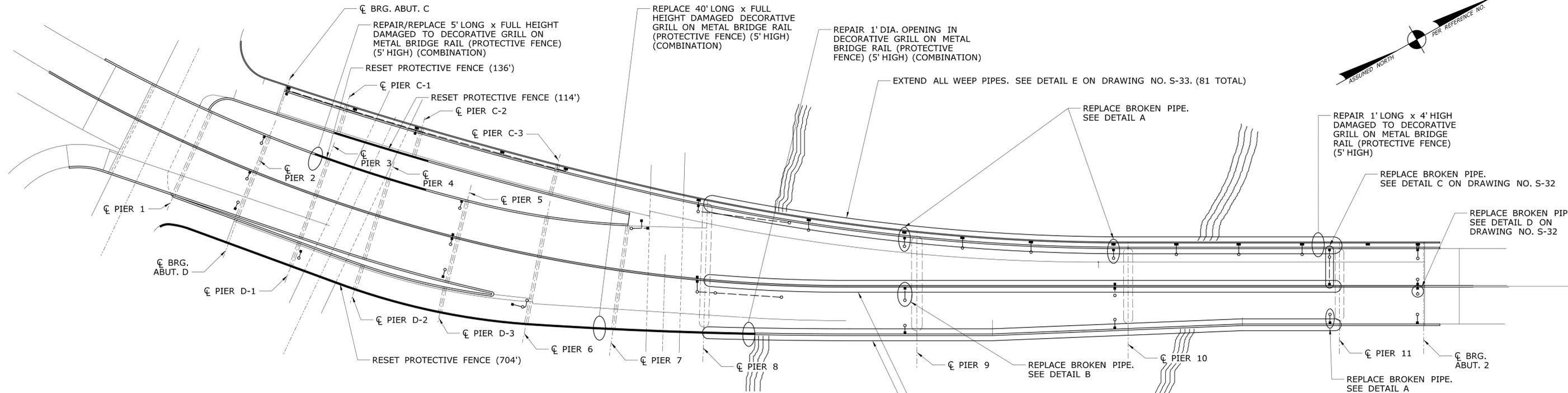
SCALE: 1 1/2" = 1'-0"

**NOTES:**

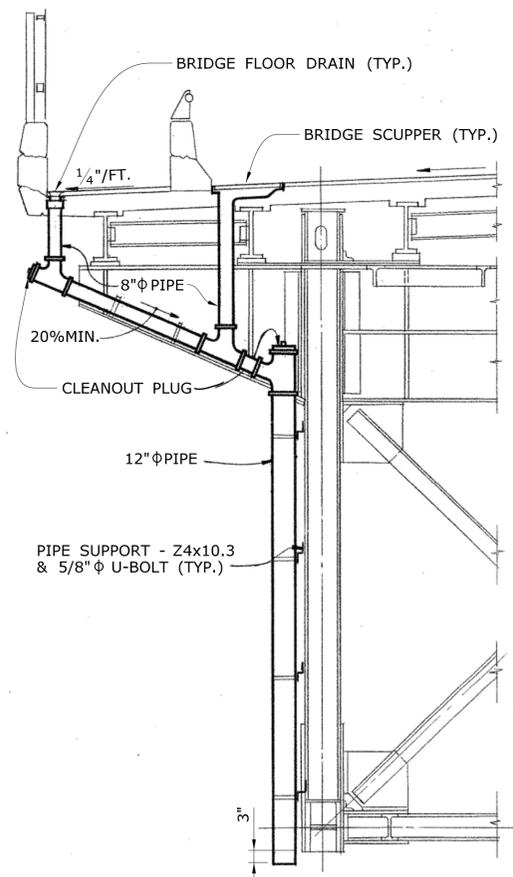
1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE "SPECIAL PROVISION 'KEEPER ASSEMBLY'".
2. ALL STEEL PLATE SHALL CONFORM TO AASHTO M270, GRADE 50 T2.
3. ANCHOR BOLTS AND NUTS SHALL BE ASTM F1554, GRADE 55, ANCHOR BOLTS AND NUTS SHALL BE MECHANICALLY GALVANIZED IN ACCORDANCE WITH THE ASTM B695, CLASS 50.

**90% PROGRESS SUBMISSION**

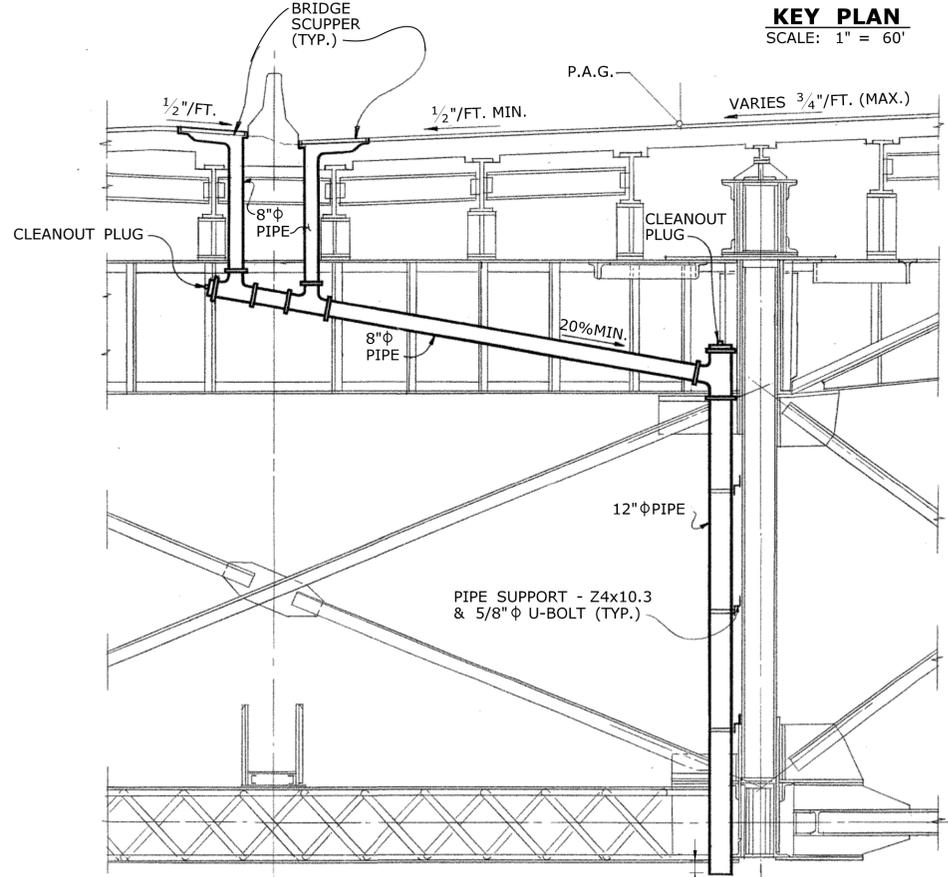
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...SB_MSH_Br00571A_126-170_BRS3.dgn	SIGNATURE/BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-30</b> SHEET NO.	
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>FIXED BEARING RESTRAINTS AT APPROACH SPANS</b>			



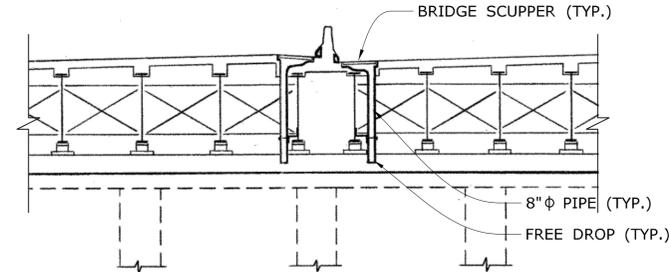
**KEY PLAN**  
SCALE: 1" = 60'



**DETAIL A**  
SCALE: 1/4" = 1'-0"



**DETAIL B**  
SCALE: 1/4" = 1'-0"



**DETAIL D**  
SCALE: 1/8" = 1'-0"

**LEGEND**

- FLOOR DRAIN
- SCUPPER - UNBLOCKING/CLEANING (30 TOTAL)
- DOWNSPOUT
- PIPE

**NOTES:**

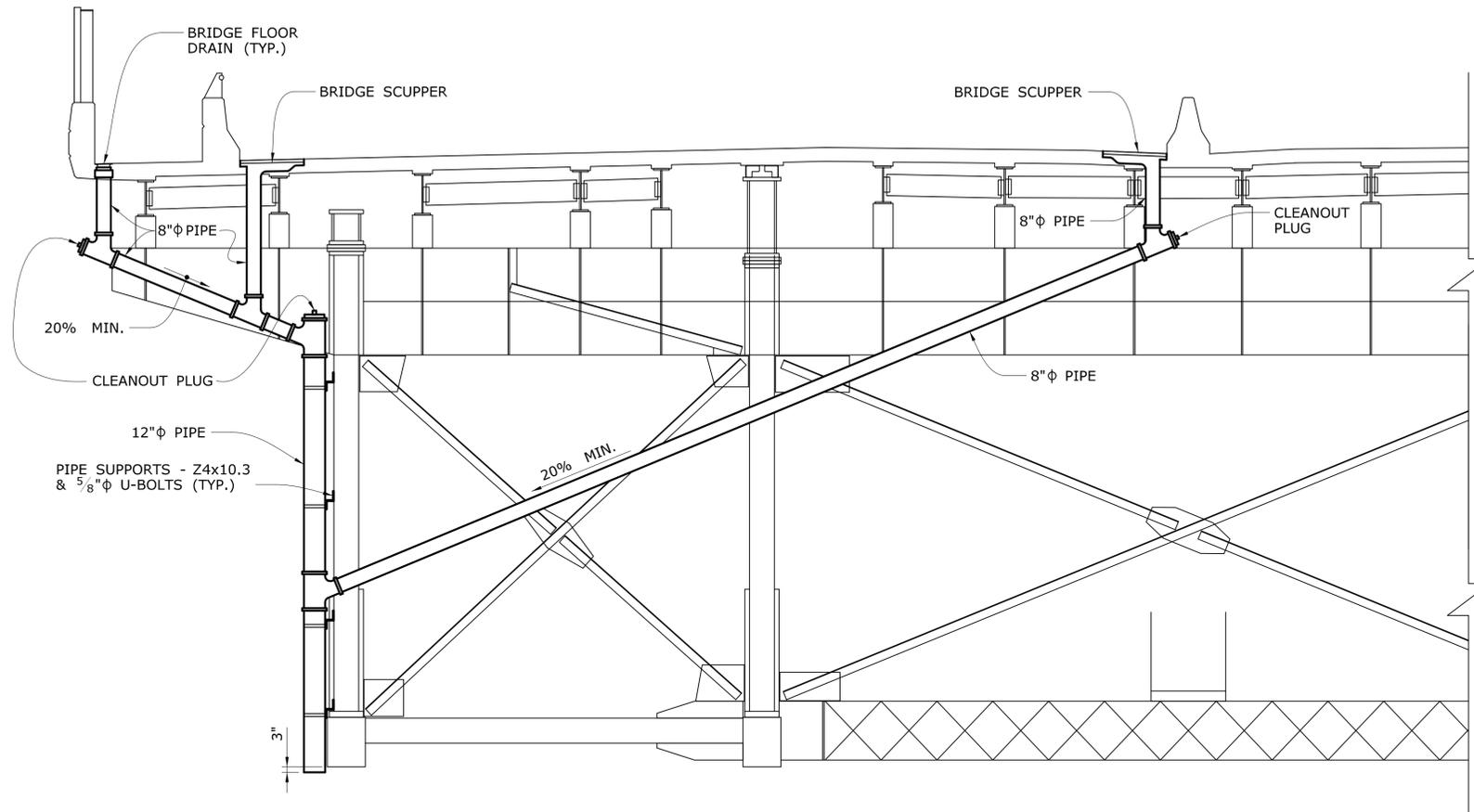
1. SCUPPER UNBLOCKING AND CLEANING WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "CLEAN EXISTING SCUPPERS".
2. REPLACE BROKEN DRAIN PIPE WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "REMOVAL AND REPLACEMENT OF EXISTING BROKEN DRAINAGE SYSTEM".
3. EXTEND WEEP PIPES WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISION "EXTEND EXISTING WEEPHOLES"
4. DETAIL A, B & D ARE TYPICAL DRAINAGE SYSTEM DETAILS. ONLY BROKEN PIPE SHALL BE REPLACED.

**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		

**NOTES:**

1. SEE DRAWING NO. S-31 FOR LOCATION OF DETAIL C AND DESCRIPTION OF WORK.
2. DETAIL C IS TYPICAL DRAINAGE SYSTEM DETAILS. ONLY BROKEN PIPE SHALL BE REPLACED.



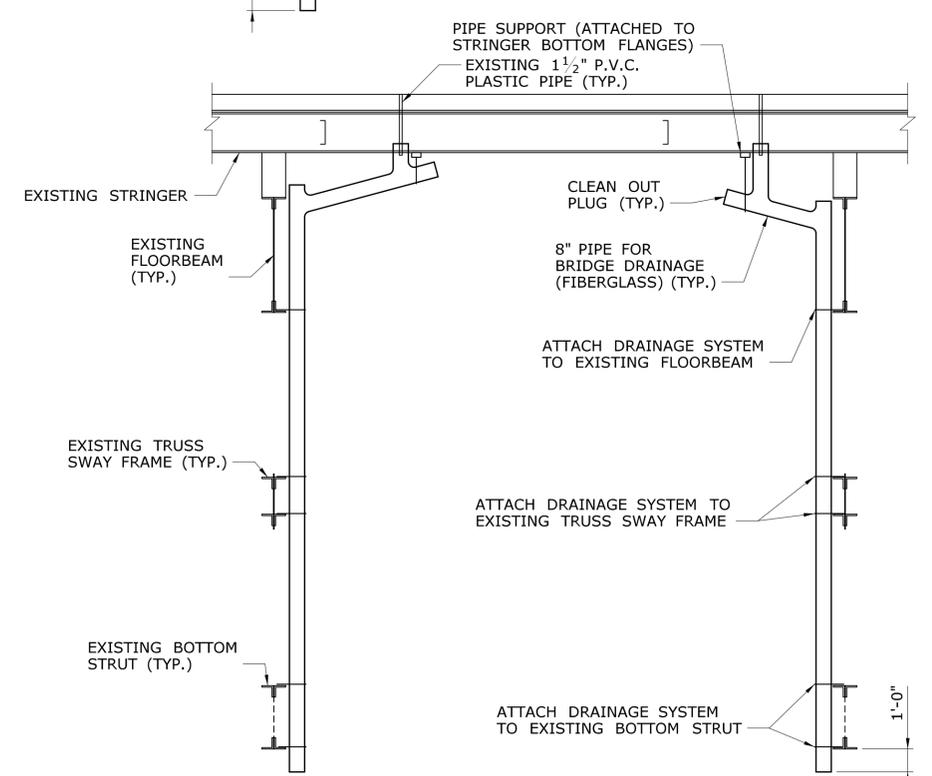
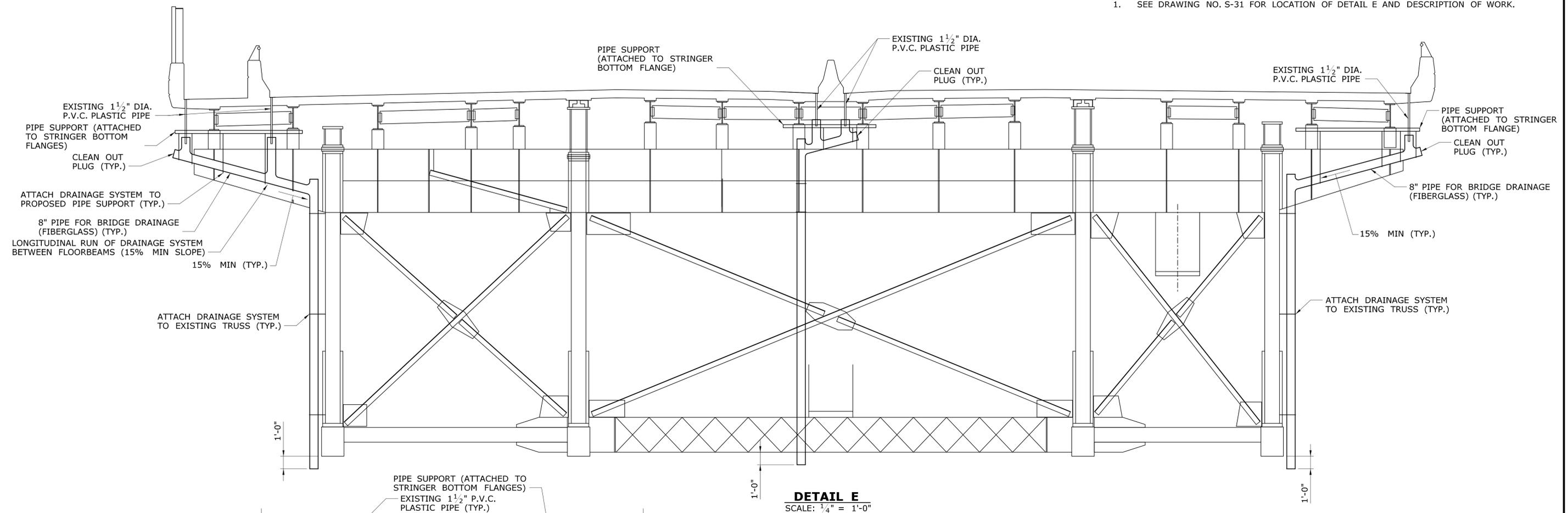
**DETAIL C**  
SCALE: 1/4" = 1'-0"

**90% PROGRESS SUBMISSION**

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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015				

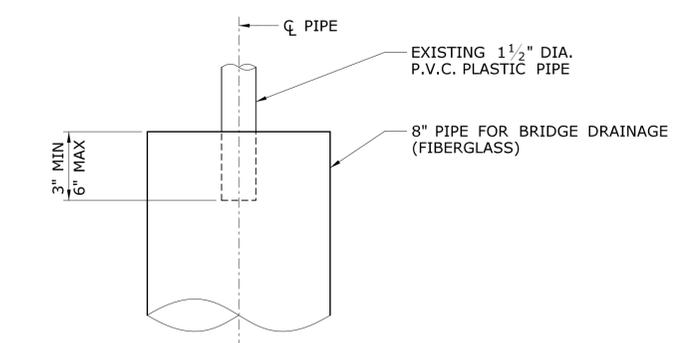
**NOTES:**

1. SEE DRAWING NO. S-31 FOR LOCATION OF DETAIL E AND DESCRIPTION OF WORK.



**LONGITUDINAL SECTION**  
SCALE: 1/4" = 1'-0"

**DETAIL E**  
SCALE: 1/4" = 1'-0"



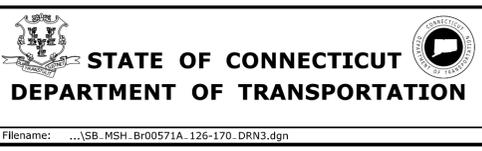
**TOP OF PIPE DETAIL**  
SCALE: 1/4" = 1'-0"

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER:  
**KAB/ALH**  
CHECKED BY:  
**CC**  
SCALE AS NOTED



SIGNATURE/  
BLOCK:

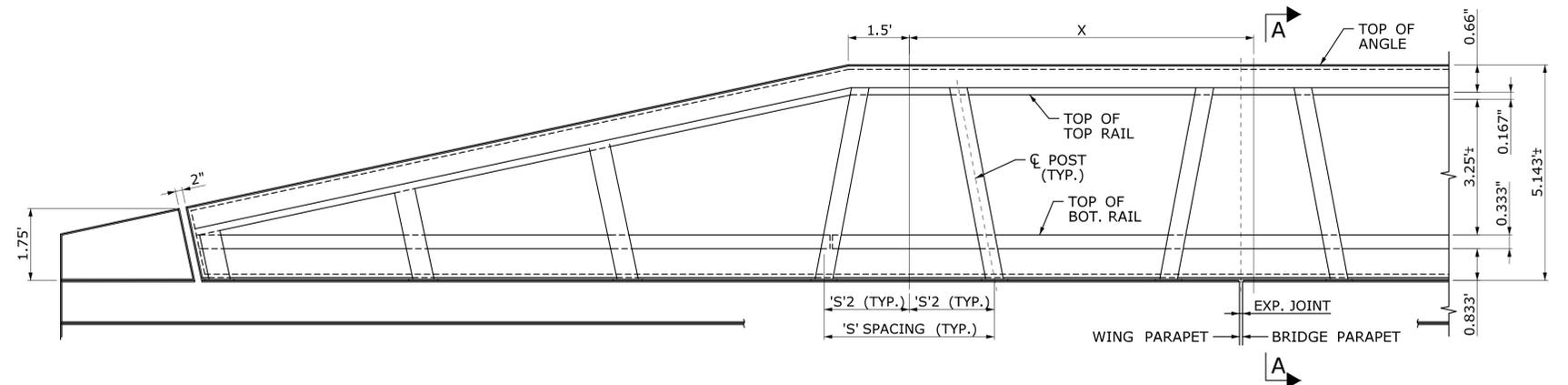
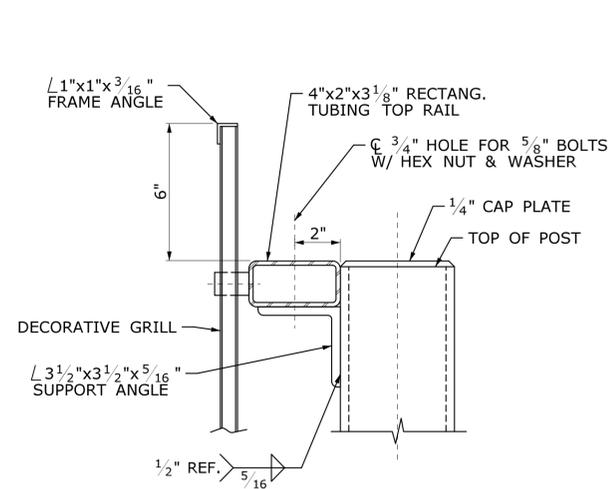
PROJECT TITLE:  
**COMMODORE HULL BRIDGE  
PAINTING AND STEEL REPAIRS**

TOWN:  
**SHELTON  
DERBY**  
DRAWING TITLE:  
**EXTEND EXISTING  
WEEPHOLE DETAILS**

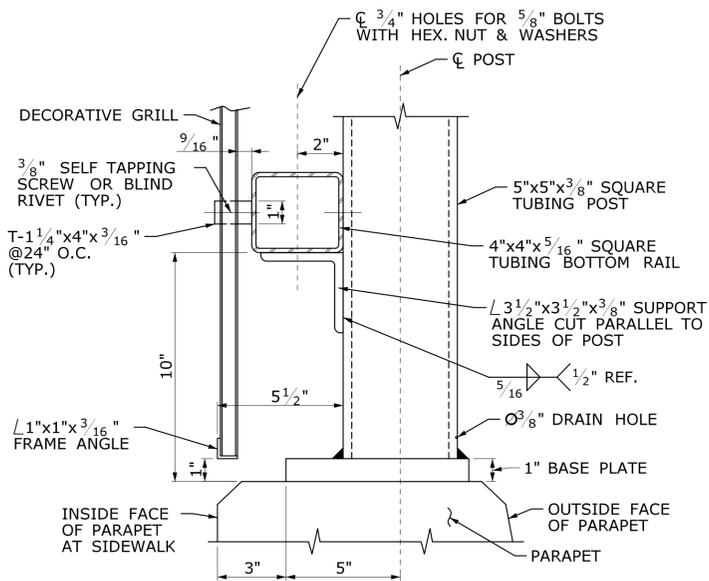
PROJECT NO.  
**126-170**  
DRAWING NO.  
**S-33**  
SHEET NO.

Plotted Date: 10/1/2015

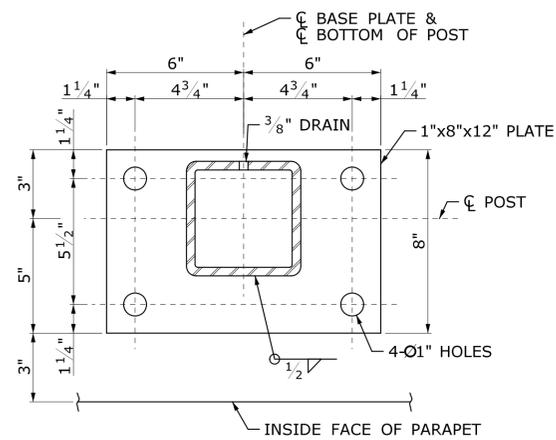
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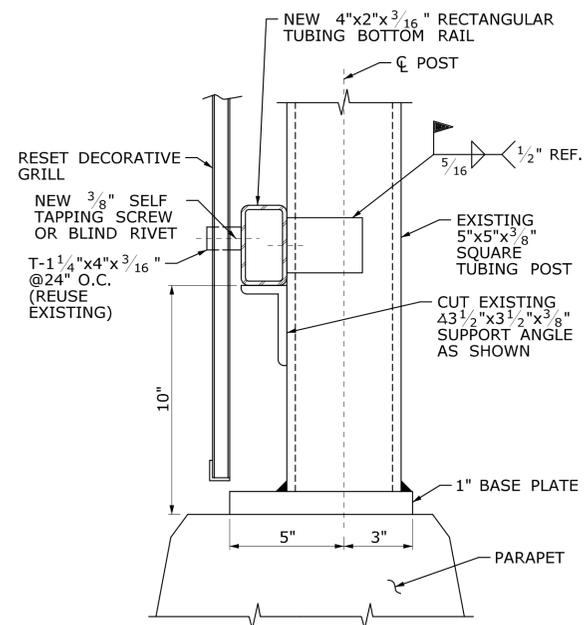
**TYPICAL EXISTING ELEVATION LOOKING AT OUTSIDE OF PARAPET**  
SCALE: 1/2"=1'-0"



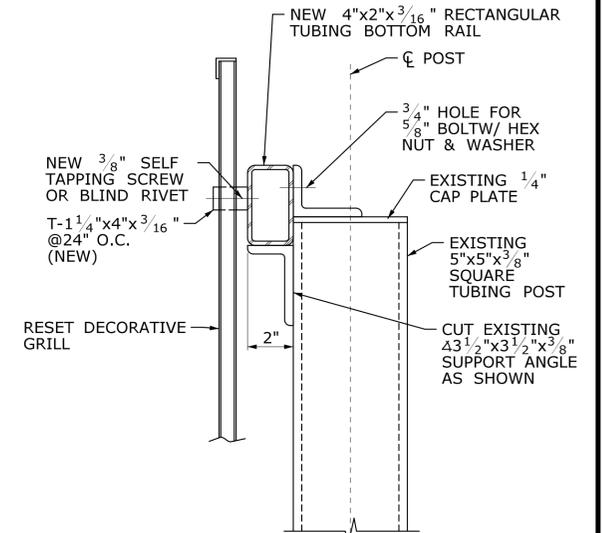
**SECTION A-A**  
SCALE: 3"=1'-0"



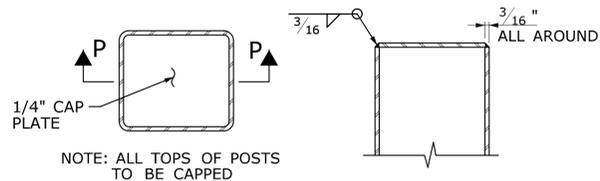
**BASE PLATE**  
SCALE: 3"=1'-0"



**RESET SECTION A-A**  
SCALE: 3"=1'-0"



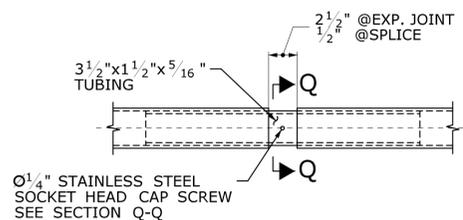
**RESET SECTION A-A**  
SCALE: 3"=1'-0"



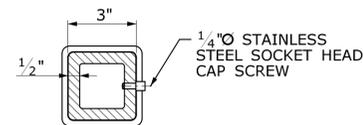
**CAP DETAIL**

**SECTION P-P**

**POST DETAILS**  
SCALE: 3"=1'-0"



**TOP & BOTTOM RAIL SPLICE**  
SCALE: 1 1/2"=1'-0"



**SECTION Q-Q**  
SCALE: 3"=1'-0"

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015

DESIGNER/DRAFTER: **KAB/ALH**  
CHECKED BY: **CC**  
SCALE AS NOTED

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

File name: ...SB\_MSH\_Br00571A\_126-170\_RLD1.dgn

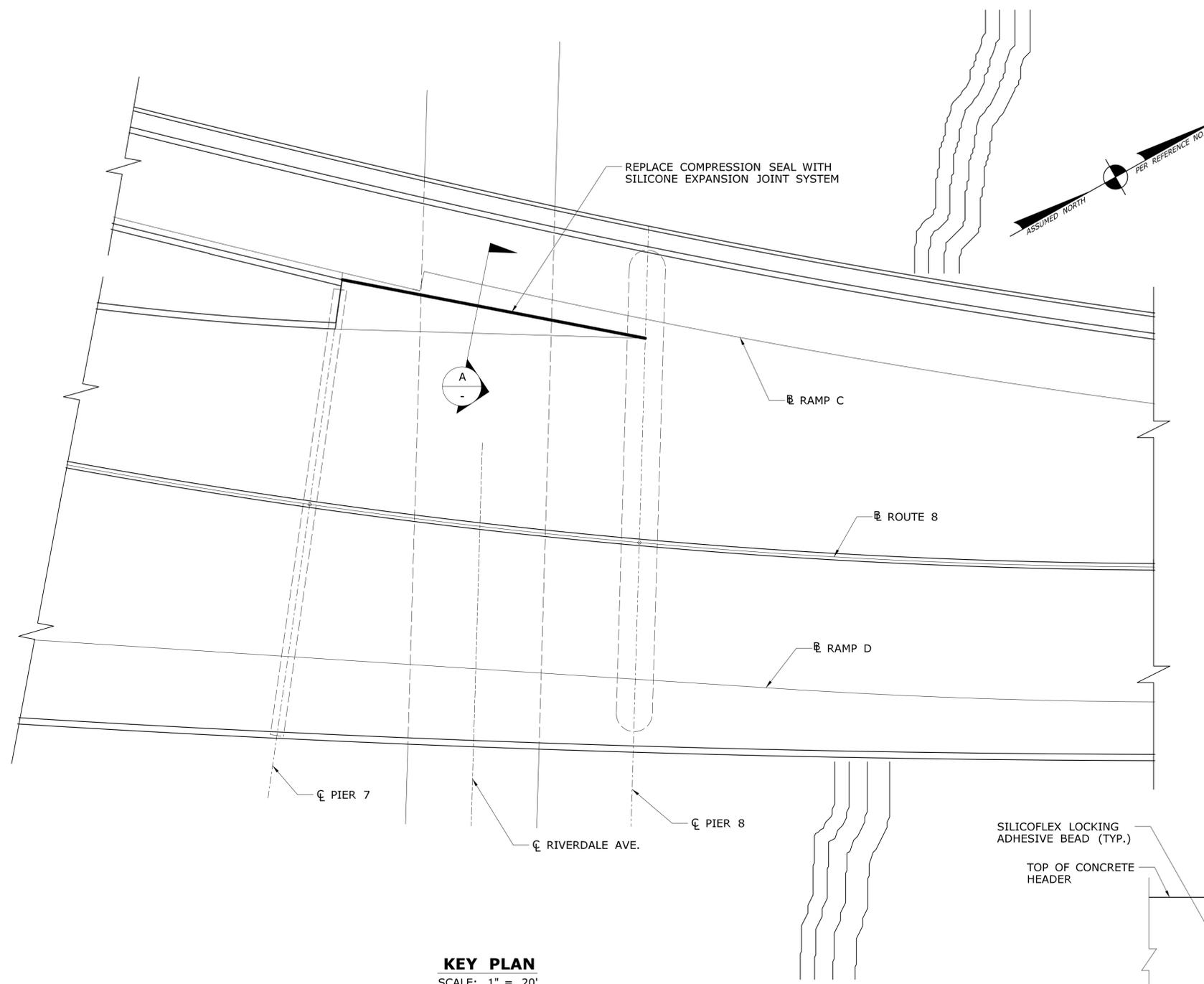
SIGNATURE/BLOCK:

PROJECT TITLE:  
**COMMODORE HULL BRIDGE  
PAINTING AND STEEL REPAIRS**

TOWN: **SHELTON  
DERBY**  
DRAWING TITLE:  
**FENCE RESET DETAILS**

PROJECT NO.: **126-170**  
DRAWING NO.: **S-34**  
SHEET NO.

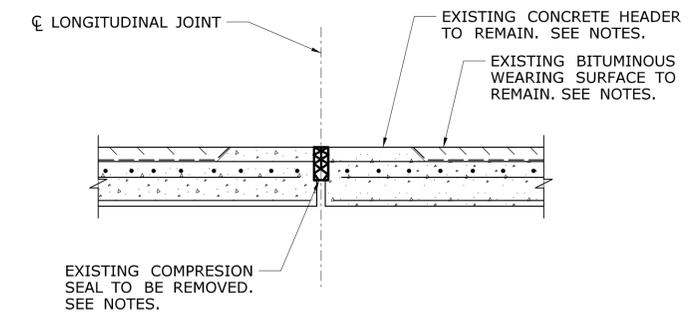




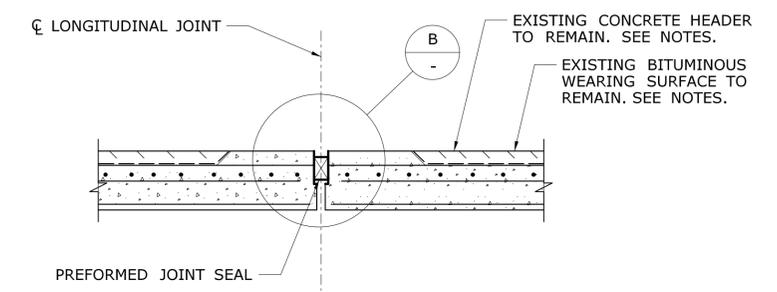
**KEY PLAN**  
SCALE: 1" = 20'

**PREFORMED JOINT SEAL NOTES:**

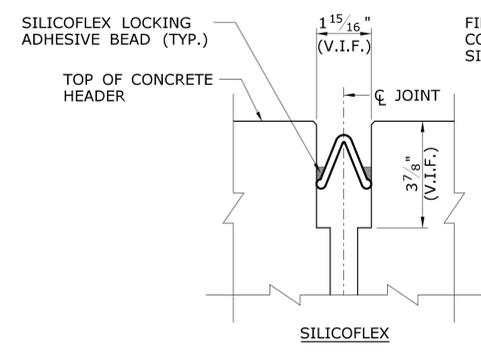
1. ALL MATERIALS SHALL BE INSTALLED PER MANUFACTURERS RECOMMENDATIONS.
2. THE EXISTING CONCRETE HEADERS SHALL BE BEVELED  $\frac{1}{8}$ " ALONG THE OPENING OF THE JOINT.
3. THE CONTRACTOR SHALL FIELD VERIFY JOINT OPENING PRIOR TO ORDERING JOINT MATERIALS.
4. THE CONTRACTOR SHALL REMOVE EXISTING IMPACT ATTENUATOR AND INSTALL TEMPORARY IMPACT ATTENUATOR PRIOR TO THE WORK. SEE DETAILS IN M&PT SET.



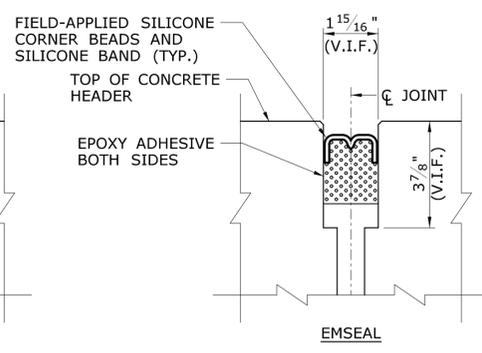
**EXISTING JOINT A**  
SCALE: N.T.S.



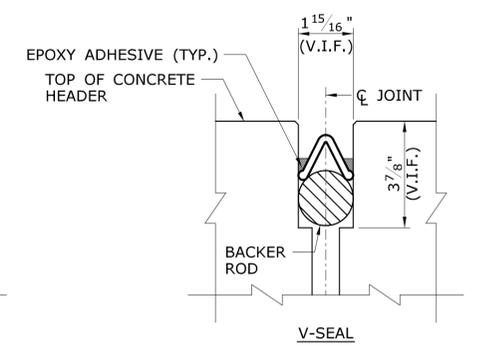
**PROPOSED JOINT A**  
SCALE: N.T.S.



**SILICOFLEX**



**EMSEAL**



**V-SEAL**

**PREFORMED JOINT SEAL B**  
SCALE: N.T.S.

**90% PROGRESS SUBMISSION**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015

DESIGNER/DRAFTER: **KAB/ALH**  
 CHECKED BY: **CC**  
 SCALE AS NOTED

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

Filename: ...SB\_MSH\_Br00571A\_126-170\_JNT1.dgn

SIGNATURE/  
BLOCK:

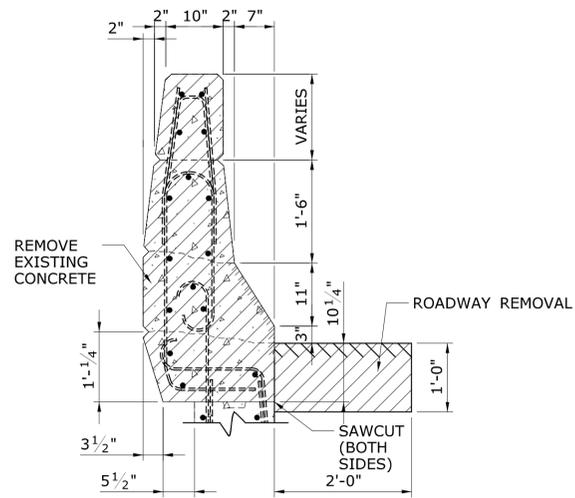
PROJECT TITLE:  
**COMMODORE HULL BRIDGE  
PAINTING AND STEEL REPAIRS**

TOWN: **SHELTON  
DERBY**  
 DRAWING TITLE:  
**EXPANSION JOINT  
DETAILS**

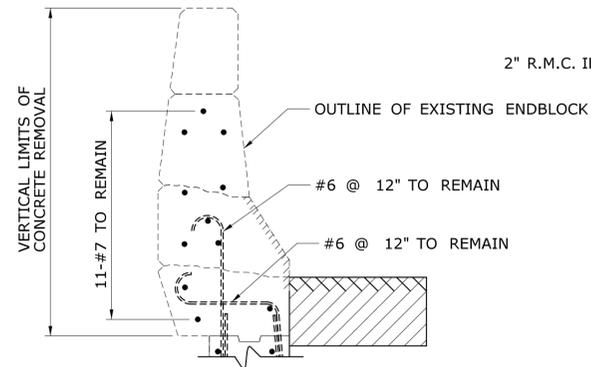
PROJECT NO.: **126-170**  
 DRAWING NO.: **S-36**  
 SHEET NO.

**NOTES:**

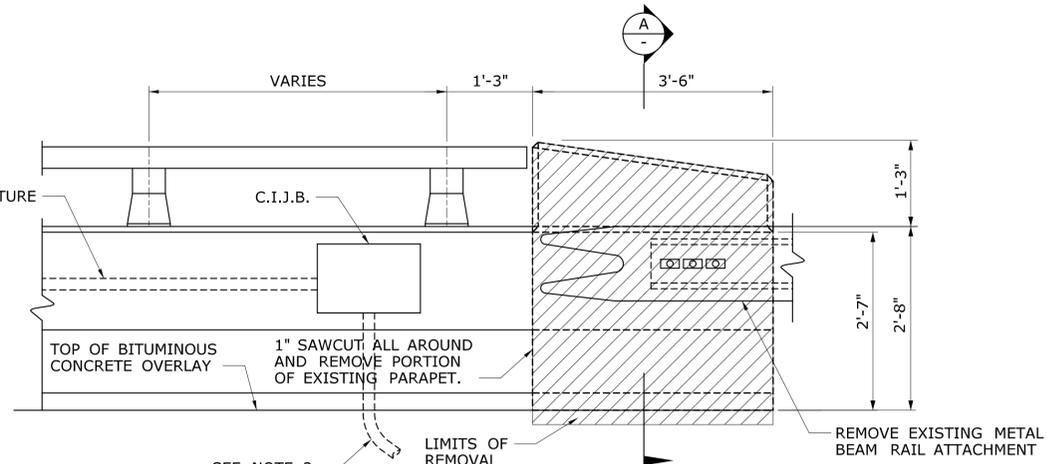
- SEE DWG. NO. S-01 FOR RECONSTRUCTION LOCATION. TOTAL 1 LOCATION.
- ALL STEEL MATERIALS INCLUDING THE ANCHOR PLATE AND 7/8" DIA. BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE REQUIREMENT OF ASTM A123.
- LOCATION OF CONDUIT BETWEEN C.I.J.B. & LIGHT STANDARD UNKNOWN. CONTRACTOR TO FIELD VERIFY LOCATION OF CONDUIT AND TAKE CARE NOT TO DAMAGE CONDUIT DURING REMOVAL AND RECONSTRUCTION OF END BLOCK AND INSTALLATION OF GUIDERAIL BRIDGE ATTACHMENT. ANY DAMAGE TO CONDUIT SHALL BE REPAIRED BY CONTRACTOR AT NO EXPENSE TO THE STATE.



**SECTION A**



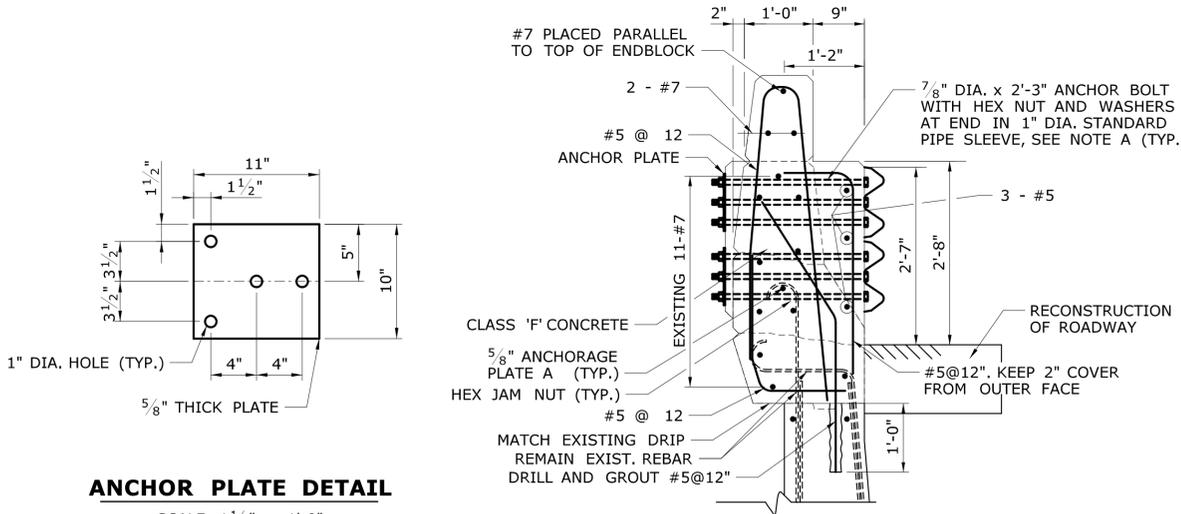
**SECTION (AFTER REMOVAL) A**



**ELEVATION**

**EXISTING ENDBLOCK**

SCALE: 3/4" = 1'-0"

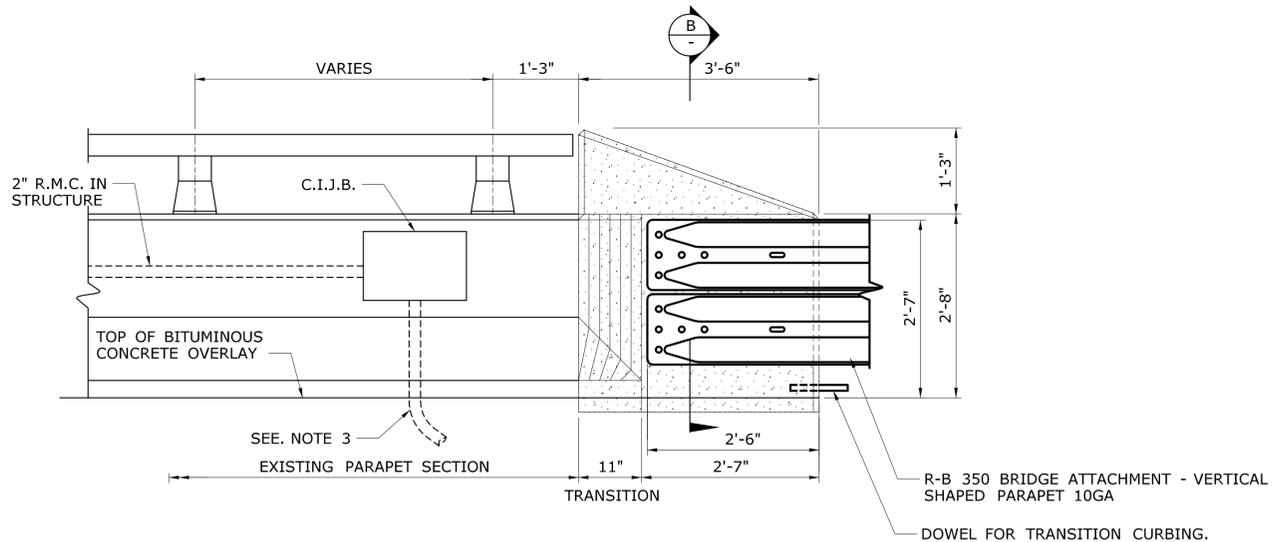


**ANCHOR PLATE DETAIL**

SCALE: 1 1/2" = 1'-0"

**NOTE A:**

1" DIA. PIPE SHALL CONFORM TO ASTM A53 GR. B OR ASTM A501 AND SHALL BE GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM A123.



**ELEVATION**

**R-B 350 BRIDGE ATTACHMENT-VERTICAL SHAPE PARAPET 10GA**

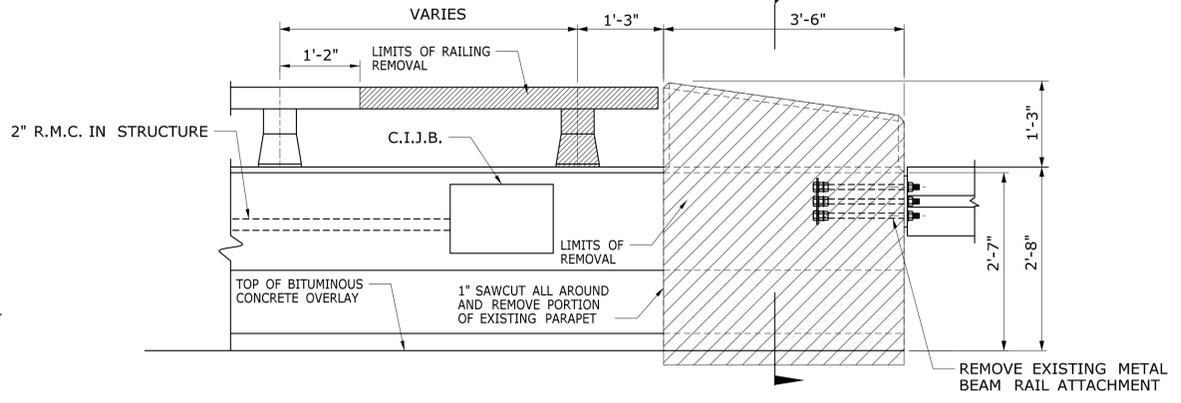
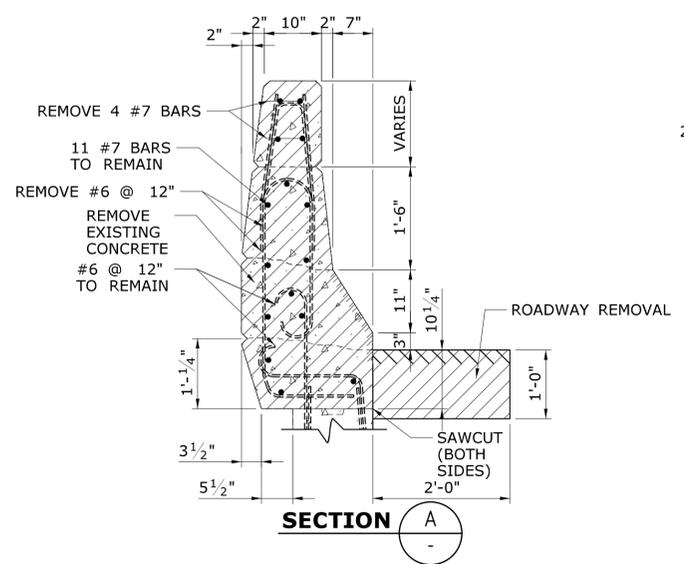
SCALE: 3/4" = 1'-0"

**90% PROGRESS SUBMISSION**

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...SB_MSH_Br00571A_126-170_RLD2.dgn	SIGNATURE/BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-37</b> SHEET NO.
REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 10/1/2015	<b>PARAPET ANCHORAGE DETAILS 1 OF 2</b>						

**NOTES:**

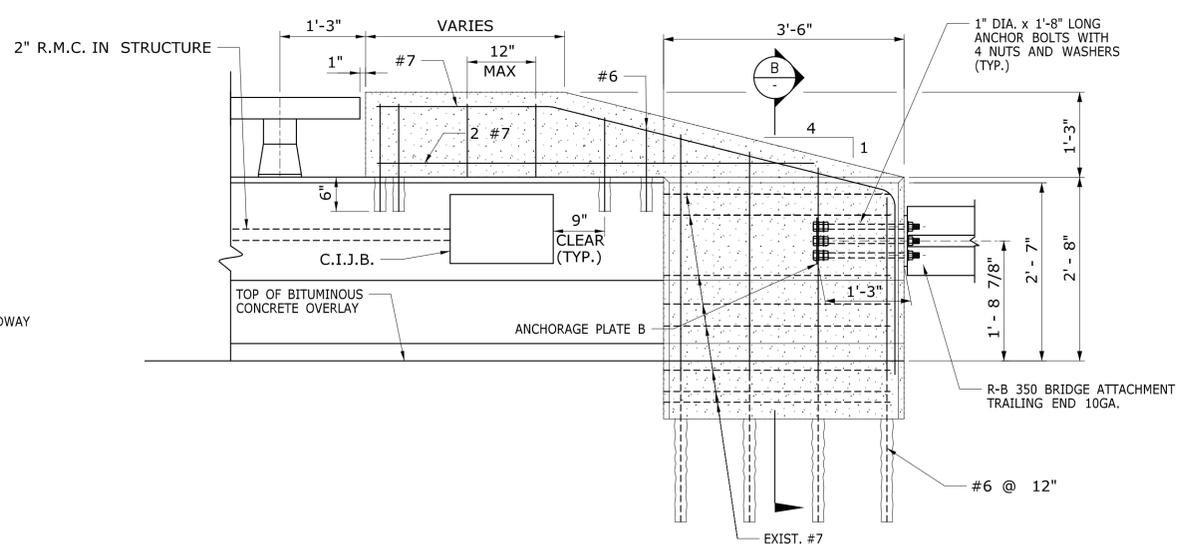
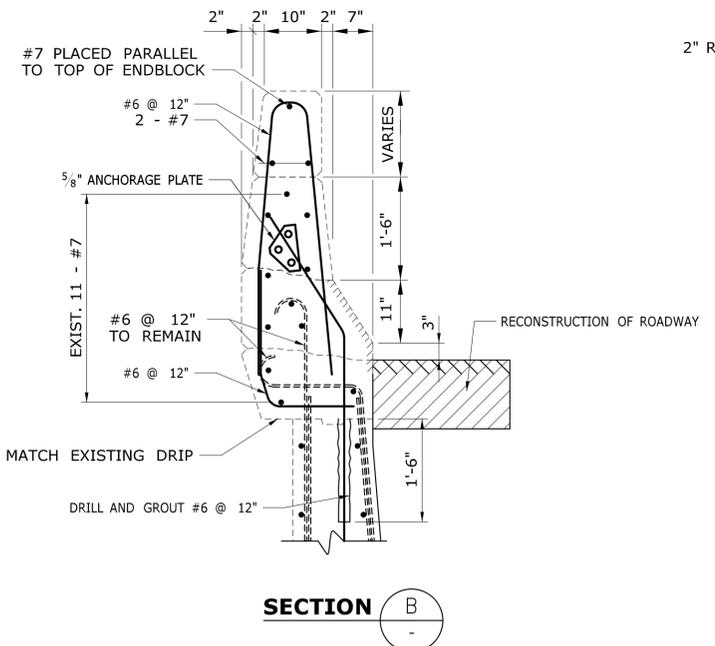
- SEE DWG. NO. S-01 FOR RECONSTRUCTION LOCATIONS. TOTAL 2 LOCATIONS.
- ALL STEEL MATERIALS INCLUDING THE ANCHOR PLATE AND 7/8" DIA. BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE REQUIREMENT OF ASTM A123.



**ELEVATION**

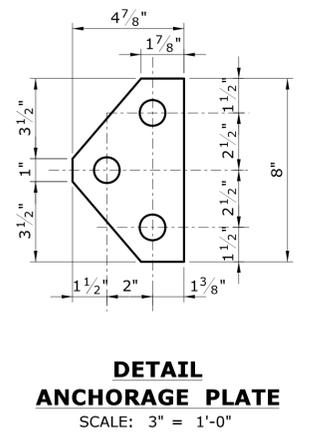
**EXISTING ENDBLOCK**  
SCALE: 3/4" = 1'-0"

LOCATION OF CONDUIT BETWEEN C.I.J.B. & LIGHT STANDARD UNKNOWN. CONTRACTOR TO FIELD VERIFY LOCATION OF CONDUIT AND TAKE CARE NOT TO DAMAGE CONDUIT DURING REMOVAL AND RECONSTRUCTION OF END BLOCK AND INSTALLATION OF GUIDERAIL BRIDGE ATTACHMENT. ANY DAMAGE TO CONDUIT SHALL BE REPAIRED BY CONTRACTOR AT NO EXPENSE TO THE STATE.

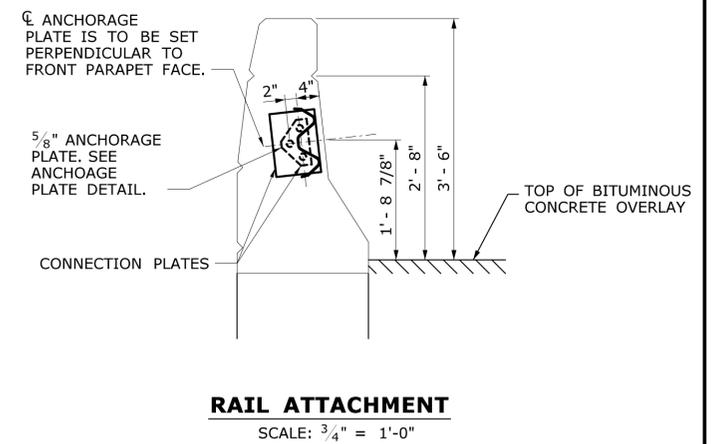


**ELEVATION**

**R-B 350 BRIDGE ATTACHMENT TRAILING END 10GA.**  
SCALE: 3/4" = 1'-0"



**DETAIL ANCHORAGE PLATE**  
SCALE: 3" = 1'-0"



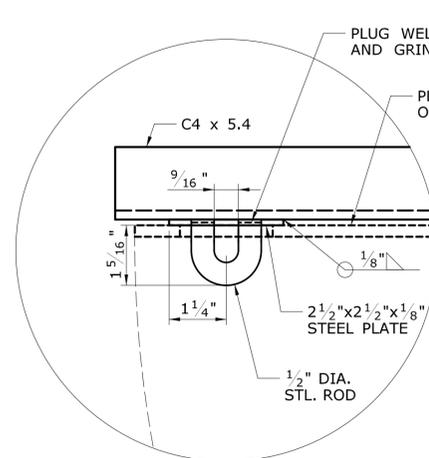
**RAIL ATTACHMENT**  
SCALE: 3/4" = 1'-0"

**90% PROGRESS SUBMISSION**

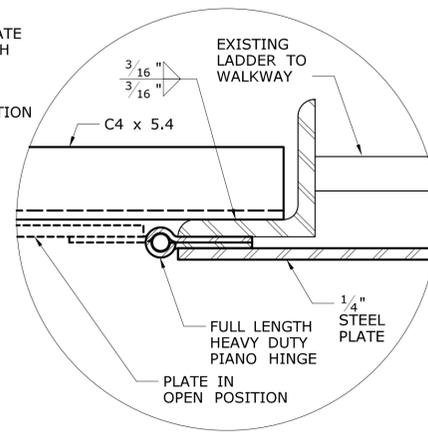
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION Filename: ...SB_MSH_Br00571A_126-170_RLD3.dgn	SIGNATURE/BLOCK:  PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b> DRAWING TITLE: <b>PARAPET ANCHORAGE DETAILS 2 OF 2</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-38</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		

**NOTES:**

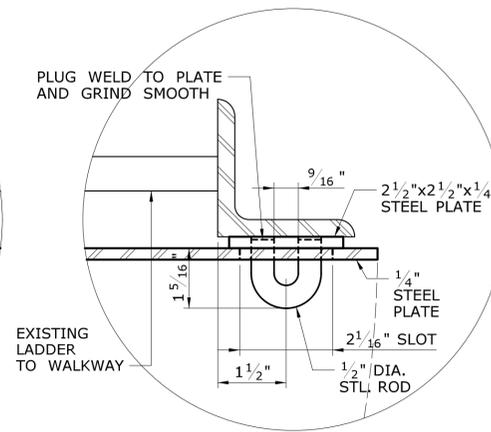
1. FURNISHING AND INSTALLING LOCKABLE SECURITY LADDER RUNG GUARD SHALL BE PAID FOR UNDER THE ITEM "STRUCTURE STEEL".
2. ALL STEEL SHALL CONFORM TO AASHTO M270, GRADE 50 T2.
3. WELDING DETAILS, PROCEDURES AND TESTING METHODS SHALL CONFORM TO THE AASHTO/AWS D1.5 BRIDGE WELDING CODE, UNLESS OTHERWISE NOTED ON THE PLANS.



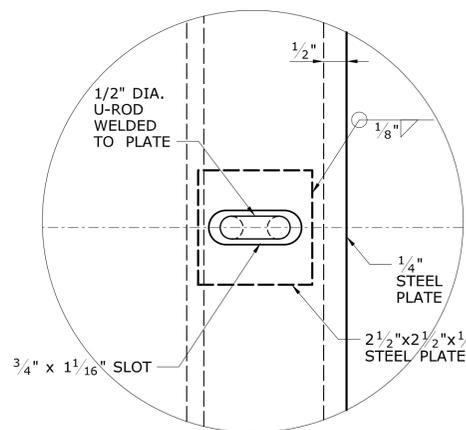
**DETAIL C**  
SCALE: 6" = 1'-0"



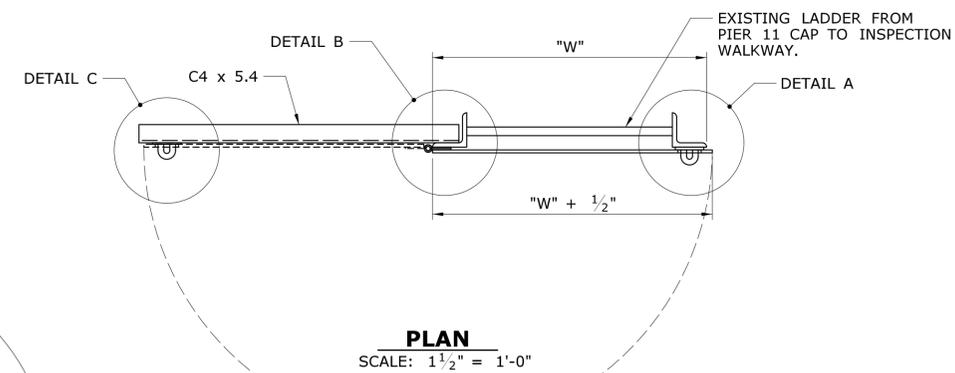
**DETAIL B**  
SCALE: 6" = 1'-0"



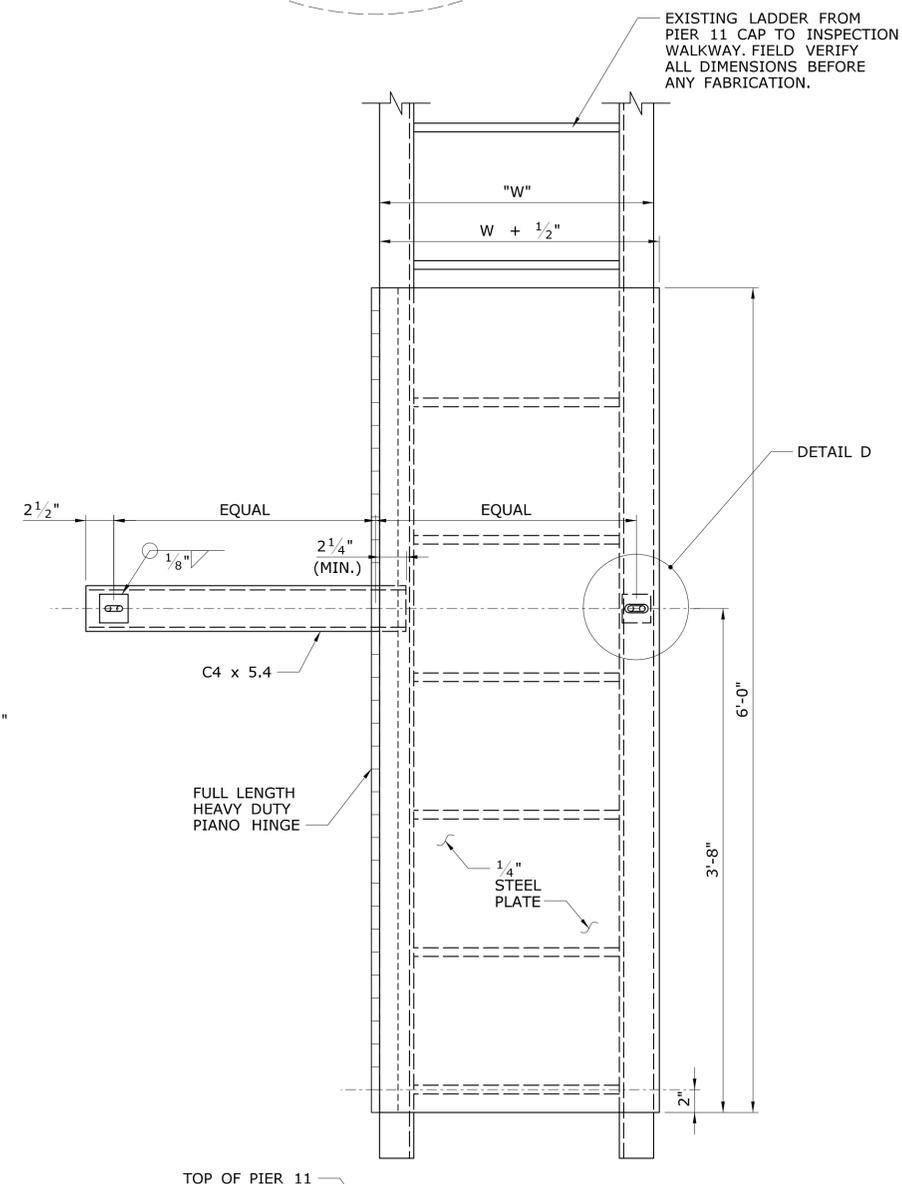
**DETAIL A**  
SCALE: 6" = 1'-0"



**DETAIL D**  
SCALE: 6" = 1'-0"



**PLAN**  
SCALE: 1 1/2" = 1'-0"

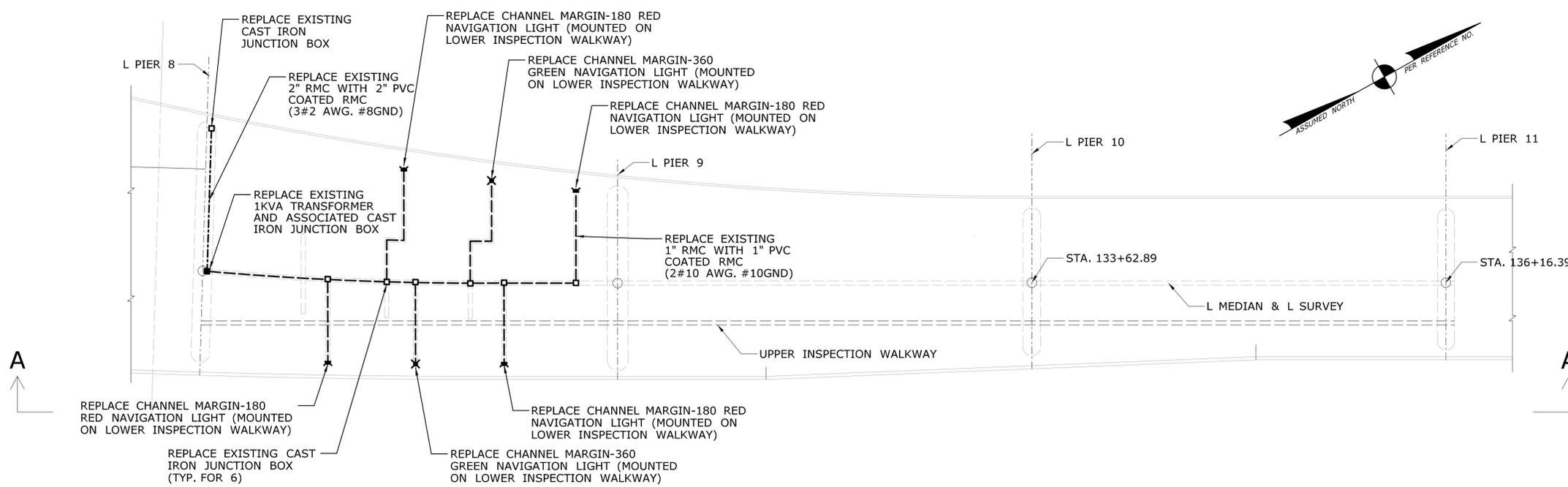


**ELEVATION**  
SCALE: 1 1/2" = 1'-0"

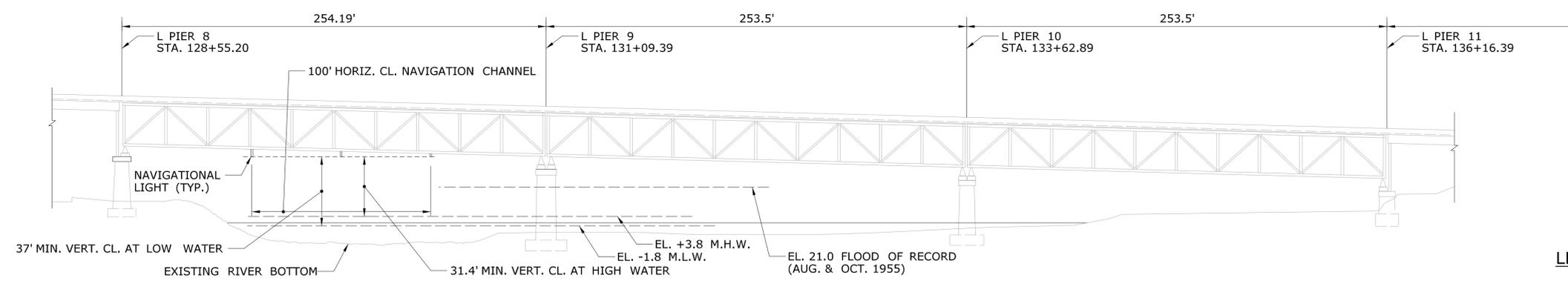
**LOCKABLE SECURITY LADDER RUNG GUARD DETAILS**

**90% PROGRESS SUBMISSION**

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>KAB/ALH</b> CHECKED BY: <b>CC</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION Filename: ...SB_MSH_Br00571A_126-170_MDS3.dgn	SIGNATURE/BLOCK:  PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b> DRAWING TITLE: <b>MISCELLANEOUS DETAILS</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-39</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015		



PLAN  
SCALE: 1" = 40'

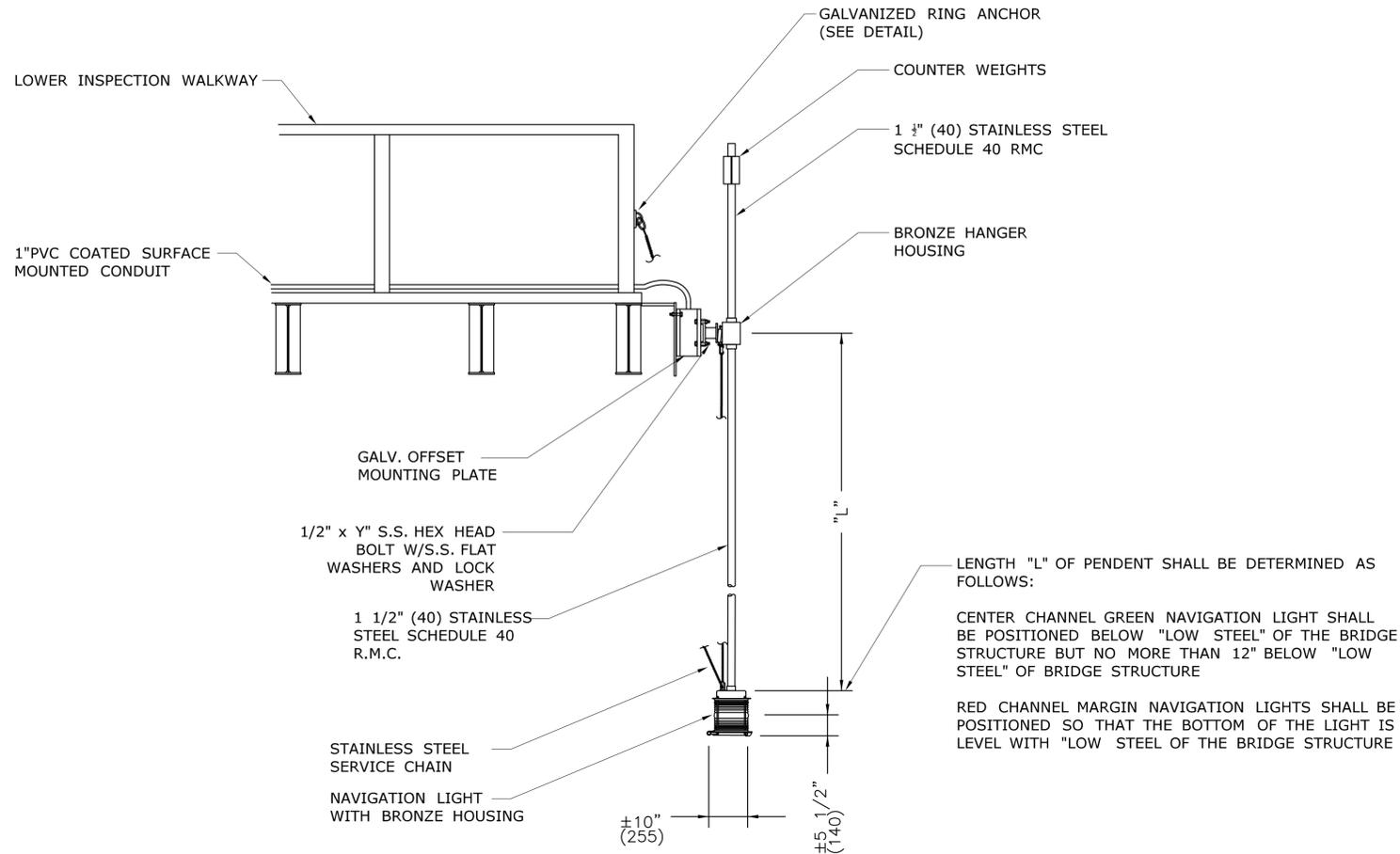


ELEVATION A-A  
SCALE: 1" = 40'

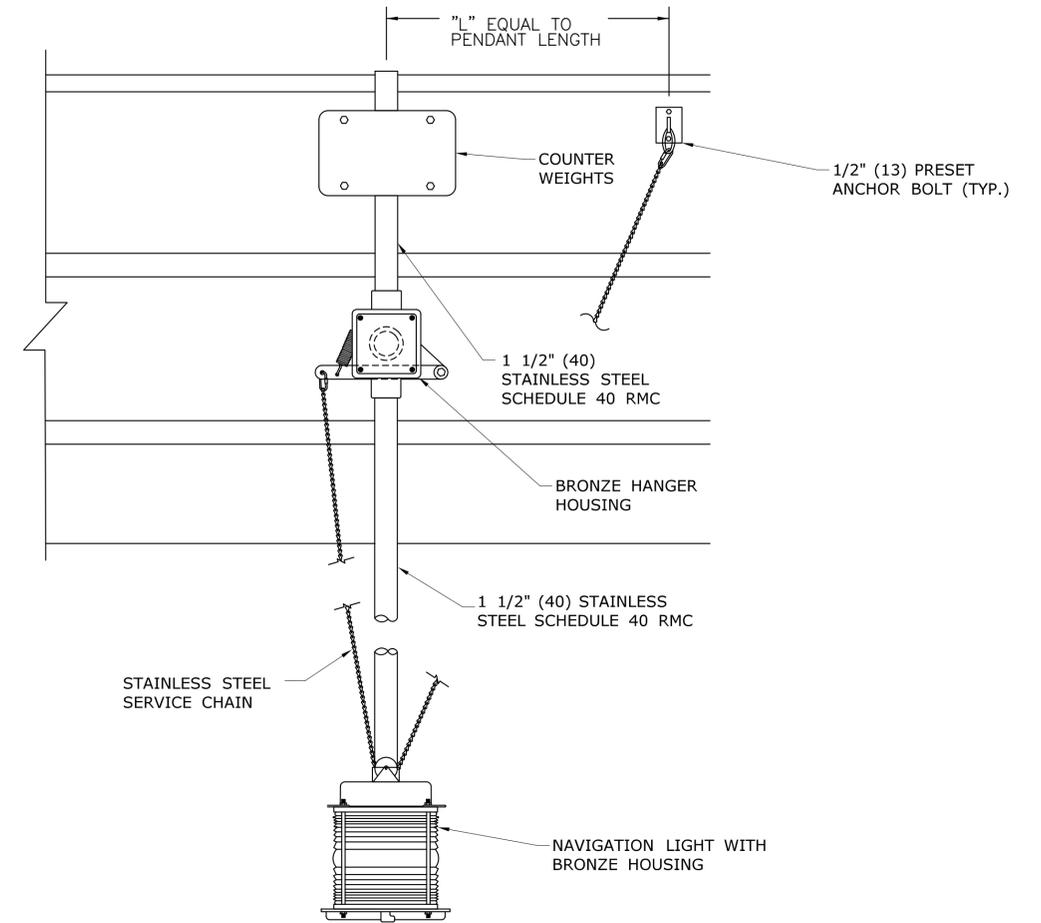
- LEGEND:**
- 1" SURFACE MOUNTED PVC COATED RIGID METAL CONDUIT (2#10 AWG + #10GND)
  - 2" SURFACE MOUNTED PVC COATED RIGID METAL CONDUIT (3#2 AWG + #8GND)
  - JUNCTION BOX 8"x8"x8" CAST IRON (SURFACE MOUNTED)
  - TRANSFORMER JUNCTION BOX 18"x18"x10" 1-1 KVA TRANSFORMER 480V PRI. FOR NAVIGATION LIGHTS 240/120V SEC.
  - ▼ 180 DEGREE RED LED NAVIGATION LIGHT
  - ✕ 360 DEGREE GREEN LED NAVIGATION LIGHT

**90% PROGRESS SUBMISSION**

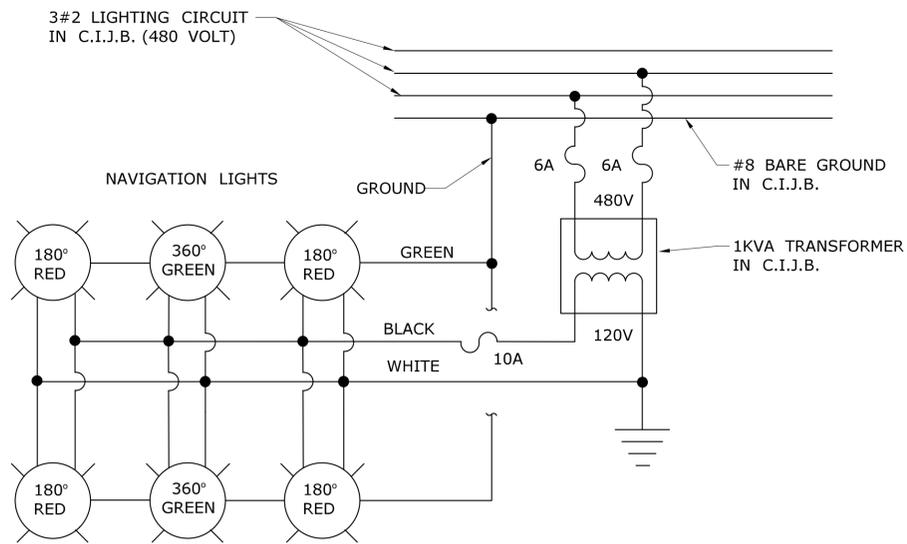
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>ALH</b> CHECKED BY: <b>MPE</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION Filename: ...SB_MSH_Br00571A_126-170_NAV1.dgn	SIGNATURE/BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON DERBY</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-40</b> SHEET NO.
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015	<b>ELECTRICAL LIGHTING PLAN</b>		



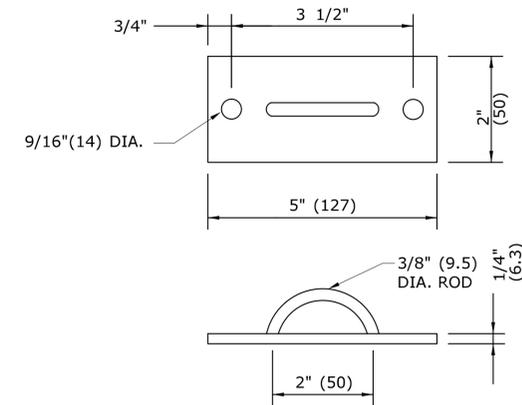
**PENDANT MOUNTED NAVIGATION LIGHT**



**ELEVATION VIEW**



**WIRING DIAGRAM**



**GALVANIZED RING ANCHOR**

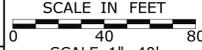
**90% PROGRESS SUBMISSION**

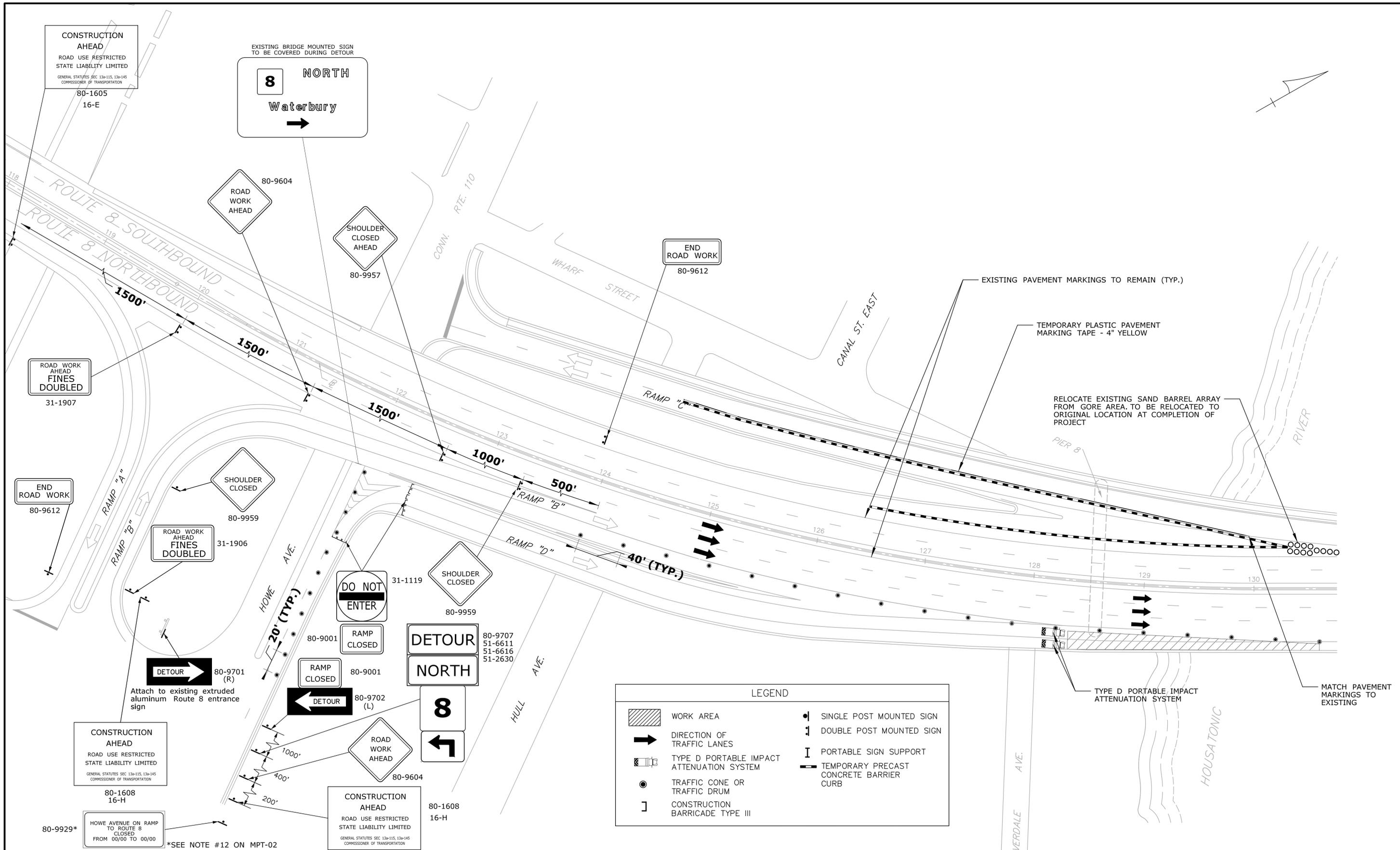
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>LL</b> CHECKED BY: <b>IF</b> SCALE AS NOTED	<b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...SB_MSH_Br00571A_126-170_NAV2.dgn	SIGNATURE/ BLOCK:	PROJECT TITLE: <b>COMMODORE HULL BRIDGE</b> <b>PAINTING AND STEEL REPAIRS</b>	TOWN: <b>SHELTON</b> <b>DERBY</b> DRAWING TITLE: <b>ELECTRICAL DETAILS</b>	PROJECT NO. <b>126-170</b> DRAWING NO. <b>S-41</b> SHEET NO.	
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/1/2015				



**GENERAL STAGE CONSTRUCTION NOTES**

1. SEE SPECIAL PROVISION SECTION 1.08 - PROSECUTION PROGRESS AND ITEM #0971001A - MAINTENANCE AND PROTECTION OF TRAFFIC FOR CONSTRUCTION STAGING REQUIRMENTS.
2. EXACT LOCATIONS OF SIGNS TO BE VERIFIED BY THE ENGINEER IN THE FIELD.
3. INSTALL TYPE III BARRICADES ACROSS RAMP ENTRANCE TO FULLY CLOSE THE ROAD TO TRAFFIC.
4. DETOUR SIGNS TO BE PAID UNDER ITEM NO. 1220027 - CONSTRUCTION SIGNS.
5. CONTRACTOR TO NOTIFY THE TOWNS OF SHELTON AND DERBY'S EMEGENCY SERVICE AT LEAST TWO WEEKS PRIOR TO THE CLOSURE.
6. CONSTRUCTION/DETOUR SIGNS ARE TO BE INSTALLED SO THAT THEY DO NOT BLOCK OR ARE NOT BLOCKED BY EXISTING SIGNS.
7. ALL CONFLICTING SIGNS SHALL BE COVERED OR REMOVED WHEN THE DETOUR IS IN EFFECT. REMOVAL/COVERING OF EXISTING SIGNS IS PAYABLE UNDER ITEM NO. 0971001A - MAINTENANCE AND PROTECTION OF TRAFFIC.
8. ALL CONSTRUCTION/DETOUR SIGNS TO BE INSTALLED ON BREAKAWAY POSTS PER TRAFFIC STANDARD SHEET TR-1208-01, TR-1208-02, AND TR-1220-02.
9. LOCATIONS OF TEMPORARY SIGNS SHOW ON PLANS ARE APPROXIMATE AND SHALL BE ADJUSTED BY THE CONTRACTOR TO MEET FIELD CONDITIONS.
10. ANY EXISTING SIGNING DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT NO COST TO THE STATE.
11. BARRICADE WARNING LIGHTS - HIGH INTESNITY SHALL BE MOUNTED ON ALL DIAMOND SHAPED POST-MOUNTED CONSTRUCTION SIGNS.
12. TWO WEEKS PRIOR TO INITIATING THE DETOUR, INSTALL SIGN NO. 80-9929 ON HOWE AVENUE IN THE VICINITY OF THE ON RAMP. THIS SIGN SHALL BE REMOVED ONCE THE DETOUR IS IN THE EFFECT.
13. INSTALL SERIES 16 SIGN IN ADVANCE OF "FINES DOUBLED" SIGN AND AT LOCATIONS DIRECTED BY THE ENGINEER. INSTALL SIGN 16-E AT ROUTE 8 NORTHBOUND AND SOUTHBOUND AND SIGN 16-H AT RAMP B AND D. THESE SIGNS TO REMAIN POSTED THROUGHOUT STAGE CONSTRUCTION.
14. ALL EXISTING SIGNS NOT SHOWN ON PLANS TO REMAIN.
15. REMOVE OR COVER ALL CONFLICTING PAVEMENT MARKINGS, INCLUDING THOSE OUTSIDE THE TRAVELWAY.
16. TWO WEEKS PRIOR TO INITIATING THE CLOSURE, INSTALL ITEM #1131002A - REMOTE CONTROL CHANGEABLE MESSAGE SIGN ON ROUTE 8 IN BOTH DIRECTIONS PRIOR TO EXIT RAMP IN THE VICINITY OF THE CLOSURE. THESE SIGNS SHALL BE REMOVED ONCE THE DETOUR IS IN EFFECT.

		THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: <b>MGK</b> CHECKED BY: <b>MWD</b> SCALE IN FEET  SCALE 1"=40'		 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>		 <b>VN ENGINEERS INC.</b> Traffic • Infrastructure • Planning 215 WASHINGTON AVENUE NORTH HAVEN, CT 06473 203.234.7862		PROJECT TITLE: <b>REHABILITATION OF BRIDGE NO. 00571A (COMMODORE HULL) ROUTE 8 OVER HOUSATONIC RIVER</b>		TOWN: <b>SHELTON - DERBY</b>		PROJECT NO. <b>126-170</b>	
										DRAWING TITLE: <b>GENERAL NOTES AND DETAILS</b>		DRAWING NO. <b>MPT-02</b>			
REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 9/22/2015				Filename: ...TR_MSH_MPT-001_126_170.dgn								SHEET NO.			



LEGEND	
	WORK AREA
	DIRECTION OF TRAFFIC LANES
	TYPE D PORTABLE IMPACT ATTENUATION SYSTEM
	TRAFFIC CONE OR TRAFFIC DRUM
	CONSTRUCTION BARRICADE TYPE III
	SINGLE POST MOUNTED SIGN
	DOUBLE POST MOUNTED SIGN
	PORTABLE SIGN SUPPORT
	TEMPORARY PRECAST CONCRETE BARRIER CURB

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 9/23/2015

DESIGNER/DRAFTER:  
**AA/MGK**

CHECKED BY:  
**MWD**

SCALE IN FEET  
0 40 80  
SCALE 1"=40'

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

File name: ...TR\_MSH\_MPT-002\_126\_159.dgn

SIGNATURE/BLOCK:

**VN ENGINEERS INC.**  
Traffic • Infrastructure • Planning  
215 WASHINGTON AVENUE  
NORTH HAVEN, CT 06473  
203.234.9862

PROJECT TITLE:  
**REHABILITATION OF BRIDGE NO. 00571A (COMMODORE HULL) ROUTE 8 OVER HOUSATONIC RIVER**

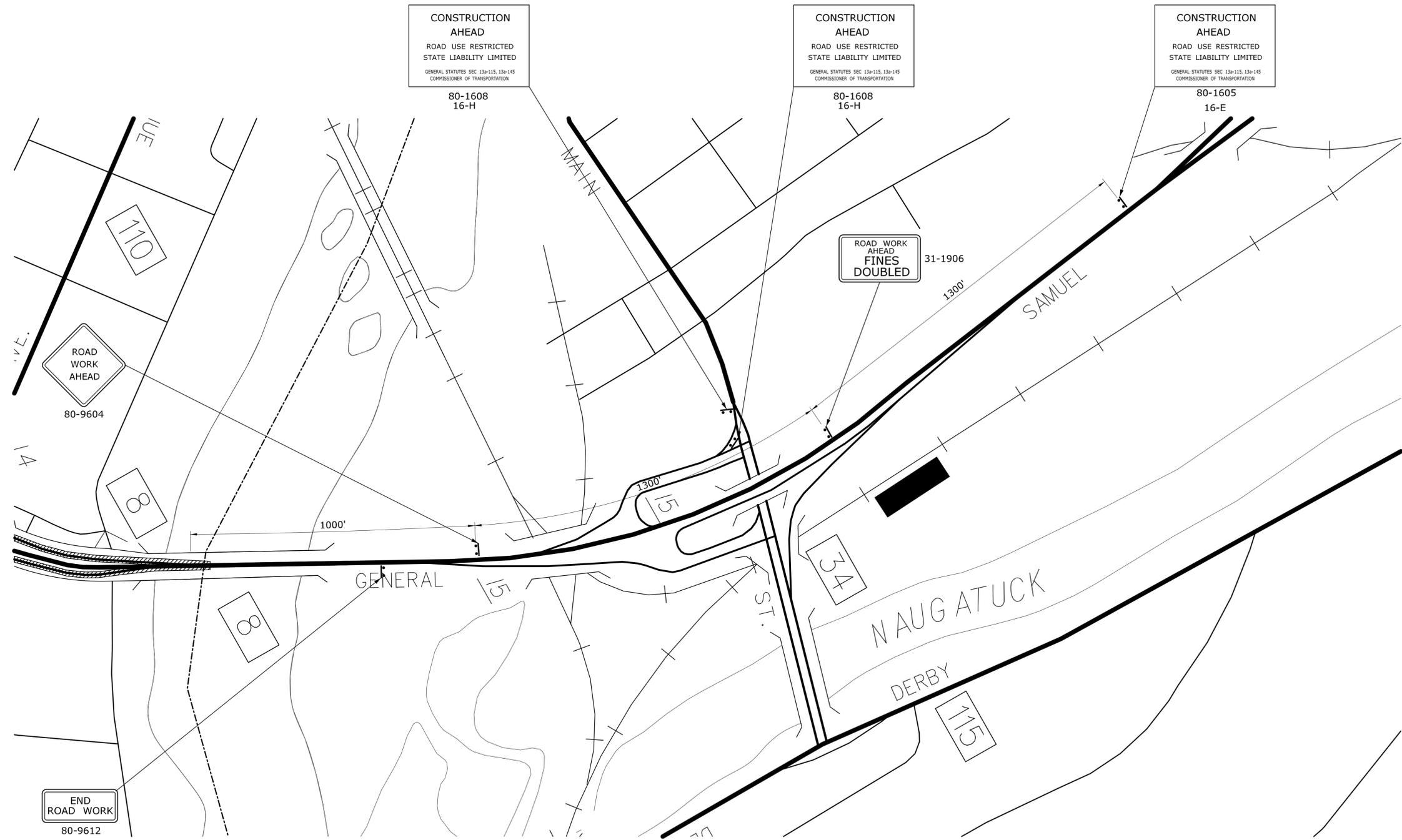
TOWN: **SHELTON - DERBY**

DRAWING TITLE:  
**MAINTENANCE AND PROTECTION OF TRAFFIC**

PROJECT NO. **126-170**

DRAWING NO. **MPT-03**

SHEET NO.



LEGEND	
	WORK AREA
	DIRECTION OF TRAFFIC LANES
	TYPE D PORTABLE IMPACT ATTENUATION SYSTEM
	TRAFFIC CONE OR TRAFFIC DRUM
	CONSTRUCTION BARRICADE TYPE III
	SINGLE POST MOUNTED SIGN
	DOUBLE POST MOUNTED SIGN
	PORTABLE SIGN SUPPORT
	TEMPORARY PRECAST CONCRETE BARRIER CURB

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 9/22/2015

DESIGNER/DRAFTER:  
**MGK**  
CHECKED BY:  
**MWD**  
SCALE IN FEET  
0 200 400  
SCALE 1"=200'

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

File name: ...TR.MSH.MPT-002B.126.159.dgn

SIGNATURE/BLOCK:  
VN ENGINEERS INC.  
Traffic • Infrastructure • Planning  
215 WASHINGTON AVENUE  
NORTH HAVEN, CT 06473  
203.234.7862

PROJECT TITLE:  
**REHABILITATION OF BRIDGE NO. 00571A (COMMODORE HULL) ROUTE 8 OVER HOUSATONIC RIVER**

TOWN: **SHELTON - DERBY**  
DRAWING TITLE:  
**MAINTENANCE AND PROTECTION OF TRAFFIC**

PROJECT NO. **126-170**  
DRAWING NO. **MPT-04**  
SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.



E5 - SERIES				G20 - SERIES				M4 - SERIES				R1 - SERIES				R9 & R11 - SERIES				W1 - SERIES				W3 - SERIES																																																																																																							
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<p>CONSTRUCTION AHEAD</p> <p>SIDEWALK USE RESTRICTED STATE LIABILITY LIMITED</p> <p>GENERAL STATUTES SEC 13a-115, 13a-145 COMMISSIONER OF TRANSPORTATION</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16-S</td><td>10.0</td><td>48X30</td><td>80-1619</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16-S	10.0	48X30	80-1619	2	<p>BUSINESS ACCESS</p> <p>VARIABLE ARROW COPY &amp; BORDER - WHITE BACKGROUND - BLUE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>50-5934</td><td>1</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	50-5934	1	<p>DETOUR</p> <p>VARIABLE ARROW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>5.0</td><td>30X24</td><td>80-9710</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.0	30X24	80-9710	2	<p>RAMP CLOSED</p> <p>* COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>10.0</td><td>48X30</td><td>80-9001</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.0	48X30	80-9001	2	<p>ROAD CLOSED</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>10.0</td><td>48X30</td><td>80-9080</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.0	48X30	80-9080	2	<p>ROAD WORK AHEAD FINES DOUBLED</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>14.0</td><td>48X42</td><td>31-1906</td><td>2</td></tr> <tr><td>22.5</td><td>60X54</td><td>31-1907</td><td>2</td></tr> <tr><td>30.0</td><td>72X60</td><td>31-1908</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	14.0	48X42	31-1906	2	22.5	60X54	31-1907	2	30.0	72X60	31-1908	2	<p>BOTH LANES SHIFT LEFT AHEAD (L)    BOTH LANES SHIFT RIGHT AHEAD (R)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>1.5</td><td>12X18</td><td>80-9402</td><td>1</td></tr> <tr><td>5.0</td><td>24X30</td><td>80-9403</td><td>1</td></tr> <tr><td>7.5</td><td>30X36</td><td>80-9404</td><td>1</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	1.5	12X18	80-9402	1	5.0	24X30	80-9403	1	7.5	30X36	80-9404	1	<p>STOP AHEAD</p> <p>TOP CIRCLE - RED MIDDLE CIRCLE - YELLOW BOTTOM CIRCLE - GREEN COPY &amp; BORDER - BLACK BACKGROUND - ORANGE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>25.0</td><td>60</td><td>80-9444L</td><td>2</td></tr> <tr><td>25.0</td><td>60</td><td>80-9446R</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9444L	2	25.0	60	80-9446R	2											
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16.0	48	80-9802	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
9.0	36	80-9506	1																																																																																																																																																																				
16.0	48	80-9508	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
9.0	36	80-9614	1																																																																																																																																																																				
16.0	48	80-9615	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
16.0	48	80-9837	2																																																																																																																																																																				
16.0	48	80-9838	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
32.0	96X48	80-9815																																																																																																																																																																					
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
1.66	30X8	80-9829																																																																																																																																																																					
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
10.5	42X36	80-9623	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
16.0	48	80-9956	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
(1) 16.0	48	80-9957	2																																																																																																																																																																				
(2) 9.0	36	80-9958	1																																																																																																																																																																				
(2) 16.0	48	80-9959	2																																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																																				
2.0	12X24	41-0815																																																																																																																																																																					
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W4-W6 - SERIES				W8-W9 - SERIES				W13 - SERIES				W20 - SERIES				W21 - SERIES				W22 - SERIES				STOP-SLOW PADDLE																																																																																																																																											
<p>(L) (R)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9945</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9945	2	<p>BUMP AHEAD</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9801</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9802</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9801	1	16.0	48	80-9802	2	<p>REDUCE SPEED TO 00 MPH</p> <p>VARIABLE SPEED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>12.0</td><td>72X24</td><td>80-9519</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.0	72X24	80-9519	2	<p>ROAD WORK 1000 FT</p> <p>VARIABLE DISTANCE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9805</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9806</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9805	1	16.0	48	80-9806	2	<p>LEFT LANE CLOSED AHEAD (L)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9847</td><td>2</td></tr> <tr><td>16.0</td><td>48</td><td>80-9848</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9847	2	16.0	48	80-9848	2	<p>RIGHT LANE CLOSED AHEAD (R)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>1.66</td><td>30X8</td><td>80-9829</td><td></td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	1.66	30X8	80-9829		<p>SLOW MOVING TRUCKS AHEAD</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>1.66</td><td>30X8</td><td>80-9829</td><td></td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	1.66	30X8	80-9829		<p>TURN OFF 2-WAY RADIO AND CELL PHONE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>10.5</td><td>42X36</td><td>80-9623</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.5	42X36	80-9623	2	<p>(VARIABLE LEGEND)</p> <p>USE SHOULDER</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9956</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9956	2	<p>SHOULDER CLOSED AHEAD (1)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>(1) 16.0</td><td>48</td><td>80-9957</td><td>2</td></tr> <tr><td>(2) 9.0</td><td>36</td><td>80-9958</td><td>1</td></tr> <tr><td>(2) 16.0</td><td>48</td><td>80-9959</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	(1) 16.0	48	80-9957	2	(2) 9.0	36	80-9958	1	(2) 16.0	48	80-9959	2	<p>SHOULDER CLOSED (2)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.0</td><td>12X24</td><td>41-0815</td><td></td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	12X24	41-0815		<p>NEW</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - YELLOW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.0</td><td>12X24</td><td>41-0815</td><td></td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	12X24	41-0815	
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**METRIC CONVERSION CHART**  
(1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC
12"	300	60"	1500
18"	450	66"	1650
24"	600	72"	1800
30"	750	78"	1950
36"	900	84"	2100
42"	1050	90"	2250
48"	1200	96"	2400
54"	1350		

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 9/22/2015

DIMENSIONS ARE IN ENGLISH ("") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: . OVER 1" TO NEAREST 5 mm . UNDER 1" TO NEAREST 1 mm.

**STATE OF CONNECTICUT**  
**DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

FILENAME: CTDOT\_TRAFFIC\_STD.DGN    Model: TR-1220\_01

SUBMITTED BY:

STANDARD SHEET TITLE: **SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS**

STANDARD SHEET NO.: **TR-1220\_01**