

STATE OF CONNECTICUT

OFFICE OF POLICY AND MANAGEMENT

OFFICE OF THE SECRETARY

TO: Mark W. Alexander, Transportation Assistant Planning
Director, DOT

FROM: Benjamin Barnes, Secretary 
OPM

DATE: January 17, 2013

SUBJECT: Environmental Impact Evaluation (EIE) for New Haven-
Hartford-Springfield Line High Speed Intercity Passenger
Rail Project

Based on a review of the subject environmental impact evaluation and related documentation conducted pursuant to C.G.S. 22a-1e, I am herewith advising you of my finding that this evaluation satisfies the requirements of the Connecticut Environmental Policy Act as it relates to the Connecticut portion of the project.

Please contact Dimple Desai (418-6412) if there are any questions with regard to this finding.

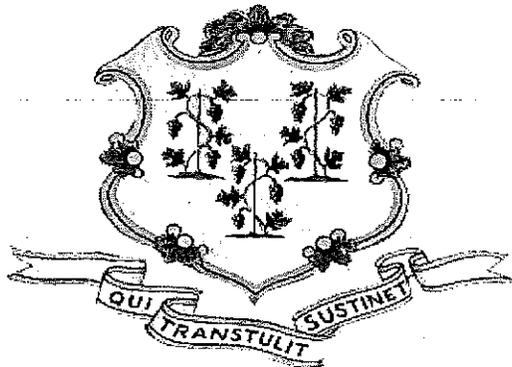
cc: Paul Potamianos, OPM
Karl Wagener, CEQ

**STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION**

RECORD OF DECISION

Prepared in accordance with the Connecticut Environmental Policy Act

NEW HAVEN-HARTFORD-SPRINGFIELD LINE HIGH SPEED INTERCITY PASSENGER RAIL PROJECT



State Project No. 170-2296

DECEMBER 2012

Participating Agencies

State of Connecticut Department of Transportation
Federal Railroad Administration
Federal Transit Administration

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Note: Because of its size, Attachment D is being provided to you on a CD. It also can be found at www.nhhsrail.com by clicking Environmental Assessment and then viewing Appendix A – Comments and Responses.

I. DECISION

The Federal Railroad Administration (FRA) and the Connecticut Department of Transportation (CTDOT) intend to continue with the implementation of the Proposed Action, which is the New Haven, Hartford, and Springfield (NHHS) High-Speed Intercity Passenger Rail Project. This decision is based upon the Environmental Assessment/Environmental Impact Evaluation (EA/EIE) (May 2012) that was prepared for the Proposed Action and the comments received during the public review period for the EA/EIE (May 8 – June 22, 2012). A copy of the Executive Summary that was included in the EA/EIE and the Errata to the EA/EIE is attached (see Attachment A).

II. STATEMENT OF ENVIRONMENTAL IMPACT

There will be no significant impacts to the environment as a result of the proposed Action. All practical means to avoid or minimize any associated environmental impacts have been adopted. The mitigation measures in the EA/EIE, and where applicable, the responses to comments have been adopted.

III. AGENCY COORINATION AND PUBLIC INVOLVEMENT

Agency coordination and public involvement for the NHHS High Speed Intercity Passenger Rail Project EA/EIE were conducted in accordance with NEPA and CEPA. These Acts and their implementing regulations require that the environmental review of any significant activity undertaken, respectively, by a federal entity or (in the case of CEPA) a state, and which has the potential to impact the environment, be prepared in consultation with appropriate agencies.

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The public has been continuously engaged since 2002 in the development of improved passenger rail service in the NHHS rail corridor. This includes public participation in the NEPA/CEPA process associated with implementation of commuter rail along the NHHS rail corridor, as well as in the 2010 Environmental Review and the current 2011 EA/EIE. During the public involvement effort for this EA/EIE, CTDOT has revisited the elected officials and managers of all of the NHHS rail corridor's towns, and has engaged the public to discuss the proposed project and receive public input. Through that effort, the proposed project has been refined to incorporate needs articulated by the towns' representatives (see Attachment B).

The public scoping process for the Proposed Action began on September 20, 2011 with the publication of the scoping notice in the Environmental Monitor (Attachment C). The purpose of the notice was to inform and solicit comments from the public concerning the Proposed Action. The public comment period for scoping ended on October 20, 2011.

A Notice of Availability for the EA/EIE, which included the notice for the three Public Hearings that were held, was advertised in the Environmental Monitor on May 8, May 22, June 5, and June 19, 2012, as well as, in the Hartford Courant, the New Haven Register, the Springfield Republican on May 8, May 15, and May 20, 2012, La Voz (a Spanish newspaper) on May 10, May 17, and May 24, 2012, and the Meriden Record-Journal on May 15 and May 20 (see Attachment C).

The EA/EIE was available for review during the entire 45-day public comment period at www.nhhsrail.com, the Connecticut Department of Transportation, the City or Town Clerk's Office and the Public Libraries

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in the affected municipalities along the rail corridor, the South Central Regional Council of Governments, the Central Connecticut Regional Planning Agency, the Capitol Region Council of Governments, and the Pioneer Valley Planning Commission.

Comments were submitted via the project website, in writing, by email, or as verbal testimony at the three public hearings. Responses to all comments received are included in Attachment D (see CD or visit www.nhhsrail.com and click on the Environmental Assessment tab and then Appendix A).

Record of Decision
New Haven-Hartford-Springfield Line
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Attachment A
Environmental Assessment/Environmental Impact Evaluation
Executive Summary and the Errata
to the Environmental Assessment

EXECUTIVE SUMMARY

ES-1.0 INTRODUCTION

Environmental Assessment/Environmental Impact Evaluation for the New Haven, Hartford, and Springfield High-Speed Intercity Rail Project

This Environmental Assessment/Environmental Impact Evaluation (EA/EIE) is a Tier 1 Service Level EA, as well as an EIE, and addresses the environmental effects resulting from implementation of the New Haven, Hartford and Springfield (NHHS) High-Speed Intercity Passenger Rail Project (the project) proposed by the Connecticut Department of Transportation (CTDOT), the project sponsor in conjunction with the Massachusetts Department of Transportation (MASSDOT). The Federal Railroad Administration (FRA), an agency of the U.S. Department of Transportation (USDOT), has awarded to the State of Connecticut three grants totaling \$190.9 million, toward the cost of designing and constructing the NHHS Rail Program improvements under the High-Speed Intercity Passenger Rail (HSIPR) Program.

The NHHS rail corridor includes 62 miles of existing rail line, owned and operated by the National Railroad Passenger Corporation (Amtrak). Historically, it included two or more tracks along its entire length, but today consists of just 23.3 miles of double track and 38.7 miles of single track. For purposes of the environmental review documented in this EA/EIE, the southern and northern termini of the NHHS rail corridor are, respectively, at Union Station in New Haven, Connecticut, and just east of Springfield Union Station in Springfield, Massachusetts (where a layover yard and maintenance facility is being proposed).

Passenger rail service in this once-robust corridor declined over the latter half of the 20th century. Currently, passenger service consists of no more than the six to eight daily round-trip passenger trains between New Haven and Springfield (with one continuing to Washington, D.C.) and a single daily round trip between Washington, D.C., via New Haven and Springfield, and St. Albans, Vermont. At New Haven, the corridor connects with Amtrak trains to New York and Boston, as well as commuter rail service operated by Metro-North Railroad to New York City and Connecticut Shore Line East service, operated by Amtrak, to New London, Connecticut.

The proposed project comprises significantly enhanced passenger rail service in the existing NHHS rail corridor, with up to 25 daily round-trip trains (up to 50 one-way trips per day) by 2030. Related operational improvements would include an increase in the capacity of the line to accommodate additional trains, an increase from a maximum of 80 miles per hour (mph) to a maximum train speed to 110 mph, service to new regional train stations, and reduced scheduled travel times. The proposed project's infrastructure improvements in the NHHS rail corridor (Figure ES-1) comprise:

- Restoration of sections of second track;
- Construction of new passing sidings;
- Construction of a layover and light maintenance facility;
- At-grade crossing upgrades;
- Facility-specific bridge and culvert rehabilitations, replacements and removals;
- Installation of new crossovers and signal upgrades;
- Improvement or relocation of existing passenger rail platforms for Amtrak intercity service, as well as additional station parking and improved station access;

- Improvements to platforms, track configuration and sidings in the Springfield Terminal area; and
- Construction of future FTA-funded new regional rail stations.

Beyond the Project, significant future investment in the NHHS corridor would be required to rehabilitate the Connecticut River Bridge between Windsor Locks and East Windsor; rehabilitation or relocation of the Hartford Viaduct, a three-bridge viaduct through the center of Hartford; upgrades in the Springfield area relating to high-level platforms at Springfield Union Station; location of a permanent layover facility; and track configuration changes in the Springfield area required to accommodate additional Amtrak trains serving the Vermonter, Knowledge, and inland Corridor. These future investments would need to be addressed in subsequent Tier 2 project-level environmental documents.

CTDOT developed this EA/EIE with close coordination and consultation with federal agencies, Massachusetts and Connecticut state-level agencies, local governments/agencies, operating railroads, stakeholder groups, and the public. Coordination included the presentation, consultation, and discussion of project attributes, alternatives, and impacts and stakeholder meetings, agency briefings, and public meetings and presentations.

FRA together with cooperating agencies the Federal Transit Administration (FTA) and CTDOT have prepared this EA/EIE pursuant to the National Environmental Policy Act (NEPA), 42 U.S.C. 4331 et seq, its implementing regulations at 40 CFR Part 1500 et seq., and FRA's Procedures for Considering Environmental Impacts (64 FR 28545). This EA/EIE has also been prepared in accordance with the Connecticut Environmental Policy Act (CEPA) Connecticut General Statutes, Sections 22a-1a through 22a-1h RA guidance on Compliance with NEPA in Implementing the High-Speed Intercity Passenger Rail Program, issued in August 13, 2009. The August 13, 2009 guidance describes service-level NEPA review for passenger rail corridor programs, such as the NHHS. The FRA has overall responsibility for facilitating completion of the environmental review process, reviewing and issuing a FONSI, if appropriate, and ensuring that CTDOT complies with all environmental review requirements.

To accommodate the flow of federal funding, the project has been divided geographically into a number of phases, as described below. The FRA previously advanced environmental review pursuant to NEPA for two of these phases – Meriden-Newington (Milepost [MP] 20.3 to MP 31.1) and Hartford-Windsor (MP 37.2 to MP 43.0) – and issued a Categorical Exclusion (CE) for both, completing the NEPA process. However, these two phases have not yet undergone CEPA review. Therefore, the potential environmental impacts of work in those two phases are documented in this EA/EIE and are subject to CEPA review. Copies of the CEs are included in Appendix 1.

The project phases comprise the following:

Phase 1 (Meriden-Newington) (CE): Using \$40 million in federal obligated American Recovery and Reinvestment Act (ARRA) funding and \$20 million in state funding, the 10.2 miles of track between Meriden and Newington will be upgraded by construction of a second track, installation of new signal and power cables and rehabilitation of bridges and culverts.

Phase 2 (New Haven-Hartford): Once the \$120.9 million in federal funding awarded in 2010 is obligated, along with a state match of \$144.8 million, the infrastructure and stations (Wallingford, Meriden, Berlin and Hartford) between New Haven and Hartford will be upgraded. The work includes installation of double track, new signal and power cables, at-grade crossing improvements, rehabilitation of bridges

and culverts and minor improvements at the Springfield Station or the Amtrak-owned Sweeney Yard site for use as a temporary layover facility.

Phase 3A (Hartford-Windsor) (CE): Using \$30 million in federal obligated ARRA funding and a state match of \$12.8 million, the railroad infrastructure between Hartford and Windsor will be upgraded. The work includes installation of double track, at-grade crossing improvements, and rehabilitation of bridges and culverts. In addition, new signal and power cables will be installed between Hartford and Springfield in order to complete installation of the cables along the entire NHHS rail corridor.

Phase 3B (Windsor-Springfield): Additional federal funding would be needed to complete the remaining improvements between Windsor and Springfield, including stations at Windsor, Windsor Locks and Springfield, and improvements to facilitate shuttle bus connections between Bradley International Airport and the Windsor Locks Station. The work includes installation of double track, at-grade crossing improvements, rehabilitation of bridges and culverts and construction of a permanent layover and light maintenance facility in the Springfield area.

Regional Rail Stations: To increase the benefits of improved rail service along the NHHS rail corridor, CTDOT intends to apply for FTA funding to add four new regional rail stations at Enfield, West Hartford, Newington, and North Haven, as well as an additional platform at the existing New Haven State Street Station.

This EA/EIE evaluates the potential environmental impacts of the proposed passenger rail service enhancement in the NHHS rail corridor and the associated capital improvements to the extent they have been defined (Figure ES-1).

The potential environmental impacts associated with the improvements evaluated in this EA/EIE are summarized in Table ES-1 (see end of this Executive Summary).

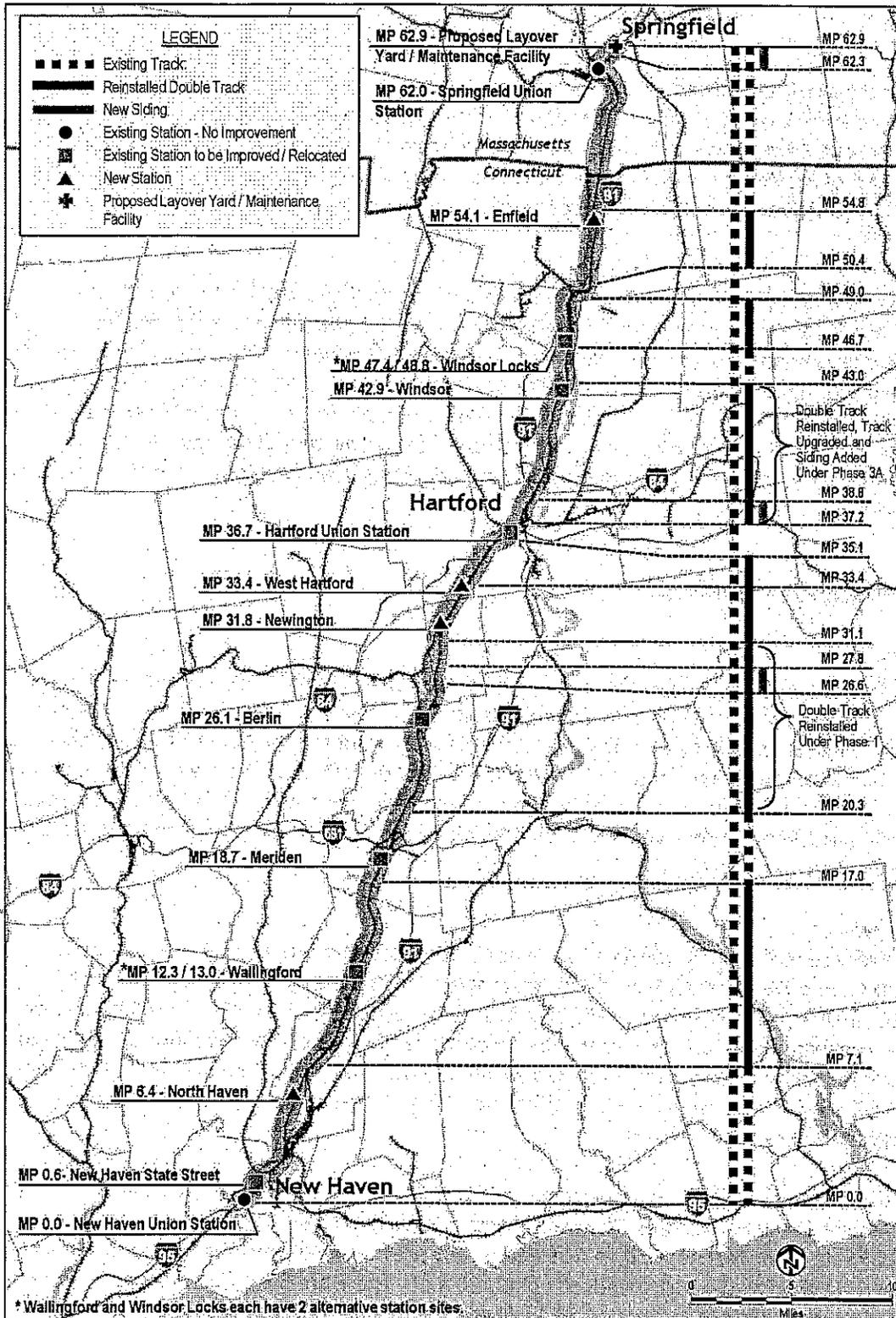


Figure ES-1 – NHHS Rail Corridor and Proposed Improvements

*Wallingford and Windsor Locks each have two alternate station sites under evaluation.

ES-2.0 PURPOSE AND NEED

Purpose

The purpose of the proposed project is to increase the safety, quality, frequency, and speed of passenger service along the NHHS rail corridor and to address the current and future transportation needs of Connecticut, Central Massachusetts, Boston and Vermont. This project is also intended to fulfill Connecticut's goal of providing additional commuting options along the corridor by increasing the number of daily trains through several of the communities along the corridor.

Need

The current rail infrastructure between Springfield, Massachusetts, and New Haven, Connecticut, is insufficient to handle the growth expected in the Northeast market. The needs for the project are the congestion, capacity constraints, and population and expected ridership growth in the project area. The Northeast market for intercity travel is estimated to reach 200 million medium-distance trips (between 100 and 400 miles) across all major transportation modes – auto, air, and rail – by 2025. With expected demographic growth, and increased capacity constraints on the study area's highways and at major airports, Amtrak's preliminary estimates are that intercity passenger rail ridership in the Northeast could double by 2030.

ES-3.0 ALTERNATIVES EVALUATION

No-Build Alternative

The No-Build Alternative assumes that passenger rail infrastructure in the NHHS rail corridor would be maintained in a state of good repair, potentially including any necessary safety and state-of-good repair improvements to the Connecticut River Bridge and the Hartford Viaduct. The No-Build Alternative also includes standard maintenance of up to 46 bridges and 115 culverts throughout the corridor that are not included in the proposed project. No restoration, improvement or new construction of passenger rail infrastructure would be undertaken in the corridor except as required by Amtrak to maintain safe rail operations.

Build Alternative

The proposed service plan would provide a one-seat ride or cross-platform transfers on service from Washington, D.C., and New York to Springfield, Boston and the Knowledge Corridor, as well as bi-directional, 30-minute peak-hour service and hourly midday service in the NHHS rail corridor. Related operational improvements include an increase in the capacity of the line to accommodate additional trains, an increase in the maximum train speed from 80 mph to 110 mph, service to future FTA-funded new regional rail stations and reduced scheduled travel times. These operational improvements, in turn, require rail infrastructure improvements.

Double Tracking: The project includes replacement of approximately 35 miles of second track that were removed by Amtrak in the early 1980s. The track, consisting of a sub-ballast foundation, wood or concrete railroad ties and steel rail, will be restored on the previously-engineered Amtrak track bed. The new track will be aligned to support speeds of up to 110 mph. There are five proposed sections of new double track, including one (MP 31.1 to MP 35.1) where the second track physically still remains, but is no longer in service and will be made serviceable:

- North Haven to Meriden (MP 7.1 to MP 17)

- Meriden to Newington (MP 20.3 to MP 31.1) (Phase 1 CE, App. 1)
- Hartford (MP 31.1 to MP 35.1)
- Hartford to Windsor (MP 37.2 to MP 43.0) (Phase 3A CE, App. 1)
- Windsor to Enfield (MP 46.7 to MP 49.0 and MP 50.4 to MP 54.8)

Amtrak also plans to widen the distance between the two tracks by two feet (to 15 feet between track centers) where feasible, to enhance ride quality and simplify track maintenance.

Existing Intercity Stations

The existing train stations at New Haven, Wallingford, Meriden, Berlin, Hartford, Windsor, Windsor Locks, and Springfield would continue to provide Amtrak intercity rail service.

New Haven Union Station: No improvements are planned at New Haven Union Station. It is anticipated that long-term (2030) additional parking demand (249 spaces) would result with enhanced service in the NHHS rail corridor (see Note 1).

Wallingford Station: The existing station location is not compatible with addition of high-level platforms, which would block local streets. Several alternative sites were considered for relocation of this station.

Both the Parker Street/North Colony Street and Ward Street/Judd Square locations are evaluated in this EA/EIE. A final recommendation for the station site will be deferred until after the EA/EIE public comment period. Both alternative sites would require some property acquisition and relocation. Additional future (2030) parking demand is estimated to be 210 spaces. Figure ES-2 indicates the mileposts for each alternative.

Meriden Station: High-level platforms and a parking structure would be added to support use of the existing station. From several conceptual station layouts presented to the City of Meriden, the preferred layout closes the Brook Street at-grade crossing and is consistent with the City's plans for TOD in the area. The improvements to the station area may require the demolition of the current Amtrak station building. Future (2030) additional parking demand is estimated to be 300 spaces.

Berlin Station: High-level platforms would be constructed near the existing station building, which would be renovated by the Town of Berlin as part of a separate project. Future (2030) additional parking demand at Berlin Station is estimated to be 232 spaces.

Hartford Union Station: At this existing station, the single 500-foot-long platform would be retrofitted or elevated to provide high-level boarding to trains. It is anticipated that future (2030) demand for an additional 342 parking spaces would result with enhanced service in the NHHS rail corridor (see Note 1, below).

Windsor Station: High-level platforms would be constructed near the existing station on Mechanic Street. Additional future (2030) parking demand is estimated to be 180 spaces; parking would be added on the east side of the tracks.

Windsor Locks Station: Two alternative station site options, each including improvements to support a bus shuttle connection to Bradley International Airport, were considered for this station. The Town of Windsor Locks has stated its preference for the location north of the Town's Central Business District, as part of a proposed renovation and expansion of the Windsor Locks Commons development and adjacent

to an existing historic station structure. Future (2030) additional parking demand at Windsor Locks Station is estimated to be 107 spaces. A final recommendation for the station site has been deferred until after the EA/EIE public comment period. Figure ES-2 indicates the mileposts for each alternative.

Springfield Union Station: High-level platforms are proposed for one or more of the platforms served by passenger trains at the existing Amtrak Springfield Union Station. Future 2030 additional parking demand resulting from enhanced service in the NHHS rail corridor is estimated at 364 spaces (see Note 1, below).

New Stations

The State of Connecticut intends to apply for FTA funding to support construction of new regional rail stations along the NHHS rail corridor to supplement planned intercity service.

New Haven State Street Station: One additional 180-foot-long high-level platform, sufficient for safe access to and from the planned two-to-three car regional trains, would be provided on the westernmost track with new (or modified existing) overhead walkway, elevator and stairs at this existing station (See Note 1 below).

North Haven Station: This proposed new station would be located at the intersection of Divine and State streets in North Haven. The site is adjacent to an existing park-and-ride lot owned by CTDOT, which would be enlarged to provide long-term future (2030) parking of up to 288 parking spaces.

Newington Station: This proposed new station would be located at the intersection of Willard and Francis Avenues on the east side of the tracks at the site of the historic station location. It would result in an intermodal site with a pedestrian overpass connecting to the proposed New Britain-Hartford Busway station located immediately west of the tracks. Future (2030) parking demand at Newington Station is estimated to be 202 spaces.

West Hartford Station: This new station would be located at the intersection of Flatbush and Newfield Avenues on the site of an unused commercial building on the east side of the tracks. It would result in an intermodal site with a pedestrian overpass connecting to the proposed New Britain-Hartford Busway station located immediately west of the tracks. Future (2030) parking demand at West Hartford Station is estimated to be 167 spaces.

Enfield Station: This new station would be located in the Village of Thompsonville at the intersection of Main and North River streets adjacent to an existing residential complex (Bigelow Commons) and a historic commercial building. Future parking demand at this station is estimated to be 214 spaces.

Layover and Light Maintenance Facility

The proposed project includes construction of a train layover and light maintenance facility in the Springfield area. The facility, required to support the planned 2030 service level, would be used for overnight storage, cleaning, and light maintenance of three regional trains.

Note 1: The project will not be constructing additional parking capacity at New Haven Union Station, New Haven State Street Station, Hartford Union Station, or Springfield Union Station. At these stations, parking will continue to be provided by the local parking authority. The parking capacity added by the local parking authorities may be phased over several years to meet the parking demand as it develops.

Three sites were considered for the Springfield layover and maintenance activities. On the basis of the qualitative analysis of the three potential sites, the Armory Street site was selected as the best long-term location to support the 2030 level of service. Access to the site will require construction of a new 2,000-foot-long access track from Springfield Union Station to the layover area along the former branch line. The site has sufficient space for the proposed facility and for potential future expansion. The other two sites – the Amtrak Sweeney Yard Site south of Springfield Union Station, and a site along tracks within Springfield Union Station -- would be sufficient for interim layover and maintenance needs but are too constrained to support service needs for the full 2030 NHHS project.

Sidings

Construction of three new railroad passing sidings is included in the proposed project. The sidings would run parallel to the main line tracks and be used to hold freight trains to avoid delays to passenger trains. The sidings would be located as follows:

- Berlin Siding (MP 26.6-27.8): This siding, which would not be required until 2030 service levels are achieved, would reduce train conflicts south of Hartford for Connecticut Southern Railroad (CSO) trains serving local area shippers.
- Hartford Yard Siding (MP 37.2-38.8): This siding would be located within the existing Hartford Railroad Yard and provide storage for freight trains operating to and from the yard and adjoining branch lines and turning passenger trains.
- Armory Street Siding (MP 62.3-62.9): This siding, consisting of construction of a parallel track, would provide access to the proposed Springfield layover and light maintenance facility, described above. This siding would be constructed only if the Armory Street site is selected as the location for the permanent layover and light maintenance facility.

Bridges and Culverts

On the basis of condition assessments and in-depth inspections of the bridges and culverts in the NHHS rail corridor, each structure's condition was rated as excellent, good, fair, poor, serious or unknown (the latter if access was not available or the structure could not be located) and an appropriate improvement was recommended for each structure, reflecting the specifics of its condition.

Based on that analysis, the proposed project includes improvements to 42 structures under the tracks called "under-grade bridges," four overhead bridges and 61 culverts located across the NHHS rail corridor. The remaining bridges and culverts in the corridor are not included in the proposed project's program of infrastructure improvements because they require only routine maintenance or no action.

Applicable Federal Regulations, Required Coordination and Permits

National Environmental Policy Act (NEPA): This environmental document has been prepared in accordance with NEPA and its implementing regulation. If it is determined that no significant impacts would result from the proposed project, a Finding of No Significant Impact (FONSI) will be issued by the FRA, the lead federal agency. If significant impacts will occur, further environmental analysis may be required.

U.S. Army Corps of Engineers (ACOE): The ACOE has jurisdiction, under the Federal Water Pollution Control Act or Clean Water Act of 1972 to regulate discharge of dredge or fill material into all waters of the United States including open water, inland wetlands, and tidal wetlands. The ACOE coordinates the issuance of a Section 404 wetlands permit with the State of Connecticut Water Quality Certification in accordance with Section 401 of the Clean Water Act.

Hazardous Materials Regulations: Risk sites, regulated by federal and/or state rules and regulations, may be located along the rail corridor. CTDOT Task 210 procedures would be implemented during the final design phase of the project for areas on or adjacent to identified high-risk sites.

Historic Preservation Act (Section 106): Section 106 of the National Historic Preservation Act of 1966 requires that federal agencies consider the effect of their undertakings on historic properties.

Section 4(f): Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of publicly owned parks, recreational areas, wildlife and waterfowl refuges and public and private historic properties unless the responsible USDOT agency makes two findings: 1) that there is no feasible and prudent alternative that avoids the use of Section 4(f) properties; and 2) that the project or action includes all possible planning to minimize the harm that would result from the use of those properties.

Section 6(f): Section 6(f) of the Land and Water Conservation Fund Act (LWCFA) requires that property acquired or developed with LWCFA funding not be used for any purpose other than public outdoor recreation without the approval of the Secretary of the U.S. Department of the Interior.

Public Health Service Act (Safe Drinking Water Act): The 1986 Federal Safe Drinking Water Act grants primary authority to the states for adoption and enforcement of regulations for the protection of water systems and supplies.

Federal Uniform Relocation Assistance and Real Property Acquisition Policy Act of 1970: Affected property owners would be afforded relocation assistance through the Federal Uniform Relocation Assistance and Real Property Acquisition Policy Act of 1970. CTDOT is authorized and required to provide monetary and other relocation assistance to displaced property owners whose properties would be acquired for implementation of the proposed federally funded project.

Executive Orders: The federal Executive Orders listed below must be taken into consideration as part of the evaluation of each alternative:

- Executive Order 11990 mandates that federal agencies ensure preservation and enhancement of wetland resources;
- Executive Order 11988 directs federal agencies to take appropriate action to minimize flood hazards and impacts resulting from modifications to floodplains; and
- Executive Order 12898 requires federal agencies to ensure that their programs, policies and activities do not result in disproportionately high or adverse human health or environmental effects on minority or low-income populations.

Other Coordination Requirements

- National Railroad Passenger Corporation (Amtrak) Coordination: The NHHS rail corridor includes 62 miles of existing rail line, owned and operated by Amtrak, currently operates regional passenger service between New Haven, Connecticut, and Springfield, Massachusetts. Coordination with Amtrak is required during the planning, design and construction phases of the project.
- Rail Freight Operations Coordination: The proposed project requires coordination with rail freight operators to avoid adverse impacts to their operations during construction and as additional passenger rail service is phased in. This includes the following freight railroads: CSX

Transportation, Connecticut Southern Railroad, Pan Am Railroad, Providence Worcester Railroad and the Central New England Railroad.

Applicable State Regulations, Required Coordination and Permits

Connecticut Environmental Policy Act (CEPA): This environmental document has also been prepared in accordance with CEPA - Connecticut General Statutes (CGS), Sections 22a-1a through 22a-1h and thus serves as an EIE under CEPA review. Connecticut's Office of Policy and Management will make a determination of adequacy as required by CEPA.

Massachusetts Environmental Policy Act (MEPA): The MassDOT has determined that the work proposed by the NHHS Project in Massachusetts does not trigger any thresholds under the Massachusetts Environmental Policy Act (MEPA) and therefore is not subject to review under MEPA.

The following regulations are applicable to the surface water resources and groundwater resources throughout the study corridor and to the proposed project:

- Connecticut Surface Water Quality Standards (Connecticut Department of Energy and Environmental Protection [CT DEEP], Effective February 25, 2011);
- State of Connecticut Integrated Water Quality Report (Draft, April 11, 2011); and
- CT DEEP Groundwater Quality Standards (Effective April 12, 1996)
- Massachusetts Surface Water Permit Discharge Program, 314CMR3.00 and 4.00

Principal applicable state law concerning the proposed project's impact to wetlands is as follows:

- The Connecticut Inland Wetland and Watercourses Act (CGS Section 22a-36 through 22a-45a, inclusive);
- The Connecticut Coastal Management Act (CCMA) (CGS Sections 22a-90 through 22a-112, inclusive); and
- The Massachusetts Wetlands Protection Act (MGL Chapter 13 Section 40).

The following regulations are applicable to the consideration of wild and scenic rivers, navigable waterways, and coastal resources in the NHHS rail corridor:

- Wild & Scenic Rivers Act, October 2, 1968;
- Navigable waterways of the United States are defined (33 CFR Part 329) as "those waters that are subject to the ebb and flow of the tide and/or are presently used, or have been in the past, or may be susceptible for use to transport interstate or foreign commerce;
- Navigable waterways are also regulated by the CT DEEP, and bridges that cross them may be regulated by the U.S. Coast Guard;
- Connecticut's Coastal Management Act; and
- Section 22a-94 of the Connecticut General Statutes, in which coastal waters are defined by the state as those waters of Long Island Sound and other associated waters that contain a salinity of at least 500 parts per million (ppm) under low-flow stream conditions.
- Massachusetts Rivers Protection Act as Amended, 1996

In addition, the following regulations are applicable to the proposed project's effects on floodplains and floodways and Connecticut-designated stream channel encroachment lines (SCEs):

- Sections 25-68b through 25-68h, inclusive, of the CGS, Connecticut's Flood Management Program. This program, administered by the CT DEEP, regulates state agency actions affecting floodplains and natural man-made storm drainage facilities. Agencies undertaking such actions must submit a Flood Management Certification (FMC) describing the project activities and the measures taken to meet the program's standards. Under recent provisions, project-related improvements that result in the loss of flood storage capacity may be required to provide flood storage compensation.
- The Connecticut SCEL program, administered by the CT DEEP, regulates activities within designated SCELs and issues permits only if there is a clear demonstration that the project would not cause an increase in flood hazard or other adverse effects.

State laws governing review of the proposed project's effects on threatened and endangered (T&E) species are as follows:

- The Connecticut Endangered Species Act (CGS 26-303) declared a policy of the State to conserve, protect, restore, and enhance any endangered or threatened species and essential habitat. The act requires that any action authorized, funded or performed by a state agency not threaten the continued existence of any endangered or threatened species or result in the destruction or adverse modification of habitat designated as essential to such species, using the best scientific data available.
- Massachusetts Endangered Species Act (310 CMR10:00)

MassDOT Coordination: The NHHS rail corridor includes 62 miles of existing rail line, of which 6 miles are in Massachusetts. Since the early inception of this NHHS Project the CTDOT and MassDOT have worked cooperatively in project planning and project development activities. Consistent with *Vision for the New England High-Speed and Intercity Rail Network*, MassDOT acknowledges CTDOT's lead on this project which is primarily in Connecticut. Coordination with MassDOT is required during the planning, design, and construction phases of the project. Massachusetts agreed to cooperate on the project in the above referenced vision through the following statement: "Through Connecticut's leadership, we understand the importance of restoring the double track and replacing bridges on the Springfield Line that serves the cities of New Haven, Hartford, and Springfield in order to provide the foundation of the larger rail network."

ES-4.0 AFFECTED ENVIRONMENT

Introduction

In order to accurately assess the potential impacts associated with the proposed project, a corridor approximately 1 mile wide was used for Community Facilities, Median Household Income, Minority Population, Ground Water, and Surface Water, while a corridor approximately 1,000 feet wide was used for Farmland Soils, Hazardous Materials, Cultural Resources, Section 4(f) and 6(f) Resources, Floodplains and Stream Channel Encroachment Lines, Noise and Vibration, Wetlands, Critical Environmental Areas and Threatened and Endangered Species. A study area of up to a 5-mile radius at each station was used to assess Socioeconomic and including environmental justice.

Table ES-1 Summary of Environmental Consequences & Mitigation, located at the end of this Executive Summary, identifies the project's potential impacts and associated mitigation.

PHYSICAL ENVIRONMENT

Air Quality

The proposed project would not result in any local or regional short-term or long-term adverse air quality impacts. As the proposed project would comply with current control measures and would be consistent with emissions budgets, it is determined to be in conformity with the Clean Air Act, as amended, pursuant to all applicable U.S. Environmental Protection Agency (EPA) regulations.

For transportation projects, the criteria pollutants of greatest concern are carbon monoxide (CO) and ozone as they are predominantly influenced by motor vehicle activity. Particulate matter smaller than or equal to 2.5 microns in size (PM_{2.5}) is also a key pollutant because of the relative proximity of the study corridor to the New York Metropolitan Area, which is in nonattainment. In the past 9 years, overall trends in annual concentration of PM_{2.5} have shown a downward trend (with the exception of a slight increase in 2005). None of the improvements with the proposed project would cause or contribute to any new violations or increase the frequency or severity of any existing CO violations in CO nonattainment or maintenance areas.

Beginning in March 2012, a quantitative PM_{2.5} hot spot analysis will be required; at that time, the EPA MOVES model will be required as the industry standard. The new MOVES model will be a more stringent analysis (incorporating more variables) than the model in use today. Consequently, based on application of the new, more stringent analysis, the potential for PM_{2.5} hot spots along the study corridor from the increased rail activity with the proposed project could theoretically be higher than that with the No-Build Alternative. However, the most likely scenario is that emissions and hot spots would be less likely along the study corridor in the future because of the overall significant reductions in emissions projected by EPA for the study corridor and the region.

Noise and Vibration

Without mitigation, train-horn noise at grade crossings and existing and new regional rail stations would result in potentially severe noise impacts. However, designation of Quiet Zones, which eliminates or significantly reduces horn-noise impacts at public crossings, could be used to mitigate severe train-horn noise impacts. As part of the proposed project, Amtrak will add supplemental safety devices at all public crossings along the NHHS rail corridor, as necessary, to meet Quiet Zone requirements and to provide additional safety protection to prevent motorists from attempting to drive around the gates. This would enable the affected municipalities and Amtrak to jointly apply to FRA for Quiet Zone designations. Other options for horn noise mitigation include wayside horns, barriers, or insulation.

CTDOT will evaluate potential vibration-sensitive receptors on a case-by-case basis and determine the impact and the need for mitigation, based on the proposed project's track configuration determined during the final design phase or project development.

Additional noise analysis would be required when the permanent layover/maintenance facility is advanced to Tier 2 environmental documentation to support the 2030 level of train service, as there are residences within 250 feet of the preferred alternative's proposed facilities and tracks. Overall, as the project advances to final design, mitigation options will be evaluated to determine their appropriateness and feasibility.

Topography and Geology

Overall, the topography, geology, seismicity, and soils within the study corridor are stable and suitable for the proposed project, which would not result in impact to topography or geology. During the design phase of the proposed project, more detailed geotechnical analyses would be performed, including test borings along the corridor to enable the final design to accommodate all of the conditions encountered at specific locations of construction. Details of this analysis are provided in the Environmental Resource Analysis reference document.

Floodplains and Stream Channel Encroachment Lines

Impacts to 100-year floodplains would total approximately 10.7 acres of encroachment. Most of this acreage would be associated with improvements at existing stations and construction of new stations. Other impacts would result from restoration of double track and construction of the new rail siding. Some of the station work in floodplains would be for surface parking lots, which would be constructed to match existing grades, to the extent possible, thereby minimizing fill volumes in the floodplain, which could otherwise cause adjacent flooding effects. Proposed parking garages and overpass structures at the stations could also reduce the flood storage capacity of the floodplains. Additional encroachment impacts would affect approximately 1.0 acre of floodways and approximately 2.1 acres of SCELs due to the restoration of double track and construction of the new rail siding.

CTDOT will be required to secure a FMC from the CT DEEP for all work involving impacts to the 100-year floodplain or floodways in Connecticut. In addition, a SCEL permit will be required from the CT DEEP. For any locations where flood storage volumes and/or flooding are projected to be adversely affected, compensatory mitigation will be required. This may involve creation of new flood storage capacity to offset lost flood storage, provided either at or immediately adjacent to the site of the impact.

Relative to Executive Order 11988 on floodplain management, every effort will be made to avoid project-related impacts to floodplains. However, in some cases, there may be no practicable alternative to encroachment on 100-year floodplains and floodways. Impact avoidance, minimization, and mitigation measures will be investigated and implemented, as appropriate, in conformance with Federal Emergency Management Agency (FEMA) and State of Connecticut regulations.

NATURAL ENVIRONMENT

Critical Environmental Areas and Threatened and Endangered Species

A variety of T&E species and/or their habitats occur near many of the proposed project's improvements in Connecticut. As many as 18 Connecticut-listed species are located in the vicinity of several regional rail station sites and double-tracking segments in Connecticut, indicating potential impacts along the corridor. There would be a possibility of additional impacts at the bridge and culvert repair/replacement sites in Connecticut once specific locations are identified. Impacts to the state and federally endangered dwarf wedge mussel and Massachusetts-listed species and/or habitats along the Connecticut River are not anticipated. No species or habitats of concern were identified near the proposed Armory Street site being considered for a layover/maintenance yard in Springfield. The other layover/maintenance sites under consideration (Springfield Union Station and the Sweeny site) would require virtually no improvements. Therefore, no impacts to T&E species and/or their habitats would result from the Massachusetts portion of the project.

As the project design advances, additional coordination with the CT DEEP will be required to determine whether the species and habitats of interest actually occur at the specific improvement sites and to

identify the need for field surveys and avoidance and/or protective measures for the particular location(s). Based on the results of this coordination, field studies may be required prior to final identification of impact avoidance and minimization measures.

Water Resources and Water Quality

No impacts to groundwater are anticipated with the proposed project. Some potential exists for adverse impacts to surface waters from changes in stormwater flows from impervious surfaces and erosion and sedimentation during the period of active construction. There may be temporary impacts to some surface waters during construction, particularly during repairs and/or replacement of culverts and bridges. The installation of the proposed new siding has the potential to affect Piper Brook near the Newington/New Britain Town Line. During final design the track length, track centers, and track bed will be adjusted to minimize impact to this waterway and its tributary.

In order to avoid or substantially reduce potential water quality impacts associated with the proposed project, design details will be developed to avoid adverse impact. Final designs will be coordinated and permitted with the CT DEEP and Massachusetts Department of Environmental Protection (MADEP) and other resource agencies.

Wetlands

The wetland mapping from available GIS data indicates 40 wetland systems in the study corridor from Hamden, Connecticut, to Longmeadow, Massachusetts. Restoration of double-tracking and construction of rail sidings would directly impact approximately 3.9 acres of wetlands along the NHHS rail corridor in Connecticut; .6 acre due to proposed sidings and .7 acre due to proposed double track in this EA/EIA and 2.6 acres due to proposed double track in the CEs for Phases 1 and 3A. No wetland impacts are anticipated in Massachusetts.

Potential indirect impacts to off-site wetlands, particularly from stormwater runoff, would be negligible due to application of pertinent design and construction standards during later design phases of the proposed project. Some as-yet-undetermined level of wetland impact is anticipated from culvert and bridge repairs or replacements; as the proposed project's design progresses, any direct wetland impact that cannot be avoided will be minimized through design measures, to the greatest extent practicable. Based on conceptual layouts, no wetland impacts are anticipated from improved or new stations.

All potential wetland impacts from restoration of double-tracking, construction of rail siding and bridge/culvert rehabilitation or replacements will be mitigated. Compensatory wetland mitigation will be provided through a wetland mitigation plan developed in coordination with the ACOE and the CT DEEP and following the guidelines set forth in the ACOE *New England District Compensatory Mitigation Guidance* (July 20, 2010). While the priority mitigation options are wetland restoration and creation, the mitigation package may include a combination of restoration, creation, enhancement and preservation to adequately compensate for the lost acreage, types and functions-values of the impacted wetlands.

Wild and Scenic Rivers, Navigable Waterways, and Coastal Resources

None of the watercourses within the study corridor are included in the National Wild and Scenic Rivers System or are currently under consideration for such a designation. Navigable waterways that cross the study corridor include major rivers, tidal waters and tributaries to the river's head or upper limit of tide and include the Mill, Quinnipiac and Connecticut rivers. Only portions of the City of New Haven and Towns of Hamden and North Haven in the NHHS rail corridor lie within Connecticut's designated coastal zone. However, tidal influences along the Connecticut River extend as far north as South Windsor.

Restoration of double track through Windsor Locks between the existing track and the Connecticut River would be done in coordination with the U.S. Coast Guard, ACOE and the CT DEEP; final track design will locate the track improvements to avoid encroaching on the Connecticut River, designated a National Heritage River. The proposed project includes no improvements to the Connecticut River Bridge.

Most of the NHHS rail corridor within Connecticut's coastal boundary is already double-tracked with the exception of a small, single-track segment in North Haven. Restoration of the second track would occur within the existing railroad track bed and consistent with the transportation use of the existing facility. Accordingly, no impact to coastal resources is anticipated with the proposed project. Potential impacts to navigable waterways and coastal resources will require further assessment and agency coordination to determine avoidance/minimization measures. This assessment will occur during project design and permitting.

Prime Farmlands and Farmlands of Statewide Importance

The proposed project would have no impacts to prime and statewide important farmland soils as a result of construction of the Springfield layover area or station improvements. Restoration of double track and new sidings could impact up to 4.0 acres of prime and statewide important farmland soils in the event track centers and track bed shoulders are widened.

Once the direct impacts are quantified, a Farmland Conversion Impact Rating Form (Form AD-1006) will be completed in coordination with the Natural Resources Conservation Service (NRCS). The NRCS will evaluate the information using a land evaluation and site assessment (LESA) system to establish a farmland conversion impact rating score. From this, mitigation will be determined. Farmland conversion mitigation may include paying a fee to protect farmland or providing permanent protection of comparable farmland.

HUMAN ENVIRONMENT

Land Use and Zoning

The proposed project would result in no direct or indirect land use impacts associated with non-station area improvements. No direct impacts would result from the proposed improvements at six station locations. At the remaining stations, direct impacts would be either neutral or positive and could include complementing TOD plans and other improvements planned by others for the station areas, while increased activity at the stations could result in direct adverse impacts to access to surrounding land uses.

The local municipalities and CTDOT agreed upon the following ongoing considerations for final station design to ensure the compatibility of rail station design with local future land use plans:

- The Meriden station parking will be located to integrate into the TOD plan for the area.
- Parking for the Berlin station will be designed to be integrated into the TOD plan for the area.
- There will be ongoing coordination with the City of Hartford to respond to parking demand for rail patrons as City redevelopment plans for the area take shape.
- CTDOT will work with the Town of Enfield and Bigelow Commons' ownership to co-locate a portion of the Enfield Station parking within the Bigelow Commons development; minimize use of riverfront access for parking; and, at the same time, work to minimize adverse effects to access or parking for Bigelow Commons residents.

Consistency with State, Regional and Local Plans

Based on a review of local, regional, and state planning documents, the implementation of new and improved passenger rail service would be consistent with the stated goals, objectives, policies and actions of the state, regional and all but two local plans. Two station location options for both the Towns of Wallingford and Windsor Locks are being carried forward. The Towns have elected to postpone a final site selection until after the EA/EIE public hearings.

Property Acquisitions and Displacement

The proposed project would result in acquisition of approximately 31 properties (42.2 acres), comprising 18 full property acquisitions, and 13 partial property acquisitions required for improvements to existing stations and to construct new stations. This number may change slightly depending on selection of the preferred station site alternatives at Wallingford and Windsor Locks, and on the selection of a site for the permanent layover and light maintenance facility. Additional minor property acquisitions could be required for track, at-grade crossing, and bridge and culvert improvements. Mitigation will consist of monetary and other relocation assistance to displaced property owners.

Socioeconomics

Project-related impacts on socioeconomic conditions would be beneficial. The proposed project would not result in any adverse socioeconomic impacts. Beneficial regional and national economic impacts would result due to job creation; near regional rail stations in the NHHS rail corridor, beneficial economic impacts would result from project-related induced development opportunities. Injection of capital infrastructure spending into the regional economy would lead to jobs directly related to the construction of the proposed improvements and, potentially, additional jobs for the suppliers of materials and equipment and for related professional services. In turn, these jobs would support additional jobs made viable through the improved access by rail (induced impacts), all of which would bolster the regional economy by increasing economic growth.

Following the initial construction/capital investment, there would be ongoing operations and maintenance expenditures for the constructed facilities, equipment and associated services. Operations and maintenance contracts would include the hiring of employees and purchase of supplies and services, which would also result in positive economic impact. These direct expenditures give rise to multiplier effects for the estimation of the total economic impact.

In addition to the beneficial impact of job-years, the proposed improvements would have a significant potential for beneficial economic development in the cities and towns with regional rail stations. As the proposed project would not result in any adverse socioeconomic impacts, no mitigation is proposed.

Community Resources and Neighborhood Character

There is some potential for a mix of adverse and beneficial impacts to some study area neighborhoods due to changes in motor vehicle access, access to services and other businesses, noise, and neighborhood visual and physical cohesiveness. The improved rail corridor would result in a substantial increase in the number of trains along the rail line. This could result in some adverse impact on neighborhood noise levels and on interaction in those areas where closure of at-grade crossings may become more frequent and opportunities to cross the tracks may be reduced.

Station locations where the proposed project would have adverse impacts to neighborhood cohesion include: Wallingford (both alternative sites), Newington, West Hartford and Enfield. Potential adverse impacts will be mitigated through ongoing coordination with the affected communities and

representative neighborhood organizations during final design to offset those impacts to the extent feasible and practicable.

Visual Resources and Quality

The NHHS Rail Corridor has served as an active rail line for over 160 years, from the earliest days of the steam era to today. Many railroad facilities – stations, water towers, signal posts, tracks – have been built, removed or replaced as railroad technology has changed and communities have grown around the corridor. The NHHS Rail Project would add new railroad platforms, a pedestrian overpass and new parking facilities at existing and proposed new train stations. These same amenities exist at nearly all of the existing rail stations in Connecticut along the Northeast Corridor rail line. Addition of these improvements has the potential for adverse impacts to the visual environment in the vicinity of the existing and proposed NHHS rail stations at Wallingford (Parker Street alternative), Berlin, Newington, Windsor, Windsor Locks, and Enfield. The impacts will be mitigated through landscaping and use of building construction materials, colors and architectural styles consistent with station sites' surroundings, to the extent possible.

Cultural Resources

The NHHS rail corridor runs through an area of southern New England that has been the scene of human habitation for some 12,000 years. Consequently, the area is rich in pre-Colonial archaeological sites, historic-period archaeological sites, historic districts and individual historic properties. The exact nature of impacts on these resources, as well as archaeological resources that may be located in the corridor, has not yet been identified or fully evaluated at this time because some of the proposed project's infrastructure improvements have been only conceptually designed or have not yet advanced to that stage. A Draft Programmatic Agreement (PA) among the FRA, CTDOT, Connecticut State Historic Preservation Office (CTSHPO), Massachusetts State Historic Preservation Office (MASHPO), and other interested parties is being developed in accordance with 36 CFR § 800.4(b)2. The PA implements a phased process for further consultation among FRA, CTDOT, CTSHPO, MASHPO, and other interested parties including Native American Tribes; identification of historic properties that may be affected by individual elements of the proposed project; and resolution of all adverse effects to historic properties that may result with the proposed project.

It is the CTSHPO's opinion that the entire NHHS rail corridor in Connecticut is eligible for listing in the National Register of Historic Places (NRHP) as a thematically linked historic district. MASHPO has concurred with the concept of the entire historic rail corridor as a linear historic district. The proposed project may require physical alteration or demolition of historic resources that are individually eligible for listing in the NRHP or contribute to the significance of the National Register-eligible linear historic district. Further evaluation of the potential effects of the proposed project will be completed in accordance with Section 106 of the National Historic Preservation Act under the terms of the PA.

Section 4(f) Resources

There are 109 publicly owned parks and recreational areas in addition to wildlife and waterfowl refuges in the NHHS rail corridor. None would be impacted by the proposed project. However, the NHHS rail corridor is rich in historic districts and individual historic properties that are listed on the NRHP or have been determined to be National Register-eligible. The proposed project may require physical alteration or demolition of some historic resources. The exact nature of impacts has not yet been identified because some of the proposed project's infrastructure improvements have been only conceptually designed or have not yet advanced to that stage.

Section 6(f) Resources

Section 6(f) resources are municipal parks or open space properties that have either been purchased, maintained or enhanced with funding from the 1965 LWCFA. Bushnell Park, located in Downtown Hartford, is the only Section 6(f) resource in the study corridor. The proposed project would not impact this resource. Since there would be no impacts to the Section 6(f) resources, no mitigation is warranted or proposed.

Transportation

Railroad: A detailed NHHS operations modeling simulation was performed to determine the projected performance of the future freight and passenger service, compared to the existing service. With the proposed project, freight service tonnage would be allowed to grow at 1.5 to 2 percent (1.75 percent average) compounded annually. Passenger train speed limits would be increased to a maximum of 110 mph at selected locations. During final design, the schedules for the future passenger and freight service will be optimized and an overall robustness study of the operating infrastructure would be completed for the corridor to validate infrastructure location.

Traffic: With implementation of the proposed project, adverse traffic impacts would occur at the following locations where traffic LOS would fall below the criteria for acceptable traffic flows.

Traffic impacts at the affected station access driveways and intersections near the stations:

- Route 173/Francis Avenue/Station access (Newington)
- Asylum Avenue/Spruce Street/Station Access (Hartford)

Traffic impacts at the affected grade crossings:

- Route 150/Hall Avenue/N. Cherry Street (Wallingford)
- Quinnipiac Avenue/N. Cherry St. (Wallingford)
- Quinnipiac Avenue/Hall Avenue/North Colony Street/Center Street (Wallingford)
- Route 150/Hall Avenue/Washington Street (Wallingford) - Signal timing changes.
- North Colony Street/Parker Street (Wallingford)
- North Colony Street/Ward Street (Wallingford)
- Route 140/Bridge Street/Route 159/Church Street (Windsor Locks)
- Main Street and Spring Street (Windsor Locks)
- Main Street and Church Street/Bridge Street (Windsor Locks)

Transit, Parking, and Non-Motorized: At Meriden Station, there are two tracks at the station, requiring passengers to cross the active track to access the train. This increases boarding time and can cause delays to other trains operating in the vicinity. The increase in train service will increase the number of passengers arriving and departing at each station and, in turn, increase the need for timed connectivity with local transit, as well as with commuter and Amtrak trains at New Haven connecting to points south/west and north/east. Parking at existing stations is not adequate to support the projected ridership for 2030.

Project related enhancements would include increased connections to and from local bus transit by providing two to four bus stalls at each rail station, with two additional stalls at the Windsor Locks Station to provide an express shuttle to Bradley International Airport. Bus stalls for private services at

the current rail stations would be maintained. Access to the New Britain – Hartford Busway buses will be integrated into the station designs at Newington and West Hartford. Parking will be increased at all stations except New Haven Union, New Haven State Street, Hartford Union and Springfield Union. At these stations, parking will continue to be provided by the local parking authority. All stations that are reconstructed, relocated, or newly constructed with the proposed project will provide Americans with Disabilities Act (ADA)-accessible routes from the existing sidewalks at the edge of the respective station to the boarding platforms. Similarly, the sites will be designed to provide access by bicycle on the station drives and bicycle storage. High-level platforms, connected by an aerial pedestrian overpass, will provide safe access to the platforms and the trains. All stations will receive high-level platforms and pedestrian overpasses except Springfield, Hartford, which will receive high-level platforms only and New Haven, where they already exist.

Public Utilities and Energy

The proposed project may require some utility relocation during project construction, which would be coordinated with utility providers to eliminate or minimize disruptions. Potential disruption to utility customers would also be minimized through coordination with utility providers. The proposed project would have a positive impact on energy requirements as increased regional rail ridership would result in a reduction in personal automobile usage and reduced fossil fuel consumption.

Overhead utilities at the Newington and North Haven stations may have to be raised to clear the proposed pedestrian crossover. Level 3 fiber optic cables running within and along the NHHS rail corridor would have to be replaced. In addition, Amtrak intends to install new power, signal, and communications cables along the west side of the corridor. CTDOT and MASSDOT have met with utility owners along the corridor to advise them of the project and to seek as-built and other design plans to help identify the location of utility crossings and their depth. As design of the improvements advance, CTDOT will work with the utility owners to optimize the scheduling of utility relocations.

Based on the preliminary Passenger Service Plan, the proposed project would result in a total reduction of 92.65 million miles in vehicle miles of travel (VMT) of light-duty vehicles and an increase of 760,000 gallons of diesel fuel used for train locomotion. Overall energy consumption would be reduced with increased regional rail ridership, particularly during peak hours of travel. The resulting reduction in regional consumption of fossil fuels would reduce greenhouse gas emissions.

Utility service disruptions during construction will be minimized through close coordination of construction activities, scheduling with utility providers, and advanced notice of any anticipated outages to nearby customers.

Hazardous Materials and Environmental Risk Sites

Hazardous waste sites were identified using the EPA's 2002 Comprehensive Environmental Response, Compensation, and Liability Information System (CERCLIS) GIS coverage for towns located in the NHHS rail corridor in Connecticut and Massachusetts. Based on the analysis, the rail corridor contains 19 CERCLIS-listed sites. In addition, 44 other potential hazardous waste sites were identified from CT DEEP Landfill, Leachate, and Waste Water Discharge GIS data layers.

The CERCLIS listed sites along with the other sites noted above are collectively called environmental risk sites – locations where hazardous materials are known to have been used and/or hazardous waste generated and potentially discharged to the ground or water.

- There is a high risk for encountering contaminated soils or debris in the existing railroad track bed during project construction. Along the existing railroad track bed and in the vicinity of the Springfield Layover site, there is a strong probability for encountering contamination during project construction. This work would be in future Tier 2 environmental documents.
- Older building structures in the NHHS rail corridor requiring demolition can contain lead and asbestos.
- The property at 17–35 Bartholomew Avenue in Hartford, which is listed as a brownfields site and is near a proposed section of double tracking to be restored, has a probability of the presence of oil and hazardous materials OHMs.
- There is a potential hazardous waste source on the site of the proposed North Haven Station consisting of treated industrial discharge from a chemical company.

For identified potential environmental risk and hazardous material sites in Connecticut, State of Connecticut regulatory requirements will be followed by CTDOT through its environmental compliance process as the proposed project progresses to final design and construction. All Massachusetts work would be in future Tier 2 environmental documents.

Safety and Security

The proposed project would not appreciably impact public health, safety and security in the NHHS rail corridor. While greater frequency of trains may increase the frequency of opportunities for conflict between trains and vehicles or people, safety improvements at crossings and improved communications among emergency responders would be a beneficial impact, serving to minimize potential conflicts and their consequences. Safety and security design features at the stations would also have a beneficial impact. An increase in rail service is expected to divert some vehicular traffic to the rail mode in the region and, thus, would indirectly improve safety on roads and highways. Implementation of the proposed project will conform to all applicable safety requirements, regulations, standards and certifications and a comprehensive NHHS System Safety Program (SSP) will be developed for the proposed project.

The NHHS Rail Program will conform to all applicable FRA, FTA, Occupational Safety and Health Administration (OSHA), Amtrak and state safety and security requirements, regulations, standards, and certifications. These measures will be incorporated into a comprehensive NHHS System Safety Program (SSP) that ensures the development and coordination of responsibilities for implementing key safety and security policies.

Environmental Justice

Concentrations of Environmental Justice (EJ) populations (minority and/or low-income) exist throughout the NHHS rail corridor study area, notably in the larger cities of New Haven, Hartford and Springfield. There are no EJ populations in the portions of the study area in Newington and Windsor Locks, or near the Springfield site of the proposed train layover/maintenance facility.

The proposed NHHS rail corridor service enhancements would have an indirect beneficial impact on EJ populations in the vicinity of improved existing stations and relocated and proposed new stations. The proposed project would provide new or improved access to regional rail transit services with station locations nearer to some EJ populations, thereby improving mobility options for those who are transit-dependent for work-related and other travel. It is anticipated that EJ populations near New Haven Union Station, Meriden, West Hartford, Hartford, and Springfield Union Station would be most beneficially impacted.

Adverse impacts due to traffic and noise that would affect EJ populations will be mitigated in the form of intersection improvements, Quiet Zones and potential noise insulation of some homes. As no other significant adverse impacts to EJ populations are anticipated, no additional mitigation is warranted or proposed.

Secondary and Cumulative Impacts

Secondary Impacts: Secondary impacts are those that are caused by the project and are later in time or farther removed in distance than are direct impacts, but are still reasonably foreseeable. Secondary impacts associated with the proposed project's improvements to existing passenger rail stations and introduction of new regional rail stations are expected to be generally beneficial and occur primarily from induced development. Increased human activity (such as pedestrian circulation) associated with a rail station can create a positive economic climate within which businesses want to locate. Such development and redevelopment can be expected to be stimulated in the vicinity of new or significantly upgraded station locations. Secondary impacts are anticipated to be mostly beneficial effects resulting from:

- Improved access and connectivity within the NHHS rail corridor, the New England region and its communities;
- Improved air quality from reduced traffic volumes;
- More employment opportunities due to increased access to jobs and the creation of new jobs associated with induced development; and
- Stimulation of TOD and community sustainability.

Cumulative Impacts: Cumulative impacts are impacts upon the environment that result from the incremental effect of the project when added to other past, present and reasonably foreseeable future actions regardless of what agency (federal or non-federal) or person undertakes such other actions. Cumulative impacts can result from individually minor but collectively significant actions taking place over a period of time.

The proposed project is consistent with and supported by all regional and local plans with the exception of the Town of Windsor Plan of Conservation and Development (June 2004), which supports constructing a second commuter railroad station, which is not included as an element of the proposed project. Therefore, development is assumed to continue to be managed consistent with local land use policies and regulations and the cumulative impacts of growth on community and natural resources will be offset by local land use management actions.

The long-term plan for High Speed Rail (HSR) service and infrastructure improvements in the NHHS rail corridor contemplates potential electrification of the line so that electric locomotives would be used in lieu of diesel locomotives. Such infrastructure improvements may involve property and environmental impacts beyond those discussed in this EA/EIE. Detailed infrastructure requirements and a full environmental assessment would be developed in the future as electrification of the line becomes a priority.

Overall cumulative effects of the development of the projects can be expected where new development emerges consistent with smart growth principles advocated by the State of Connecticut, Commonwealth of Massachusetts, and State of Vermont. These include concentrating new development in designated growth areas, preserving neighborhood character as development occurs, and locating new growth where supportive infrastructure, including all modes of transportation, is available.

Regional cumulative impacts due to the station improvements generally occur when the impacts of new local development or redevelopment new or improved station locations are added together. All proposed stations locations occur in existing well-developed communities where growth or change in land use patterns is ongoing. Adverse cumulative impacts for the station locations are not expected to be substantial, because changes in land use (development and redevelopment) are assumed to be managed consistent with local land use policies and regulations.

Construction Period Impacts

Construction of the proposed project would result in some temporary impacts, which would be minimized or mitigated through design- and construction-related measures and controls and implementation of plans developed in compliance with applicable state and federal requirements. Potential impacts would include fugitive-dust emissions; light pollution during any nighttime construction activity; erosion and sedimentation of wetlands, waterways and reduced quality of surface and ground waters; contaminated materials exposure; business disruptions; and localized increases in traffic volumes, parking relocation and detours to typical traffic patterns.

The most important way to reduce construction-related impacts is through proactive communications with residents, businesses, public officials, railroads and communities to ensure that those impacted by construction know when the work will take place, how long it will take, and the extent of likely impacts such as crossovers, detours and runarounds will be used. CTDOT is committed to a highly proactive communications program that relies on providing impacted parties detailed information about the project and project activities through the project website, Facebook, Twitter, newsletters, press releases, public meetings and other written materials and correspondence. CTDOT will work with all entities involved in the planning and implementation of construction work – including Amtrak and contractor forces – to maximize communications with the public and coordinate notifications in advance of work activities, track outages, and any schedule changes in train service. All Massachusetts work would be in future Tier 2 environmental documents.

Construction-related impacts would be temporary at any given location along the NHHS rail corridor. Overall, the proposed project could take approximately 60 months but may take longer based on funding availability. Track and signal work for comparable stretches of rail line often can be completed within a matter of weeks. Bridge and culvert repairs may be seasonally limited; the duration of work depends on both project scope and the availability of track outages. Station improvements are expected to take approximately 18 months each.

During final design and construction, Best Management Practices (BMPs) will be followed for track restoration, construction of rail sidings, bridge and culvert repair and replacement and station improvements. These BMPs include design features to properly manage storm water during/after construction, as well as temporary measures to minimize direct/indirect impacts during construction.

Irreversible and Irretrievable Commitment of Resources

The proposed project would result in an irretrievable and irreversible use of energy, construction materials, and human labor. It would also require a commitment of federal and state funds that are not retrievable for construction and future maintenance over the life of the facility. Labor, energy and natural resources would be used in the fabrication and preparation of construction materials. These materials are generally not retrievable. However, they are not in short supply and their use would not have an adverse effect upon the continued availability of these resources.

Cost Benefit Analysis

The proposed project would require a capital cost of about \$647 Million and, as a result, would generate significant transportation, economic, community, and environmental benefits within the NHHS corridor and the region. By providing the railroad infrastructure to support the 2030 service plan, the project would facilitate the following benefits:

- Car trips diverted to rail: 1.5 million
- Increase in passenger miles per year from 52 million to 133 million
- Increase in ridership: 1.26 million new annual trips by 2030
- Reduction in number of vehicles: 3.2 million
- Reduction in vehicle miles driven: 100+ million
- Fuel saved: 3.5+ million gallons
- Promotion of development of active, vibrant communities
- Percent of population living within 25-mile radius of planned service: 80 percent
- Service for transit-dependent populations
- Project-related construction and related jobs: 12,590
- Connection of regional travelers with local businesses and activities

Freight rail service also would be improved by the additional track capacity allowing operators to better serve their customers. It is anticipated that freight operations would grow at a rate of 1.75 percent per year. The proposed project has been developed to accommodate this growth; while freight delays would increase by about 8 minutes per 100 train miles or about 3 minutes per trip, it is expected that this will be mitigated by optimizing future freight and passenger schedules.

ES-5.0 AGENCY COORDINATION AND PUBLIC INVOLVEMENT

Agency coordination and public involvement for the NHHS High Speed Intercity Passenger Rail Project EA/EIE are being conducted in accordance with NEPA and CEPA.

Agency Coordination: The FRA is the Lead Agency for the environmental review pursuant to NEPA. The FRA has overall responsibility for facilitating completion of the environmental review process, reviewing the EA/EIE and issuing the decision document. The FTA is a Cooperating Agency for this EA/EIE because CTDOT anticipates seeking FTA funding for four future stations and improvements to one additional existing station along the line. CTDOT is the Sponsoring Agency for this project and is coordinating with the Connecticut Office of Policy and Management (OPM) regarding CEPA compliance.

Public Involvement: The public has been continuously engaged since 2002 in the development of improved passenger rail service in the NHHS rail corridor. This includes public participation in the NEPA/CEPA process associated with implementation of commuter rail service along the NHHS rail corridor, as well as for the 2010 Environmental Review and the current EA/EIE. During the public involvement effort for this EA/EIE, CTDOT has revisited the elected officials and managers of all of the NHHS rail corridor's towns, and has engaged the public to discuss the proposed project and receive public input. Through that effort, the proposed project has been refined to incorporate needs articulated by the towns' representatives.

Public support for the program has been constant since 2002. Public input from individuals and community leaders has focused on two primary issues: 1) coordination of station improvements and new station construction to ensure compatibility with local development plans; and 2) management of traffic congestion at grade crossings due to the increased number of gate closures necessitated by the proposed service enhancements.

Preparation of this NEPA/CEPA EA/EIE has involved a public involvement program intended to inform the public and receive input regarding the proposed project. Meetings with officials from each of the 13 communities in the NHHS rail corridor and public information meetings in Wallingford and Windsor Locks were held between April and September, 2011. Two public information meetings were held in August and September of 2011. Additional public outreach occurred via a project website, newsletters, public advertisements and press releases. Three Public Hearings are being held during the public comment period as part of the NEPA/CEPA process. The required 45-day comment period for the EA/EIE begins with the issuance of the EA/EIE and publication of the Notice of Availability.

Agency and Railroad Coordination: Coordination meetings with railroads currently operating freight and passenger service on the NHHS rail corridor were held on:

- March 17, 2008
- December 11, 2008
- March 20, 2009
- June 11, 2009
- January 13, 2011

An initial agency coordination meeting was held on March 26, 2010, with representatives from Connecticut, Massachusetts, Vermont, Amtrak, and freight railroads. During preparation of this EA/EIE, these representatives have communicated frequently, both formally and informally, on service, planning and funding issues. Freight railroads, which participated in these discussions, include Connecticut Southern Railroad (CSO), Providence and Worcester Railroad (PW), CSX Transportation (CSX), New England Central Railroad (NECR) and Pan Am. Amtrak, which owns and operates the NHHS rail corridor, has attended bi-weekly progress meetings, as well as other technical meetings, since January 2011.

Table ES-1 – Summary of Potential Environmental Consequences & Potential Mitigation

Environmental Resources		Proposed Improvements					
		Environmental Assessment/Environmental Impact Evaluation				Phase 1 Categorical Exclusion	Phase 3A Categorical Exclusion
		Reinstall Track: Mile Posts 7.1 to 17.0, 31.1 to 35.1, 46.7 to 49.0 and 50.4 to 54.8; New Siding: Mile Posts 26.6 to 27.8	Increased Passenger Train Frequency and Speed (Outcome of Proposed Improvements)	Springfield Layover and Maintenance (See Note 1)	Station Improvements (All stations identified are included in this project except for the future commuter stations, printed in bold italics)	Reinstall Track: Mile Posts 20.3 to 31.1	Reinstall Track and New Siding: Mile Posts 37.2 to 43.0
4.2.1	Air Quality	No impacts anticipated.	No impacts anticipated. A shift to public transportation should reduce vehicle miles traveled and improve air quality.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.2.2	Noise and Vibration	No impacts anticipated.	Severe noise impacts at 1847 receptors; moderate noise impacts at 2767 receptors caused by horn noise at crossings and stations. Moderate wayside noise impacts to 214 receptors and 7 severe receptors. No vibration impacts anticipated.	Low to moderate impact; no mitigation required.	No impacts anticipated. The duration of train horn noise at stations would be very brief and not considered an adverse impact; no mitigation required.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: It is proposed that severe horn noise will be mitigated by establishing Quiet Zones. It is proposed the severe wayside noise impacts may be individually mitigated.	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.2.3	Topography and Geology	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.2.4	Floodplains and Stream Channel Encroachment Lines	Anticipated impacts of up to 0.5 acres floodway, 3.4 acres floodplains, and 2.0 acres SCEL from double-tracking and rail siding. Additional impacts from bridges and culverts.	No impacts anticipated.	No impacts anticipated.	Up to 3.4 acres of floodplain impacts from <i>North Haven</i> , Meriden, <i>Newington</i> , Windsor, and Windsor Locks (South Main St) stations (combined).	See Note 2. Impacts to up to 2.1 acres of 100-year floodplains, 0.5 acres of floodway, and 0.1 acres of SCELs. Will be reduced by minimizing expansion of the trackbed.	Up to 1.8 acres of impact to 100-year floodplains. No other impacts. Will be reduced by minimizing expansion of the trackbed.
		Proposed Mitigation: It is proposed that mitigation to be provided through coordination with CTDEEP and compliance with all federal requirements.	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: It is proposed that mitigation to be provided through coordination with CTDEEP and compliance with all federal requirements.	Proposed Mitigation: It is proposed that mitigation to be provided through coordination with CTDEEP and compliance with all federal requirements.	Proposed Mitigation: It is proposed that mitigation to be provided through coordination with CTDEEP and compliance with all federal requirements.
4.3.1	Critical Environmental Areas and Threatened and Endangered Species	16 to 18 listed species and/or their habitats occur in CT in vicinity of double-track areas. USFWS has identified the dwarf wedge mussel has been known to occur within the Farmington River (MP44).	No impacts anticipated.	No impacts anticipated.	Between 2 and 15 listed species and/or their habitats occur in CT in vicinity of New Haven, <i>North Haven</i> , Wallingford, Windsor, Windsor Locks, and <i>Enfield</i> stations.	One potential sensitive/critical habitat at MP23.	16 listed species and/or their habitats occur in CT at MP43 and in the Windsor area.
		Proposed Mitigation: Work will be within existing RR ROW, minimizing potential risk to this habitat. Coordinate with USFWS if work occurs in Farmington River at MP 44.	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: impacts will be avoided and/or mitigated through further design and coordination with CTDEEP.	Proposed Mitigation: Risk would be avoided by remaining within existing RR ROW.	Proposed Mitigation: Risk would be avoided by remaining within existing RR ROW.
4.3.2	Water Resources and Water Quality	No impacts anticipated to ground water, some potential impacts to surface water.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated to ground water, some potential impacts to surface water.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: Coordinate with CTDEEP and FRA, including appropriate mitigation and comply with all federal and state	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Coordinate with CTDEEP and FRA, including appropriate mitigation and comply with all federal and state	Proposed Mitigation: None	Proposed Mitigation: None
4.3.3	Wetlands	Up to 1.3 acres of potential impact from double-tracking and rail siding. Additional impacts from bridges and culverts.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	See Note 2. Up to 1.6 acres of potential impact.	Up to 1.0 acre of potential impact.
		Potential Mitigation: Impacts to be reduced by minimizing expansion of ROW and mitigated through CTDEEP and USACE permitting process and appropriate compensatory mitigation.	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Potential Mitigation: Impacts to be reduced by minimizing expansion of ROW and mitigated through CTDEEP and USACE permitting process and appropriate compensatory mitigation.	Potential Mitigation: Impacts to be reduced by minimizing expansion of ROW and mitigated through CTDEEP and USACE permitting process and appropriate compensatory mitigation.

Table ES-1 - Summary of Potential Environmental Consequences & Potential Mitigation (Continued)

Environmental Resources		Proposed Improvements					
		Environmental Assessment/Environmental Impact Evaluation				Phase 1 Categorical Exclusion	Phase 3A Categorical Exclusion
		Reinstall Track: Mile Posts 7.1 to 17.0, 31.1 to 35.1, 46.7 to 49.0 and 50.4 to 54.8; New Siding: Mile Posts 26.6 to 27.8	Increased Passenger Train Frequency and Speed (Outcome of Proposed Improvements)	Springfield Layover and Maintenance (See Note 1)	Station Improvements (All stations identified are included in this project except for the future commuter stations printed in bold italics)	Reinstall Track: Mile Posts 20.3 to 31.1	Reinstall Track and New Siding: Mile Posts 37.2 to 43.0
4.3.4	Wild and Scenic Rivers, Navigable Waterways, and Coastal Resources	No impacts anticipated to the Connecticut River in Windsor Locks.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated; however, Coastal Area Management review required for <i>North Haven</i> Station.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.3.5	Prime Farmlands and Farmlands of State wide Importance	Potentially a total of 4.0 acres of impact along the 62 mile corridor	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: Mitigation through application of the Farmland Conversion Impact Rating Form, and compensatory mitigation.	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.4.1	Land Use and Zoning	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	Proposed use is consistent with local zoning. Both beneficial and adverse impacts for 6 station locations. No other anticipated land	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Will maintain ongoing coordination with affected communities during final design.	Proposed Mitigation: None	Proposed Mitigation: None
4.4.2	Consistency with State, Regional and Local Plans	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	The Towns of Wallingford and Windsor Locks have not selected between two station locations options.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Continue to consult with Towns of Wallingford and Windsor Locks to reach	Proposed Mitigation: None	Proposed Mitigation: None
4.4.3	Property Acquisitions and Displacements	No impacts anticipated.	No impacts anticipated.	2 full and 2 partial property acquisitions, which would be consistent with local development plans.	16 full and 11 partial property acquisitions, which would be consistent with local development plans.	See Note 3.	Possible requirement for small takings, which will be mitigated by minimizing expansion of ROW.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Uniform Relocation Act will apply to any property acquisition or taking.	Proposed Mitigation: Uniform Relocation Act will apply to any property acquisition or taking.	Proposed Mitigation: Uniform Relocation Act will apply to any property acquisition or taking.	Proposed Mitigation: Uniform Relocation Act will apply to any property acquisition or taking.
4.4.4	Socio-economics	No impacts anticipated.	Project related impacts on socioeconomic conditions would be beneficial.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.4.5	Community Resources and Neighborhood Character	No impacts anticipated.	Minimal impact due to noise and traffic congestion at grade crossings. Project related impacts on community resources and neighborhood character would be beneficial.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: Horn noise will be mitigated by establishing Quiet Zones. Traffic congestion at grade crossings will be mitigated with traffic signal and intersection improvements.	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.4.6	Visual Resources and Quality	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	Potential adverse impacts at Berlin, Windsor Locks (North Main Street), Wallingford (Rt 5), <i>Newington</i> , Windsor and <i>Enfield</i> stations. No impacts anticipated at other station locations.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Impacts to be minimized and mitigated through ongoing community coordination and design reviews.	Proposed Mitigation: None	Proposed Mitigation: None

Table ES-1 - Summary of Potential Environmental Consequences & Potential Mitigation (Continued)

Environmental Resources		Proposed Improvements					
		Environmental Assessment/Environmental Impact Evaluation				Phase 1 Categorical Exclusion	Phase 3A Categorical Exclusion
		Reinstall Track: Mile Posts 7.1 to 17.0, 31.1 to 35.1, 46.7 to 49.0 and 50.4 to 54.8; New Siding: Mile Posts 26.6 to 27.8	Increased Passenger Train Frequency and Speed (Outcome of Proposed Improvements)	Springfield Layover and Maintenance (See Note 1)	Station Improvements (All stations identified are included in this project except for the future commuter stations printed in bold italics)	Reinstall Track: Mile Posts 20.3 to 31.1	Reinstall Track and New Siding: Mile Posts 37.2 to 43.0
4.4.7	Cultural Resources	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement
		Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement
4.4.8	Section 4(f)	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts will be as stipulated in Programmatic Agreement	Impacts to historical bridges and culverts resulting from repairs/replacement.	Impacts to historical bridges and culverts resulting from repairs/replacement.
		Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation will be as stipulated in Programmatic Agreement	Proposed Mitigation: Mitigation through consultation and compliance with requirements of the SHPO and FRA and USACE	Proposed Mitigation: Mitigation through consultation and compliance with requirements of the SHPO and FRA and USACE
4.4.9	Section 6(f) Resources	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.4.10	Parkland Resources	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
4.4.11	Transportation	No impacts anticipated.	Project related impacts on transportation options available would be beneficial. Increased traffic congestion at 9 grade crossings.	No impacts anticipated.	Increased traffic congestion at 2 intersections.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: Traffic congestion at grade crossings will be mitigated with traffic signal and intersection	Proposed Mitigation: None	Proposed Mitigation: Traffic congestion at intersections will be mitigated with traffic signal and	Proposed Mitigation: None	Proposed Mitigation: None
4.4.12	Public Utilities and Energy Requirements	No impacts anticipated.	No impacts anticipated. Project related impacts on energy would be beneficial.	No impacts anticipated.	May require some utility relocation.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Coordinate with utilities to eliminate or minimize disruption.	Proposed Mitigation: None	Proposed Mitigation: None
4.4.13	Hazardous Materials	Waste and toxic materials typical of railroad rights-of-way.	No Impacts anticipated.	Waste and toxic materials typical of railroad ROW.	Buildings requiring demolition can contain lead and asbestos. Potential hazardous waste at <i>North Haven</i> property to be acquired.	Waste and toxic materials typical of railroad ROW.	Waste and toxic materials typical of railroad ROW.
		Proposed Mitigation: Investigation during final design. Develop and comply with Waste Management Plan.	Proposed Mitigation: None	Proposed Mitigation: Investigation during final design. Develop and comply with Waste Management Plan.	Proposed Mitigation: Investigation during final design. Develop and comply with Waste Management Plan.	Proposed Mitigation: Investigation during final design. Develop and comply with Waste Management Plan.	Proposed Mitigation: Investigation during final design. Develop and comply with Waste Management Plan.
4.4.14	Safety and Security	No impacts anticipated.	Increased Passenger Train Frequency and Speed	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: Grade crossings will be improved with supplemental safety	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None

Table ES-1 - Summary of Potential Environmental Consequences & Potential Mitigation (Continued)

Environmental Resources		Proposed Improvements					
		Environmental Assessment/Environmental Impact Evaluation				Phase 1 Categorical Exclusion	Phase 3A Categorical Exclusion
		Reinstall Track: Mile Posts 7.1 to 17.0, 31.1 to 35.1, 46.7 to 49.0 and 50.4 to 54.8; New Siding: Mile Posts 26.6 to 27.8	Increased Passenger Train Frequency and Speed (Outcome of Proposed Improvements)	Springfield Layover and Maintenance (See Note 1)	Station Improvements (All stations identified are included in this project except for the future commuter stations printed in bold italics)	Reinstall Track: Mile Posts 20.3 to 31.1	Reinstall Track and New Siding: Mile Posts 37.2 to 43.0
4.4.15	Environmental Justice	No impacts anticipated.	Beneficial impact of new or improved access to regional rail service. Adverse impact from increased traffic congestion at several grade crossings.	No impacts anticipated.	Provide stations near to EJ Populations improving mobility options. Impact due to increased traffic congestion at several intersections.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: Traffic congestion at grade crossings will be mitigated with traffic signal and intersection improvements.	Proposed Mitigation: None	Proposed Mitigation: Traffic congestion at intersections will be mitigated with traffic signal and intersection improvements	Proposed Mitigation: None	Proposed Mitigation: None
4.4.16	Secondary and Cumulative Impacts	No impacts anticipated.	Secondary impacts are generally beneficial due to induced development.	No impacts anticipated.	Secondary impacts are generally beneficial due to induced development. Potential for traffic congestion impacts at intersections as station development increases.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: Traffic congestion at intersections will be mitigated with traffic signal and intersection improvements	Proposed Mitigation: None	Proposed Mitigation: None
4.4.17	Construction Impacts	Impacts will be temporary, including train speed restrictions, noise, air quality, water quality, disposal of construction waste, contaminated soils, and utility impacts.	No impacts anticipated.	Impacts will be temporary, including train speed restrictions, noise, air quality, water quality, disposal of construction waste, contaminated soils, and utility impacts.	Impacts will be temporary, including lane restrictions, train speed restrictions, noise, air quality, water quality, disposal of construction waste, contaminated soils, and utility impacts.	Impacts will be temporary, including train speed restrictions, noise, air quality, water quality, disposal of construction waste, contaminated soils, and utility impacts.	Impacts will be temporary, including train speed restrictions, noise, air quality, water quality, disposal of construction waste, contaminated soils, and utility impacts.
		Proposed Mitigation: Mitigation incorporating Best Management Practices, maintenance of traffic, and compliance with permits.	Proposed Mitigation: None	Proposed Mitigation: Mitigation incorporating Best Management Practices, maintenance of traffic, and compliance with permits.	Proposed Mitigation: Mitigation incorporating Best Management Practices, maintenance of traffic, and compliance with permits.	Proposed Mitigation: Mitigation incorporating Best Management Practices, maintenance of traffic, and compliance with permits.	Proposed Mitigation: Mitigation incorporating Best Management Practices, maintenance of traffic, and compliance with permits.
4.4.18	Irreversible and Irrecoverable Commitment of Resources	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.	No impacts anticipated.
		Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None	Proposed Mitigation: None
Note 1.		The impacts for the Springfield Layover are based on using the Armory site. If the Springfield station or the Sweeny Yard are used, there would be no impacts anticipated.					
Note 2.		Based on the information available during the preparation of the CE and knowing that the corridor historically carried two tracks, it was expected that the new track could be installed without permanent wetland or flood plain impacts; wetland impacts would be temporary during bridge and culvert work. During PE, with the topographical/ROW survey completed and design criteria established for track centers and shoulders, it is now recognized that permanent impacts would occur to avoid new retaining walls. However, Amtrak has indicated that it will work with CTDOT to avoid any such adverse impacts.					
Note 3.		Based on the information available during the preparation of the CE and knowing that the corridor had historically carried two tracks, it was expected that the new track could be installed without ROW takes. During PE, with the topographical/ROW survey completed and track center design criteria established, it has been recognized that sliver takes may be required. Any necessary property takes would comply with federal requirements.					

Errata to the Environmental Assessment

Page ES-7 **West Hartford Station**; revise the second line to delete the word "unused".

Page ES-12 **Air Quality**, third paragraph; revise the first sentence to read: Beginning in **December 2012**, a quantitative...

Page ES-20 **Environmental Justice**, second paragraph; revise the first sentence to delete the word "indirect".

Page ES-27 **Table ES-1- Summary of Potential Environmental Consequences & Potential Mitigation, 4.4.11 Transportation**; for the Increased Passenger Train Frequency and Speed revise the proposed mitigation to read: Traffic congestion at grade crossings will be mitigated with traffic signal and intersection **improvements**.

Page ES-27 **Table ES-1- Summary of Potential Environmental Consequences & Potential Mitigation, 4.4.11 Transportation**; for the Station Improvements revise the proposed mitigation to read: Proposed Mitigation: Traffic congestion at intersections will be mitigated with traffic signal and **intersection improvements**.

Page 3 **This EA/EIE is organized as follows**; revise the text for the fourth bullet to read,

- **Section 5** summarizes agency and public...

Page 36 **Table 4-1- Summary of Potential Environmental Consequences & Potential Mitigation, 4.4.11 Transportation**; for the Increased Passenger Train Frequency and Speed revise the proposed mitigation to read: Traffic congestion at grade crossings will be mitigated with traffic signal and intersection **improvements**.

Page 36 **Table 4-1- Summary of Potential Environmental Consequences & Potential Mitigation, 4.4.11 Transportation**; for the Station Improvements revise the proposed mitigation to read: Proposed Mitigation: Traffic congestion at intersections will be mitigated with traffic signal and **intersection improvements**.

Page 169 **Table 4-31 Energy Requirements**; revise the heading of the second column to read: Reduction in **VMT**

Page 169 Delete the **Methodology** paragraph in its entirety and replace with the following:
Methodology

Hazardous waste sites were identified using the EPA's Comprehensive Environmental Response, Compensation, and Liability Information System (CERCLIS) GIS coverage for those towns located in the New Haven-Hartford-Springfield Rail Corridor in Connecticut and Massachusetts. In addition, the EPA's Brownfield mapping, CT DEEP GIS coverage, Landfill Leachate and Wastewater Discharges, and MassGIS (MassDEP) were used to augment the CERCLIS information obtained for the study corridor. Potential hazardous materials sites located within 250 feet of the Rail Corridor were identified on Project mapping (included as Section 2.6 of Volume II of this EA/EIE). No field verification or visual inspection of these locations was conducted.

Page 170 Delete the second paragraph of Existing Conditions in its entirety and replace with the following:

Based on GIS analysis, the Rail Corridor contains no CERCLIS-listed sites within 250 feet of the rail line. Twenty-one leachate wastewater sites were identified on the CT DEEP Landfill Leachate and Wastewater Discharges data layer and three Brownfield sites were identified on the EAP mapping. These sites are listed in Table 4-32 and are collectively called environmental risk sites – locations where hazardous materials are known to have been used and/or hazardous waste generated and potentially discharged to the ground or water. Hazardous Materials and Leachate Waste Maps indicating the location of these sites in relation to the Rail Corridor can be found in Section 2.6 of Volume II of this EA/EIE.

Pages 170 and 171 Delete Table 4-32 in its entirety and replace with the following:

Table 4-32 -Hazardous Materials Risk Sites within 250 Feet of the Rail Corridor

Study Area City/Town	Number of Sites	Site Type
New Haven	1	Leachate Wastewater: Combined Sewer Overflow
Hamden	1	Leachate Wastewater: Industrial pit - seepage
	1	Leachate Wastewater: Oil Spill
North Haven	1	Leachate Wastewater: Cooling Water Discharge
	1	Leachate Wastewater: Former salt storage
	1	Leachate Wastewater: Bulky Waste Landfill
Wallingford	1	Leachate Wastewater: Industrial wastewater discharge
Meriden	1	Leachate Wastewater: Industrial pit/lagoons
	1	Hazardous Materials : Brownfields sites
Berlin	1	Leachate Wastewater: Solid waste transfer station
Newington	3	Leachate Wastewater: Industrial Wastewater Discharge
	1	Leachate Wastewater: Cooling Water Discharge
West Hartford	1	Leachate Wastewater: Industrial waste discharge
	1	Leachate Wastewater: Cooling Water Discharge
Hartford	2	Leachate Wastewater: Cooling Water Discharge
	1	Leachate Wastewater: Automobile Junkyard
	1	Leachate Wastewater: Oil Spill
	2	Hazardous Materials : Brownfields sites
Windsor Locks	1	Leachate Wastewater: Industrial Wastewater Discharge
Enfield	1	Leachate Wastewater: Sewage Treatment Plant

Page 171 Delete the Impacts paragraph in its entirety and replace with the following:

Impacts

Impacts from environmental risk sites and hazardous materials were evaluated based on proximity of the proposed Project site(s) to the potential hazardous risk sites. Those hazardous risk sites with potential to be directly impacted by installation of double-tracking, sidings, or station locations were identified as potential risk/impact areas.

Page 171 Delete the fifth bullet under Proposed Project in its entirety and replace with the following:

- There are no CERCLIS, leachate wastewater, or other potential hazardous materials sites in the vicinity of the existing and proposed station locations. However, the property located east of the tracks adjacent to the proposed new North Haven station was observed in the field to may have hazardous waste present.

Section 2.6 of Volume II Delete the Hazardous Materials and Leachate Waste maps (28 sheets) in their entirety and replace with the revised text for pages 169-171 as indicated in this Appendix B Errata to the Environmental Assessment (EA/EIE).

Record of Decision
New Haven-Hartford-Springfield Line
High Speed Intercity Passenger Rail Project

Attachment B
Agency Coordination and Public Involvement

5.0 Agency Coordination and Public Involvement

Agency coordination and public involvement for the NHHS High Speed Intercity Passenger Rail Project EA/EIE were conducted in accordance with NEPA and CEPA. These Acts and their implementing regulations require that the environmental review of any significant activity undertaken, respectively, by a federal entity or (in the case of CEPA) a state, and which has the potential to impact the environment, be prepared in consultation with appropriate agencies. In addition, a public involvement process must be implemented to seek input from project stakeholders and the general public. Opportunities for public involvement are provided, at minimum, through scoping and a hearing(s) and comment period on the environmental document, before a decision concerning the proposed project is made. The following sections summarize agency coordination and public involvement conducted for this EA/EIE.

5.1 Agency Coordination

Federal Transportation Agencies

FRA is the Lead Agency for the environmental review pursuant to NEPA. The FRA has overall responsibility for facilitating completion of the environmental review process, reviewing the EA/EIE and issuing the decision document.

The FTA is a Cooperating Agency for this EA/EIE because CTDOT anticipates seeking FTA funding for four future stations and improvements to one additional existing station along the line.

Sponsoring Agency

CTDOT is the Sponsoring Agency for this project and is coordinating with the Connecticut Office of Policy and Management (OPM) regarding CEPA compliance.

Other Agencies

Federal, state, and local government agencies that have jurisdiction by law or special expertise regarding the environmental impacts that may result with implementation of the proposed transportation project will have the opportunity to review and provide comments on this EA/EIE and supporting technical reports. These agencies are as follows:

- Amtrak
- Connecticut Office of Policy and Management
- Connecticut Department of Energy and Environmental Protection
- Connecticut State Historic Preservation Office
- Massachusetts Highway District 2
- Massachusetts Executive Office of Transportation and Public Works
- Massachusetts Department of Environmental Protection
- Massachusetts Department of Housing and Community Development
- Massachusetts Historic Commission
- U.S. Fish and Wildlife
- U.S. Environmental Protection Agency Region 1
- U.S. Army Corps of Engineers
- Federal Transit Administration

5.2 Public Involvement

The public has been continuously engaged since 2002 in the development of improved passenger rail service in the NHHS rail corridor. As noted above, this includes public participation in the NEPA/CEPA process associated with implementation of commuter rail along the NHHS rail corridor, as well as in the 2010 Environmental Review and the current 2011 EA/EIE. During the public involvement effort for this EA/EIE, CTDOT has revisited the elected officials and managers of all of the NHHS rail corridor's towns, and has engaged the public to discuss the proposed project and receive public input. Through that effort, the proposed project has been refined to incorporate needs articulated by the towns' representatives.

Meeting minutes are included in the reference document "Public Information Meetings, Local Officials Meetings, and Rail Coordination Meetings."

Public support for the program has been constant since 2002. Public input, raised by individuals and community leaders, has focused on two primary issues:

- Coordination of station improvements and new station construction to ensure compatibility with local development plans; and
- Management of traffic congestion at grade crossings due to the increased number of gate closures necessitated by the proposed service enhancements.

Traffic congestion issues have been analyzed and, if appropriate, addressed to the satisfaction of community officials, as detailed in Section 4.4.10. With the exception of the towns of Wallingford and Windsor Locks, station-related issues also have been resolved for all communities. The resulting concept plans are provided in Section 1.3 of Volume II of this EA/EIE. For Wallingford and Windsor Locks, two station alternatives each are presented and fully evaluated in this EA/EIE. Following the public comment period, the preferred station alternative in each town will be identified.

Implementation Study - Steering Committee

For the *Implementation Study* (June 2005), a project Steering Committee was established to oversee the study's development and provide information to key decision makers throughout the process. In addition to appropriate CTDOT staff, the following were invited to participate on the committee:

- A representative from the Governor's Transportation Strategy Board;
- A representative from MADOT;
- The first official, or his/her representative, from each of the NHHS rail corridor's municipalities;
- A representative from each of the regional planning agencies in the corridor – South Central Regional Council of Governments, Central Connecticut Regional Planning Agency, Capitol Region Council of Governments, and Pioneer Valley Planning Commission;
- A representative from the Federal Transit Administration;
- A representative from the Federal Railroad Administration;

- Representatives from Amtrak and freight railroads using the corridor (Guilford Rail Systems, CSX, Connecticut Southern Railroad, and Providence and Worcester Railroad);
- A representative from state and federal regulatory agencies including, but not limited to, the Connecticut Department of Environmental Protection, US Environmental Protection Agency, Connecticut State Historic Preservation Office, Connecticut Department of Economic and Community Development, US Fish & Wildlife Service, and US Army Corps of Engineers, among others;
- A representative from the I-91 Transportation Investment Area (TIA);
- A representative from the Peter Pan Bus Company; and
- Other interested parties, such as freight railroads, All Aboard, CT Fund for the Environment, and Rideworks.

A total of four (4) Steering Committee meetings, all of which were open to the public, were held on the following dates:

- Steering Committee Meeting #1 - October 16, 2002
- Steering Committee Meeting #2 - March 24, 2003
- Steering Committee Meeting #3 - October 16, 2003
- Steering Committee Meeting #4 - June 30, 2004

Implementation Study - Public Meetings

Public meetings held during the course of the Implementation Study included public informational meetings, local official/town meetings, and meetings with other interested stakeholders. The meetings were structured to serve the large number of communities and stakeholders associated with the study corridor. The purpose of all of these meetings was to provide information and solicit input for the development of the proposed project. Public meetings were held twice during the course of the Implementation Study, with the first set of meetings taking place in April and May 2003 and the second set of meetings taking place in November 2004. The meetings consisted of a presentation and discussion of the existing conditions, alternatives evaluated, and recommended action. A total of ten (10) public information meetings were held as follows:

- April 29, 2003 - Windsor Town Hall, 275 Broad Street, Windsor, CT
- April 30, 2003 - Meriden City Hall, 142 East Main Street, Meriden, CT
- May 6, 2003 - Hartford Union Station, One Union Place, Hartford, CT
- May 7, 2003 - Mildred Wakeley Community Center, 7 Linsley Street, North Haven, CT
- May 22, 2003 - Enfield Town Hall, 820 Enfield Street, Enfield, CT
- November 3, 2004 - Mildred Wakeley Community Center, 7 Linsley Street, North Haven, CT
- November 9, 2004 - Windsor Locks Town Offices, 50 Church Street, Windsor Locks, CT
- November 10, 2004 - Berlin Town Hall, 240 Kensington Road, Berlin, CT
- November 16, 2004 - Wallingford Town Hall, 45 South Main Street, Wallingford, CT
- November 17, 2004 - Enfield Town Hall, 820 Enfield Street, Enfield, CT

Public Outreach

The Commuter Rail EA/EIE was prepared with involvement of the public, as well as federal and state agencies with jurisdiction over potentially affected resources. A coordination and public outreach plan was implemented during 2008 and 2009. Coordination with all the municipalities

along the NHHS rail corridor was instrumental for understanding municipal concerns, as well as communicating study progress. Coordination among CTDOT, Amtrak and rail freight operators was required to ensure the proposed project did not negatively impact railroad operations.

Commuter Rail Service - Steering Committee Meeting

A meeting of the NHHS Commuter Rail Steering Committee was held on April 16, 2009. Minutes from this meeting will be made available upon request.

Commuter Rail Service - Agency Scoping Meeting

A meeting with agencies having jurisdiction, special expertise or other interest in the proposed project was held on December 2, 2008.

Commuter Rail Service - Public Scoping Meetings

Meetings to provide the public with an opportunity to comment on the scope of the assessment for the Commuter Rail EA/EIE were held as follows:

- December 8, 2008 in Springfield, MA
- December 9, 2008 in Hartford, CT
- December 11, 2008 in Windsor, CT
- December 16, 2008 in North Haven, CT

Commuter Rail Service - Local Officials Meetings

Meetings were held with local officials in the following communities to provide them an opportunity to comment on the proposed project; minutes of these meetings are included in the reference document "Public Information Meeting, Local Officials Meetings, and Rail Coordination Meetings":

- November 13, 2008 - North Haven, CT
- January 21, 2009 - Wallingford, CT
- January 26, 2009 - Berlin, CT
- February 2, 2009 - Meriden, CT
- February 2, 2009 - Windsor, CT
- February 11, 2009 - Newington, CT
- February 19, 2009 - New Haven, CT
- February 25, 2009 - Enfield, CT
- March 10, 2009 - Hartford, CT
- March 27, 2009 - Windsor Locks, CT

2030 Vision for High Speed, Intercity, and Regional Rail Service in New England

Public informational meetings on *the* 2030 Vision Plan (and the related Service NEPA Environmental Review Document, July 2010) were held in June 2010 at the following locations:

- June 2, 2010 in Springfield, MA
- June 3, 2010 in Hartford, CT
- June 9, 2010 in New Haven, CT
- June 10, 2010 in Brattleboro, VT
- July 29, 2010 in Hartford, CT

At each of these meetings, a presentation of the 2030 Vision was made and public comment was encouraged. The public comments revealed solid public support for the advancement of increased rail service throughout New England. The availability of increased choices in transportation modes and improved connectivity of rail services to make intercity travel more attractive received particularly strong support. Minutes of these meetings are included in the reference document "Public Information Meeting, Local Officials Meetings, and Rail Coordination Meetings". Comments from the public were solicited via the project website (www.nhhsrail.com), via comment cards at the public meetings and via email to the project team. Through the public outreach effort, input was received on the study goals and objectives, alternatives for improvements to passenger rail service in the corridor, and on the scope of services for the program.

High-Speed Intercity Passenger Rail Project EA/EIE

Preparation of this NEPA/CEPA EA/EIE involves a public involvement program intended to inform the public and receive input the public may have regarding this project. Meetings with officials from each of the 13 communities in the NHHS rail corridor and public information meetings in Wallingford and Windsor Locks were held between April and September, 2011. The EA/EIE is available for public review and comment; public hearings are planned during the comment period. A Notice of Availability has been published in area newspapers and posted on the project website (www.nhhsrail.com) to provide the public with information about where the EA/EIE can be viewed and details of the public hearings.

In addition to the public hearings and comment period on this document, the public involvement process for this EA/EIE comprises the following elements, each of which is briefly described below:

- Local Official Meetings
- Public Information Meetings
- Project Website
- Project Mailing List
- Project Newsletters
- Public Advertisements/Press Releases
- Public Hearings and Public Comment

Local Officials Meetings

An initial meeting was held with each of the 13 corridor municipalities to brief officials on the study scope, existing conditions, study schedule and on the site selection process for proposed new train stations, where required. The meetings, for which minutes are included in the reference document "Public Information Meeting, Local Officials Meetings, and Rail Coordination Meetings," were held as follows:

Town	Date of Meeting
Berlin, CT	4/14/2011
Enfield, CT	4/15/2011
Hamden, CT	5/23/2011
Hartford, CT	4/19/2011
Meriden, CT	4/26/2011
New Haven, CT	4/29/2011
Newington, CT	4/25/2011
North Haven, CT	5/17/2011
Springfield, MA	4/26/2011
Wallingford, CT	4/11/2011
West Hartford, CT	4/29/2011
Windsor, CT	4/13/2011
Windsor Locks, CT	5/02/2011

An additional meeting was held with several of the 13 corridor municipalities during summer 2011 to brief officials on the study's progress and to present and discuss general station and parking concepts, ridership forecasts, and rail operations. The meetings were as follows:

Town	Date of Meeting
Berlin, CT	8/25/2011
Enfield, CT	8/04/2011
Hartford, CT	8/5/2011
Meriden, CT	6/28/2011
Newington, CT	6/22/2011
North Haven, CT	7/25/2011
Springfield, MA	7/28/2011
Wallingford, CT	8/11/2011
West Hartford, CT	7/27/2011
Windsor, CT	6/27/2011

Public Information Meetings

Public meetings (see the reference document "Public Information Meeting, Local Officials Meetings, and Rail Coordination Meetings") were held in both Wallingford and Windsor Locks, where alternative locations have been evaluated for reconstruction of the existing train stations, as follows:

Town	Date of Meeting
Wallingford, CT	8/4/2011
Windsor Locks, CT	9/20/2011

Project Website

The project website (www.nhhsrail.com), re-launched in April 2011, is updated periodically to provide current information on the NHHS Rail Program. Content on the website includes an overview of the proposed project, current project status, project objectives, schedule and cost; reports of past meetings and information regarding future meetings; photographs of the project area; newsletters; and links to relevant websites. The website can be translated to Spanish. This EA/EIE has been posted on the website for review during the public comment period, and a link to it is available on the CTDOT website (www.ct.gov/dot).

Between April, 2011, when CTDOT re-launched the project website, and December 31, 2011, the website was viewed by more than 16,000 visitors. The project website also provides the public with an opportunity to make comments. CTDOT provides e-mail responses to comments submitted by the public.

Project Contact List

A contact list of interested groups or individuals is maintained and is continually updated during the course of the project in order to provide information (e.g., newsletters) to interested parties as it becomes available. The contact list includes more than 900 contacts, including residents and businesses, general public, and local, state, and federal agencies.

Project Newsletters

Two newsletters were prepared and distributed to interested individuals or groups and posted on the project website. The first issue of the newsletter provided an overview of the project and summarized key elements of the project, such as schedule, costs, and benefits. The second issue of the newsletter focused on the EA/EIE, the environmental review process, and the tentative schedule for its completion.

Public Advertisements/ Press Releases

Public advertisements and press releases have been prepared, as required, and disseminated to news outlets (e.g., Hartford Courant, New Haven Register) to advertise the public meetings and hearings. The advertisements have been prepared in English and in Spanish and disseminated to both English and Spanish-language publications. A Notice of Availability has been published, indicating the locations where the EA/EIE is available and the date, time, and place of the public hearings. In addition, the public advertisements have indicated that the EA/EIE is available for download from the project website.

Public Hearings and Public Comment

Three Public Hearings are being held during the public comment period as part of the NEPA/CEPA process. The required 45-day comment period for the EA/EIE begins with the issuance of the EA/EIE and the Notice of Availability is published. See Section 7.0, Distribution List for details on where the document has been sent. All comments received during the 45-day comment period will be addressed in the Final EA/EIE.

5.3 Agency and Railroad Coordination

Agency Information Requests

Written and verbal requests for specific information, essential to the environmental studies performed as part of this document, were made to various federal and state agencies. This correspondence included requests for data on resources such as threatened and endangered species, historical and archaeological sites, fisheries, water quality, and agricultural lands. The following lists the agencies that responded to these initial efforts in the EA/EIE process. Response letters are included in Appendix 8 Formal Agency Coordination.

Federal Agency Information Requests:

- U.S. Fish and Wildlife Service
- Natural Resources Conservation Services of the U.S. Department of Agriculture

State Agency Information Requests:

- Connecticut Department of Energy and Environmental Protection, Natural Diversity Data Base
- Connecticut Department of Agriculture
- Connecticut Historical Commission, including the State Historic Preservation Office, are now part of the Connecticut Commission on Culture and Tourism

Other Coordination Meetings

Study Coordination Meetings were held with the four Metropolitan Planning Organizations (MPOs) along the rail corridor and those that exercise regulatory authority over the project such as FRA, the Army Corps of Engineers, and the Connecticut Department of Energy and Environmental Protection (CTDEEP). CTDOT hosted two Regional Meetings with Towns and Regional Planning Agencies at its headquarters on March 14, 2011 and October 2011. The purpose of these meetings was to brief municipal and regional officials on the project's purpose and need, the scope of the project, the current EA/EIE process, the proposed coordination and public involvement process, and the project's schedule and to solicit questions and comments.

As part of further coordination, the Army Corps of Engineers attended Project Manager's Meetings on December 15, 2011 and January 9, 2012 to discuss with both CTDOT and CTDEEP a "phased" approach to permitting for the entire NHHS program. The ACOE has issued "phased" permits in the past and also stressed the need to address cumulative impacts in the permit applications. Coordination is on-going.

Rail Coordination

Coordination meetings with railroads currently operating freight and passenger service on the NHHS rail corridor were held on:

- March 17, 2008
- December 11, 2008
- March 20, 2009
- June 11, 2009
- January 13, 2011

An initial agency coordination meeting was held on March 26, 2010, with representatives from Connecticut, Massachusetts, Vermont, Amtrak, and freight railroads. From that time through the preparation of this document, these representatives have communicated frequently, both formally and informally, on service, planning and funding issues. Freight railroads, which have participated in these discussions, include: Connecticut Southern RR (CSO), Providence and Worcester RR (PW), CSX Transportation (CSX), New England Central Railroad (NECR) and Pan Am.

Amtrak, which owns and operates the NHHS rail corridor, has attended bi-weekly progress meetings, as well as other technical meetings, since January 2011.

Record of Decision
New Haven-Hartford-Springfield Line
High Speed Intercity Passenger Rail Project

Attachment C
Scoping Notices and EA/EIE Notice of Availability
with Affidavits



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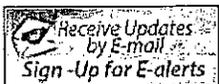


Barbara C. Wagner
Chair

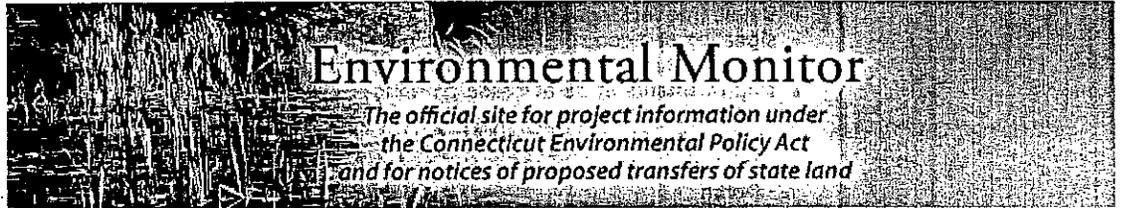
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- [How to Request a Public Scoping Meeting](#)
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Monitor Archives



September 20, 2011

Scoping Notices

1. **NEW!** New Haven - Hartford - Springfield Rail Improvements
2. **NEW!** Demolition of Buildings #11 and #29 at the Former Cedarcrest Hospital, Newington
3. University of Connecticut Health Center New Construction and Renovation Project, Farmington

Post-Scoping Notices: Environmental Impact Evaluation Not Required

No Post-Scoping notices have been submitted for publication in this edition.

Environmental Impact Evaluations

No Environmental Impact Evaluations have been submitted for publication in this edition.

State Land Transfers

1. Mystic Oral School, Groton (In Step I of Transfer Process)

The next edition of the Environmental Monitor will be published on October 4, 2011.

Subscribe to e-alerts to receive an e-mail when The Environmental Monitor is published.

Scoping Notices

"Scoping" is for projects in the earliest stages of planning. At the scoping stage, detailed information on a project's design, alternatives, and environmental impacts does not yet exist. Sponsoring agencies are asking for comments from other agencies and from the public as to the scope of alternatives and environmental impacts that should be considered for further study. Send your comments to the contact person listed for the project by the date indicated.

1. Notice of Scoping for New Haven - Hartford - Springfield Rail Improvements

Municipalities where proposed project might be located: The proposed improvements are along the existing 62 mile rail corridor starting at Union Station in New Haven, Connecticut, travelling northward through Hartford, and continuing to Union Station in Springfield, Massachusetts. It traverses the municipalities of New Haven, North Haven, Hamden, Wallingford, Meriden, Berlin, Newington, West Hartford, Hartford, Windsor, Windsor Locks, Enfield, Longmeadow and Springfield, Massachusetts.

Project Description: The New Haven-Hartford-Springfield (NHHS) Rail Project will provide significant new

regional passenger rail service options as a key component of a robust and vibrant multi-modal regional transportation system. With funding from both the new High-Speed Intercity Rail Program created by President Barack Obama in 2008, and the State of Connecticut, the NHHS Rail Project will provide the infrastructure and trains to operate some of the nation's best passenger rail services. As the gateway to New England, the NHHS Rail Project will also facilitate improved service to Massachusetts, Vermont and eventually Montreal. New train service will connect communities, generate sustainable economic growth, help build energy independence, and provide links to travel corridors and markets within and beyond the region.

The following infrastructure improvements to the existing rail corridor will be built to accommodate projected 2030 service:

- Re-installation of 38.7 miles of double track which was previously removed by AMTRAK in 1989.
- 5.8 miles of new passing sidings and rehabilitation of two existing sidings
- 9 new interlockings (so trains can change tracks)
- signaling and control systems, including Positive Train Control
- Repair, rehabilitation and replacement of bridges and culverts
- Improvements at 38 at-grade crossing
- High-level platforms, pedestrian overpasses, parking, and other amenities at:
 - Wallingford
 - Meriden
 - Berlin
 - Windsor
 - Windsor Locks
- Future new stations at North Haven, Newington, West Hartford and Enfield, and a new platform at New Haven State Street station
- Layover and maintenance facility near Springfield, MA station

Project Map: [Click here to view a map of the project area.](#)

Additional Project Information: [Click here to be directed to the project website.](#)

Written comments from the public are welcomed and will be accepted until the close of business on: October 20, 2011

Any person can ask the sponsoring agency to hold a Public Scoping Meeting by sending such a request to the address below. If a meeting is requested by 25 or more individuals, or by an association that represents 25 or more members, the sponsoring agency shall schedule a Public Scoping Meeting. Such requests must be made by September 30, 2011.

Written comments and/or requests for a Public Scoping Meeting should be sent to:

Name: Mr. Mark W. Alexander - Transportation Assistant Planning Director
Agency: Connecticut Department of Transportation
Address: 2800 Berlin Turnpike
 Newington, CT 06131
Fax: 860-594-3028
E-Mail: Mark.W.Alexander@ct.gov

If you have questions about the public meeting, or other questions about the scoping for this project, contact:

Name: Mr. Stephen V. Delpapa - Transportation Supervising Planner
Agency: Connecticut Department of Transportation
Address: 2800 Berlin Turnpike
 Newington, CT 06131
Phone: 860-594-2941
Fax: 860-594-3028
E-Mail: Stephen.Delpapa@ct.gov

The agency expects to release an Environmental Impact Evaluation for this project, for public review and comment by the end of 2011

2. Notice of Scoping for Demolition of Buildings #11 and #29 at the Former Cedarcrest Hospital Property



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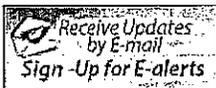


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Chair

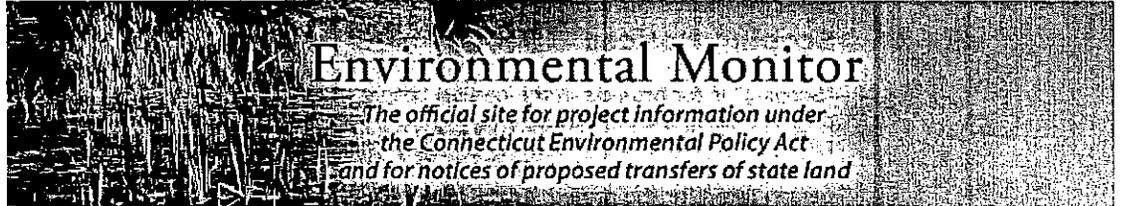
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Monitor Archives



May 8, 2012

Special Notice

1. **NEW!** State Conservation and Development Policies: A Plan for Connecticut, 2013-2018 (State C & D Plan) - Draft Available

Scoping Notices

1. **REVISED!** 20 North Water Street, Norwalk
2. **NEW!** Quinebaug Regional Technical Park, Putnam
3. **NEW!** Stamford Transportation Center, Stamford

Post-Scoping Notices: Environmental Impact Evaluation Not Required

1. **NEW!** Thames Shipyard Improvements, New London

Environmental Impact Evaluations

1. **NEW!** The Villages, Montville
2. **NEW!** New Haven - Hartford - Springfield Line High Speed Intercity Passenger Rail Project

State Land Transfers

No State Land Transfer Notices have been submitted for publication in this edition.

The next edition of the Environmental Monitor will be published on May 22, 2012.

Subscribe to e-alerts to receive an e-mail when The Environmental Monitor is published.

Special Notice

The following notice is published at the request of the Office of Policy and Management to provide notice of the availability of the revised draft of *Conservation and Development Policies: A Plan for Connecticut, 2013-2018*. There is a strong link between this plan and CEPA.

1. Conservation and Development Policies: A Plan for Connecticut, 2013-2018 (State C & D Plan)

The Office of Policy and Management (OPM) has published a revised draft of *Conservation and Development Policies: A Plan for Connecticut, 2013-2018* (the State C&D Plan), in accordance with CGS Section 16a-28 (b). The Draft State C&D Plan, including the Draft Locational Guide Map, can be viewed at: <http://www.ct.gov/opm/cwp/view.asp?a=2990&q=467686>.

OPM, in cooperation with Regional Planning Organizations (RPOs), will schedule, publicize, and conduct formal public hearings on the Draft State C&D Plan in each of the state's 14 planning regions between the

Comments on this EIE will be accepted until the close of business on: June 22, 2012

The public can view a copy of this EIE at: The Montville Town Clerk , 310 Norwich-New London Tpke., Uncasville, CT 06382

The Raymond Library, 832 Raymond Hill Rd, Oakdale, CT 06370

There is a public hearing scheduled for this EIE on:

DATE: June 14, 2012

TIME: 6:00 PM

PLACE: Montville Town Hall, Town Council Chambers, 310 Norwich-New London Tpke. Uncasville 06382

Send your comments about this EIE to:

Name: Mark Hood

Agency: Department of Economic and Community Development

Address: 505 Hudson Street
Hartford, CT 06106

E-Mail: mark.hood@ct.gov

If you have questions about the public hearing, or where you can review this EIE, or similar matters, please contact:

Name: Mark Hood

Agency: Department of Economic and Community Development

Address: 505 Hudson Street, Hartford, CT 06106

E-Mail: mark.hood@ct.gov

Phone: (860) 270-8089

2. Notice of EIE for the New Haven - Hartford - Springfield Line High Speed, Intercity Passenger Rail Project

Municipalities where project is proposed: New Haven, North Haven, Hamden, Wallingford, Meriden, Berlin, Newington, West Hartford, Hartford, Windsor, Windsor Locks, Enfield, and Springfield

Address of Possible Project Location: Rail Corridor from New Haven-Hartford-Springfield

Project Description: The proposed rail service enhancement in the NHHS rail corridor would provide for up to 25 daily round-trip trains (up to 50 one-way trips per day) by 2030. The proposed service plan would provide one-seat or cross-platform transfers on service from Washington, D.C., and New York to Springfield, Boston and the Knowledge Corridor, as well as bi-directional, 30-minute peak-hour service and hourly midday service in the NHHS rail corridor. Related operational improvements include an increase in the capacity of the line to accommodate additional trains, an increase in the maximum train speed to 110 miles per hour (mph), service to future new regional train stations in North Haven, Newington, West Hartford, and Enfield (to be constructed with Federal Transit Administration (FTA) funding), and reduced scheduled travel times. These operational improvements, in turn, require rail infrastructure improvements. Therefore, Connecticut has proposed the NHHS Rail Program, a program of capital projects to support enhanced passenger rail service in the NHHS rail corridor. The proposed project's infrastructure improvements in the NHHS rail corridor consist of:

- restoration of sections of track;
- construction of new passing sidings;
- construction of a layover and light maintenance facility;
- at-grade crossing upgrades;
- facility-specific bridge and culvert rehabilitations, replacements and removals;
- installation of new crossovers and signal upgrades;
- improvement or relocation of existing passenger rail platforms for Amtrak intercity service, as well as additional station parking and improved station access;
- improvements to platforms, track configuration and sidings in the Springfield Terminal area; and construction of future FTA-funded new regional rail stations in North Haven, Newington, West Hartford, and Enfield.

Project Map: [Click here to view a map of the project area.](#)

Comments on this EIE will be accepted until the close of business on: June 22, 2012

The public can view a copy of this EIE at: This document is available for public inspection at the Connecticut Department of Transportation, Bureau of Policy and Planning, 2800 Berlin Turnpike, Newington, CT; the City or Town Clerk's Office and the Public Libraries in the affected municipalities along the rail corridor, the South Central Regional Council of Governments, the Central Connecticut Regional Planning Agency, the Capitol Region Council of Governments, and the Pioneer Valley Planning Commission. The document is also available at www.nhhsrail.com.

There are three public hearings scheduled for this EIE on:

DATE: Thursday, June 7, 2012

TIME: 7:00 PM

PLACE: Torp Theatre, Davidson Hall, Central Connecticut State University, 1615 Stanley Street, New Britain, CT

DATE: Wednesday, June 13, 2012

TIME: 7:00 PM

PLACE: Asuntuck Community College, 170 Elm Street, Enfield, CT

DATE: Thursday June 14, 2012

TIME: 7:00 PM

PLACE: North Haven High School, 221 Elm Street, North Haven, CT

NOTES: All hearing locations are accessible to persons with disabilities. Deaf and hearing impaired persons or persons speaking a language other than English wishing to attend a hearing and requiring an interpreter may make arrangements by contacting the Department's Office of Communications at (860) 594-3062 (voice only) at least five days prior to the hearing.

The study team will be available at each hearing from 6:00pm-7:00pm to discuss the proposed improvements. The hearing presentations will begin at 7:00pm.

Additional information about this project can be found online at: www.nhhsrail.com

Send your comments about this EIE to:

Name: Mr. Mark W. Alexander - Transportation Assistant Planning Director

Agency: State of Connecticut Department of Transportation

Address: 2800 Berlin Turnpike, Newington, CT 06131

E-Mail: Mark.W.Alexander@ct.gov

If you have questions about the public hearing, or where you can review this EIE, or similar matters, please contact:

Name: Mr. Stephen V. Delpapa - Transportation Supervising Planner

Agency: State of Connecticut Department of Transportation

Address: 2800 Berlin Turnpike, Newington, CT 06131

E-Mail: Stephen.Delpapa@ct.gov

Phone: 860-594-2941

State Land Transfer Notices

Connecticut General Statutes [Section 4b-47](#) requires public notice of most proposed sales and transfers of state-owned lands. The public has an opportunity to comment on any such proposed transfer. Each notice includes an address where comments should be sent. [Read more about the five-step process...](#)



CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Notice of Availability of Environmental Document and Public Hearing

An Environmental Assessment/Connecticut Environmental Impact Evaluation
has been published for State Project No. 170-2296

**NEW HAVEN-HARTFORD-SPRINGFIELD LINE HIGH SPEED,
INTERCITY PASSENGER RAIL PROJECT**

The environmental document is available beginning on Tuesday May 8, 2012 for a
45 day public review and comment period. Public Hearings will be held at the following locations:

Location 1: Torp Theatre,
Davidson Hall
Central Connecticut State
University
1615 Stanley Street,
New Britain, CT

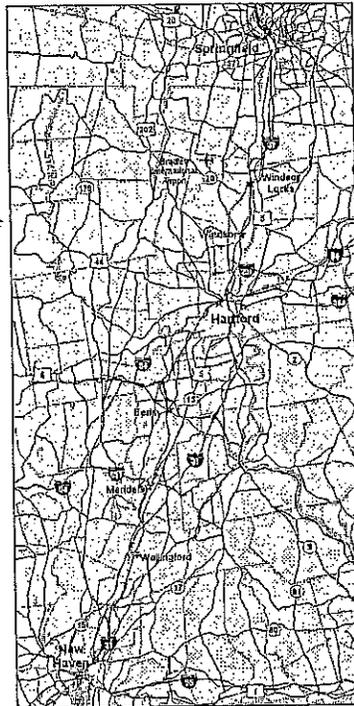
Time: 7:00 p.m.
Date: June 7, 2012

Location 2: Asnuntuck Community
College
170 Elm Street
Enfield, CT

Time: 7:00 p.m.
Date: June 13, 2012

Location 3: North Haven High School
221 Elm Street
North Haven, Connecticut

Time: 7:00 p.m.
Date: June 14, 2012



PLEASE JOIN US AT THE PUBLIC HEARINGS IN JUNE.

All hearing locations are accessible to persons with disabilities. Deaf and hearing impaired persons or persons speaking a language other than English wishing to attend this meeting and requiring an interpreter may make arrangements by contacting the Department's Office of Communications at (860) 594-3062 (voice only) at least five days prior to the meeting.

The study team will be available at each hearing from 6:00pm-7:00 pm to discuss the proposed improvements. The hearing presentation will begin at 7:00 pm. Comments may be given verbally or in writing at each public hearing. Comments will also be accepted by mail or via the project website <http://www.nhhsrail.com> through June 22, 2012. Written questions or comments sent by mail can be directed to: Mr. Mark W. Alexander, Transportation Assistant Planning Director, P.O. Box 317546, Newington, CT 06131-7546 or at e-mail address: Mark.W.Alexander@ct.gov

A copy of the environmental document is available for review via the project website <http://www.nhhsrail.com> and at the: Connecticut Department of Transportation, each City or Town Clerk's Office and the Public Libraries in the affected municipalities along the rail corridor, the South Central Regional Council of Governments, the Central Connecticut Regional Planning Agency, the Capitol Region Council of Governments, and the Pioneer Valley Planning Commission.

Affidavit of Publication

State of Connecticut
County of Fairfield

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Date: May 8, 2012

Ad Title: CT DOT - Notice of Availability
State Project No. 170-2296

Appeared in: Hartford Courant
publication and the newspaper extracts hereto annexed were clipped from the above named issue of said newspaper.

Subscribed and sworn to this 14th day of June, 2012 before me.

KATHLEEN VITKO
NOTARY PUBLIC
State of Connecticut
My Commission Expires
July 31, 2017

Kathleen Vitko
Notary Public

Hartford Courant

Publication Date: 05/08/2012

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CONNECTICUT
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Notice of Availability of
 Environmental Document and Public Hearing

An Environmental Assessment/Connecticut Environmental Impact Evaluation has been published for State Project No. 170-2296

NEW HAVEN-HARTFORD-SPRINGFIELD LINE HIGH SPEED, INTERCITY PASSENGER RAIL PROJECT

The environmental document is available beginning on Tuesday, May 8, 2012 for a 45 day public review and comment period. Public Hearings will be held at the following locations:

Location 1: Torp Theatre, Davidson Hall, Central Connecticut State University
 1615 Starley Street
 New Britain, CT
 Time: 7:00 p.m.
 Date: June 7, 2012

Location 2: Aentnuck Community College
 170 Elm Street
 Enfield, CT
 Time: 7:00 p.m.
 Date: June 13, 2012

Location 3: North Haven High School
 221 Elm Street
 North Haven, CT
 Time: 7:00 p.m.
 Date: June 14, 2012

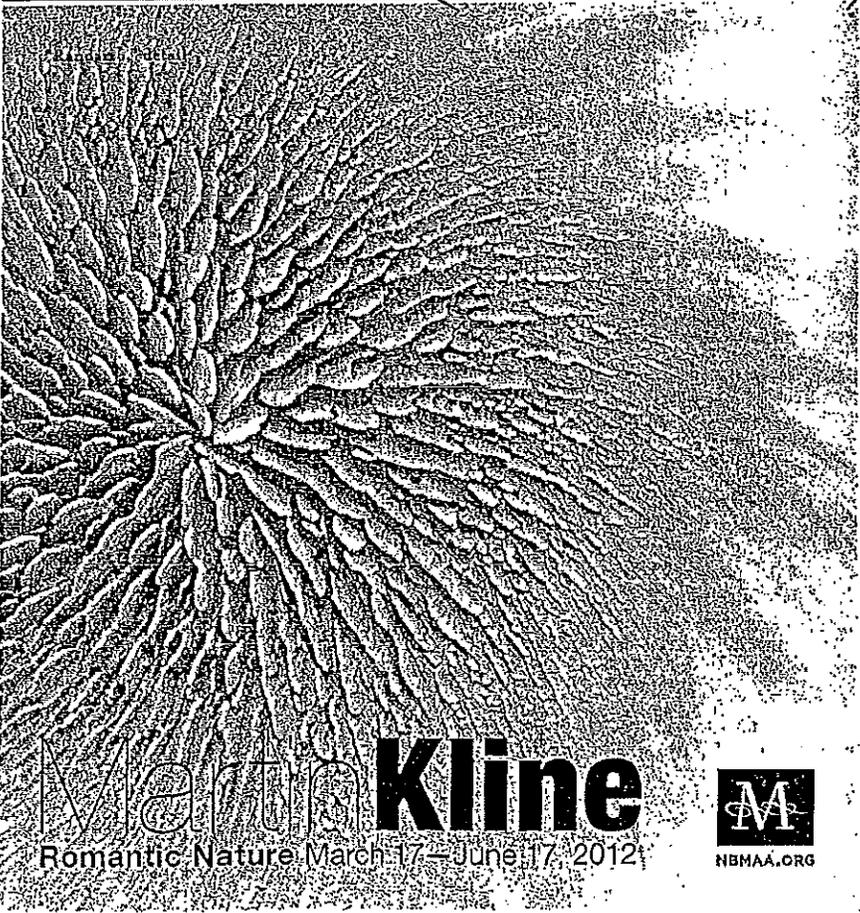


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All hearing locations are accessible to persons with disabilities. Deaf and hearing impaired persons or persons speaking a language other than English wishing to attend these meetings and requiring an interpreter may make arrangements by contacting the Department's Office of Communications at (860) 374-3063 (voice only) at least five days prior to the meeting.

The study team will be available at each hearing from 6:00 p.m. - 7:00 p.m. to discuss the proposed improvement. The hearing presentation will begin at 7:00 p.m. Comments may be given verbally or in writing at each public hearing. Comments will also be accepted by mail or via the project website <http://www.ctdot.com> through June 21, 2012. Written questions or comments sent by mail can be directed to: Mr. Mark W. Alexander, Transportation Advisory Planning Division, P.O. Box 317546, Newington, CT 06131-7546 or at e-mail address: Mark.W.Alexander@vt.gov

A copy of the environmental document is available for review via the project website <http://www.ctdot.com> as well as the Connecticut Department of Transportation, State City or Town Clerk's Office and the Public Libraries in the affected municipalities along the rail corridor, the South Central Regional Council of Governments, the Central Connecticut Regional Planning Agency, the Capital Region Council of Governments, and the Pioneer Valley Planning Commission.



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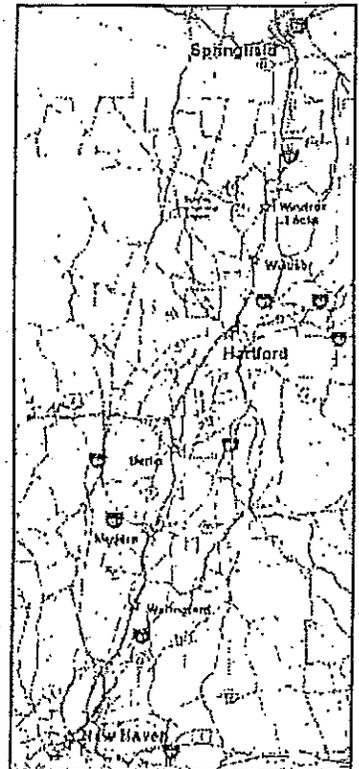
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**Time: 7:00 p.m.
Date: June 13, 2012**

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North Haven, CT**

**Time: 7:00 p.m.
Date: June 14, 2012**



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The study team will be available at each hearing from 6:00 p.m. - 7:00 p.m. to discuss the proposed improvements. The hearing presentation will begin at 7:00 p.m. Comments may be given verbally or in writing at each public hearing. Comments will also be accepted by mail or via the project website <http://www.nhhsrail.com> through June 22, 2012. Written questions or comments sent by mail can be directed to: Mr. Mark W. Alexander, Transportation Assistant Planning Director, P.O. Box 317546, Newington, CT 06131-7546 or at e-mail address: Mark.W.Alexander@ct.gov

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Affidavit of Publication

State of Connecticut
County of Fairfield

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 Time: 7:00 p.m.
 Date: June 7, 2012

Location 2: Ansonia Community College, 170 Elm Street, Enfield, CT
 Time: 7:00 p.m.
 Date: June 13, 2012

Location 3: North Haven High School, 221 Elm Street, North Haven, CT
 Time: 7:00 p.m.
 Date: June 14, 2012



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The study team will be available at each hearing from 5:00 p.m. - 7:00 p.m. to discuss the proposed improvements. The hearing presentation will begin at 7:00 p.m. Comments may be given verbally or in writing at each public hearing. Comments will also be accepted by mail or via the project website <http://www.ctdot.gov> through June 12, 2012. Written questions or comments sent by mail can be directed to: Mr. Mark W. Alexander, Transportation Assistant Planning Director, P.O. Box 217246, Newington, CT 06131-7246 or e-mail: Mark.W.Alexander@ct.gov

A copy of the environmental document is available for review via the project website <http://www.ctdot.gov> and at the Connecticut Department of Transportation, each City or Town Clerk's Office and the Public Libraries in the affected municipalities along the rail corridor, the South Central Regional Council of Governments, the Central Connecticut Regional Planning Agency, the Capitol Region Council of Governments, and the Pioneer Valley Planning Commission.



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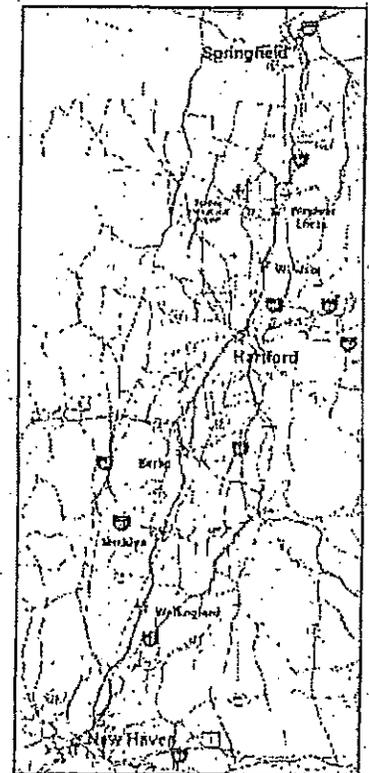
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Location 2: Asnuntuck Community
College
170 Elm Street
Enfield, CT

Time: 7:00 p.m.
Date: June 13, 2012

Location 3: North Haven
High School
221 Elm Street
North Haven, CT

Time: 7:00 p.m.
Date: June 14, 2012



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County of Fairfield

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NOTARY PUBLIC
State of Connecticut
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July 31, 2017

Notary Public

Kathleen Vitko

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Hartford Courant

Publication Date: 05/20/2012

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World briefing

Timeline: News and news services

NAACP endorses gay marriage as a civil right

In a move that some called historic, the country's oldest African-American civil rights group voted Saturday to endorse same-sex marriage.

The National Association for the Advancement of Colored People passed a resolution supporting gay marriage at a meeting of its board of directors in Miami, saying it opposed any policy or legislative initiative that seeks to curtail discrimination or hinder from the law or to remove the constitutional rights of LGBT citizens.

Director of the National Gay and Lesbian Task Force expressed in applause at their board meeting Saturday as their phones buzzed with the news.

"Today is a historic day," Rev. Curry, executive director of the task force, said. "This is what leadership looks like in this country."

President Barack Obama publicly supported gay marriage this month, and a recent Gallup Poll found, for the first time in the poll's history, a majority of Americans support the legalization of gay marriage, 53 to 43 percent.

The NAACP's support for marriage equality is deeply rooted in the 19th Amendment of the United States Constitution, which guaranteed equal protection of all people.

Still, it may be a long time before the entire community joins in support. Many African-Americans oppose same-sex marriage, viewing it as a religious matter, not a civil rights issue.

Last October, a Pew Forum poll found that 63 percent of African-American Protestants opposed gay marriage.



Funeral service held for Mary Kennedy

BEDFORD, N.Y. — Mary Kennedy's casket is carried out of St. Patrick's Church by family, including estranged husband Robert F. Kennedy Jr., back right, after her funeral service Saturday. Kennedy, 51, who died Tuesday, was recalled Saturday as an "angel" who was ultimately overwhelmed by a largely fight-or-flight response.

Possible mob tie probed in fatal Italy school blast

ROME — A bomb exploded at the entrance of a high school in southern Italy Sunday for the wife of a slain anti-Mafia judge, killing a 16-year-old girl and injuring at least four others as students were arriving for Saturday classes.

Police were investigating the likelihood of organized crime involvement in the attack in the Adriatic Sea port city of Brindisi, but authorities said it was too early to exclude other possibilities.

Car bomb at military complex in Syria kills 9

BEIRUT — A car bomb Saturday targeted a military complex in eastern Syria, killing at least nine people and left dozens injured, said Reuters that al-Qaida might be behind some of the attacks in the country.

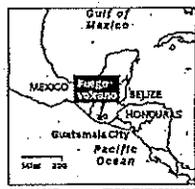
The bombing in Dar Al-Zaur damaged homes and government buildings, state media reported.

Moscow, President Barack Obama told the G-8 summit that Syrian President Bashar Assad must leave power.

Volcano eruption prompts advisory in Guatemala

Guatemala's Fuego volcano belched burning lava and black ash into the sky early Saturday, leading the government to issue an advisory for airports and to close roads.

The volcano, about 15 miles southeast of the capital of Guatemala City, erupted about 2:45 a.m. local time, spewing a column of ash up to 16,000 feet above the crater and launching red lava nearly 1,700 feet high. Planes were warned not to fly within a 25-mile radius of the volcano.



Tropical Storm Alberto forms off S.C. coast

Tropical Storm Alberto formed off the South Carolina coast Saturday, bringing an early start to the Atlantic hurricane season. Forecasters at the U.S. National Hurricane Center said.

With top sustained winds at 60 mph, Alberto was forecast to make a slow loop during the next few days and then turn northeast, making its way along the mid-Atlantic seaboard before dissipating in about five days.

THE NEWSMAKER

Zuckerberg weds longtime girlfriend

Facebook founder and CEO Mark Zuckerberg has updated his status to married. A day after his company's IPO, the 28-year-old billionaire married Priscilla Chan in California on Saturday, according to his own Facebook page.



Obama cites \$2B loss by Chase to tout reform

Allying squarely with Republican critics of Wall Street reform, President Barack Obama said Saturday that JPMorgan Chase's \$2 billion loss serves as a reminder of the importance of Washington's role in preventing another financial crisis.

"We can't afford to go back to an era of weak regulation and little oversight," Obama said in his weekly radio address.

The GOP, meanwhile, criticized Democrats for failing to approve an annual budget beyond one agreed to last year.

Yemen pushes offensive against militants

ADEN, Yemen — At least 22 al-Qaida-linked militants and 12 Yemeni soldiers were killed in clashes and airstrikes overnight during a U.S.-backed offensive against insurgents, officials said.

Government troops, backed by U.S. drone strikes, have been trying to push the insurgents out of the country's south, which has near oil-shipping routes. Washington and Yemen's neighbor, Saudi Arabia, fear the instability in Yemen could give al-Qaida's regional wing a stronger foothold in the region.

Court orders Texas to pay \$2M to jailed man

HOUSTON — The Texas Supreme Court has ordered the Lone Star State to pay more than \$1 million to Eby Frederick Allen, who spent 26 years in prison for murder, a ruling that could set a precedent for compensating prisoners whose convictions are overturned.

Unlike other inmates freed after DNA evidence proved their innocence, Allen was freed in 2009 after a court found problems with witness testimony and his trial attorney's representation.

Advertisement for Travelers Championship Golf at its Best. Includes dates from June 18-24, 2012, and event details for Monday through Sunday.

Connecticut Department of Transportation Notice of Availability of Environmental Document and Public Hearing. Details the New Haven-Hartford-Springfield Line High-Speed, Intercity Passenger Rail Project and lists three public hearing locations.



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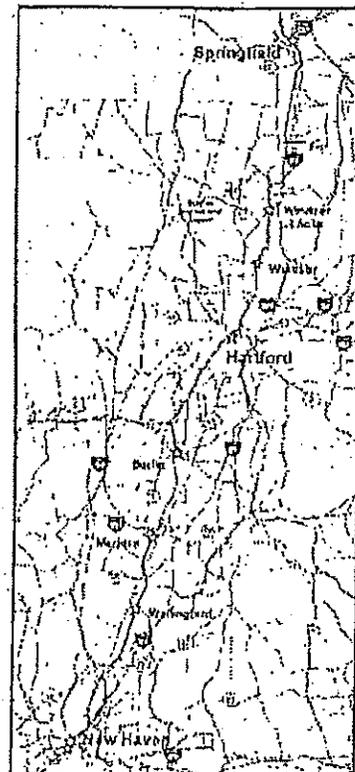
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Affidavit of Publication

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County of Fairfield

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New Haven - Hartford - Springfield

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KATHLEEN VITKO
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Kathleen Vitko

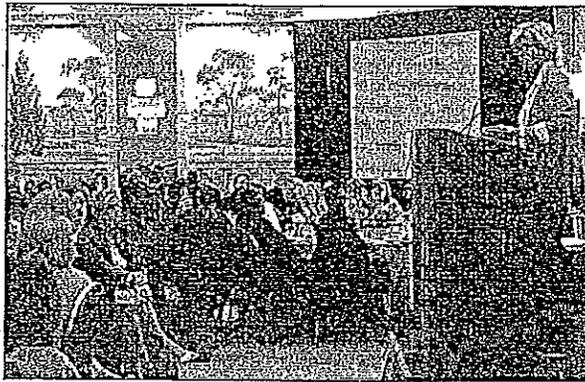
Estrella de Hollywood testimonia sobre abuso y violencia doméstica

STAMFORD El pasado jueves el Centro de Crisis de Violencia Doméstica (DVCC), reunió a más de quinientos invitados para festejar su décimo aniversario, en el conocido "Almuerzo Anual de Primavera", en donde se anunció que en estos años de vida, el Centro ha recaudado más de un millón de dólares en donaciones, contando con el testimonio de la actriz de Hollywood, Meredith Baxter sobre cómo afrontar la violencia doméstica y salir de ella.

En el almuerzo, al que asistieron mujeres de organizaciones del Condado de Fairfield y de forma masiva 126 socios del Country Club Birchwood de Westport en el hotel Marriott de Stamford, la Directora Ejecutiva del DVCC, Rachelle Kucera Mehra, expresó que los números son importantes a la hora de proteger a las víctimas de violencia y anunció que en los últimos doce meses han asistido a 3.860 personas, en los poblados de Stamford, Norwalk, Westport, Darien, New Canaan, Wilton y Westton.

"Durante este período se han protegido a más de 25.000 niños, 423 mujeres y un hombre, se han refugiado en nuestras casas de seguridad. Una de cada diez vidas han sido impactadas de forma directa por el DVCC en el condado de Fairfield", acotó.

La actriz Meredith Baxter, conocida por interpretar a Elyse Keaton en "Family Ties",



Por tres dos horas consecutivas, los asistentes al Almuerzo Anual de Primavera, organizado por el DVCC, compartieron las cifras impresionantes sobre abuso, en las familias del Condado de Fairfield y el testimonio de la popular actriz de Hollywood, Meredith Baxter, autora del libro: "Desatado" (Untied), una crónica sobre su viaje a la violencia y su lucha por recuperar su autoestima.

en forma amena habló sobre cómo permitió ser abusada y cómo logró salir de ese ciclo. Y contó como creció en manos de sus hijeras, sin la figura materna y cómo ingresó de manera sutil en el mundo del abuso y violencia doméstica tras casarse siendo aun una adolescente, trayendo consigo una historia de abuso infantil desde su hogar.

"La violencia doméstica está en todas partes y en silencio por eso la permitimos que continúe. Los niños no hablan de esto, las mujeres no hablan tampoco. "Tenemos que elevar nuestra voz para que la aceptemos, la encontremos y la hagamos ir", dijo.

E inmediatamente contó a la audiencia, como fuera de la pantalla la vida de Baxter estaba marcada por el contraste de su papel en la pantalla de televisión con la imagen de

una familia perfecta; pero que en privado estaba luchando en un matrimonio abusivo, el alcohol y las cicatrices emocionales que éstos dejan. "La gente me preguntaba cómo era el modelo de un matrimonio perfecta nada, porque tenía mucho que protegerme", anotó.

Y aunque no recuerda como fue el inicio de su recuperación, las palabras de su hija fueron detonantes para el cambio. "Fue en una Día de Acción de Gracias, estaba destrozada y llorando y mi hija mayor me dijo: "¿Qué estás esperando? Yo no voy a seguir con esto" y abrió la puerta y la casa empezó a tener otro significado. Era muy consciente de que mis hijas estaban mirándome y pensando que si seguía en esto, para ellos esto era el significado de ser madre. No podía soportar lo que mis hijos

estaban viendo", anotó.

Y empezó según su testimonio, una profunda interiorización de su ser y el inicio de una "reeducación" de sus patrones de conducta y de sus decisiones. Criada por una madre distante, que insistió en que sus hijos la llamaran por su nombre artístico, Baxter siguió el patrón de abuso que aprendió desde temprana edad.

"Me enteré de que mis sentimientos y mis necesidades no importaban. Yo no era ni amada ni digna de ser amada" "Mis sentimientos no estaban disponibles para mí desde hace décadas. Fui abusada desde pequeña y estaba preparándome para una vida de abuso", remarcó Baxter. Pero el coraje y la determinación logró vencer su búsqueda por un sentimiento saludable de autoestima, que se ha traducido en una relación sana con su familia y los demás.

Agradecimiento público de Peruanos Unidos de Connecticut



(PUC), hace extensivo su agradecimiento muy cordial a todos los establecimientos comerciales, medios de comunicación, organizaciones comunitarias, pequeños empresarios y personas particulares que se unieron a nosotros para ofrecer el grandioso Homenaje a la Madre, realizado el pasado sábado 5 del presente, a partir del mediodía hasta las 6 de la tarde, en los ambientes de la Cafetería del Domus Foundation.

Gracias al apoyo de todos ellos y el trabajo incansable y desinteresado de los miembros del PUC, se pudo culminar en una tarde de mucha diversión y entretenimiento, cuyo principal objetivo fue el de quitarles una

sonrisa a las Madres, quienes fueron las encargadas de la tarde.

Nuestros agradecimientos a: La Voz Hispana de Connecticut, DOMUS Foundation, Alberto Rojas International, Las Américas Market, Produce Market, Matthew's Bakery, González Law Office, Salones de Belleza: Lady's Unisex Salon, Cerero New Image Salon, Why Not Silvia's, Restaurantes: Omar, Fiesta, Maya, Misti, Daddy's Deli, Old House, El Chalán, Casa Villa, Bachata, McDonald's y Dunkin' Donuts.

A las organizaciones comunitarias: Chapiques Unidos de Stamford, Crisol-Acuarela, Damas Hispanas del Condado de Fairfield. Muchas personas particulares que donaron artículos para ser sorteados entre las madres asistentes. Y a los artistas: Trío Bohemial, Perd Rolk, El Charro de Jalisco y su pequeña hija Marian, Peruvian Yachay, Músicos Unidos de Stamford y Coro de Niños de la Iglesia Santa María, a Brian Amaya, Anahí Rossini y Bala Perri.

Muchas gracias y un Feliz Día de las Madres.

Stamford, 9 de mayo de 2012
La Junta Directiva del PUC



CONNECTICUT

DEPARTMENT OF TRANSPORTATION

visado de disponibilidad de documentación ambiental y audiencia pública

Ha sido publicada para el proyecto estatal No. 170-2296 una evaluación ambiental/Evaluación del impacto ambiental en Connecticut

PROYECTO DE LÍNEA DE ALTA VELOCIDAD PARA TRANSPORTE FERROVIARIO INTERCITY NEW HAVEN-HARTFORD-SPRINGFIELD

El documento ambiental estará ahora disponible el martes 8 de mayo de 2012. Habrá las siguientes audiencias públicas a las 7:00 pm:

Lugar 1ro.: **Torp Theatre, Davidson Hall Central Connecticut State University**
1615 Stanley Street,
New Britain, CT

Hora: 7:00 p.m.

Fecha: 7 de junio, 2012

Lugar 2do.: **Asnuntuck Community College**
170 Elm Street
Enfield, CT

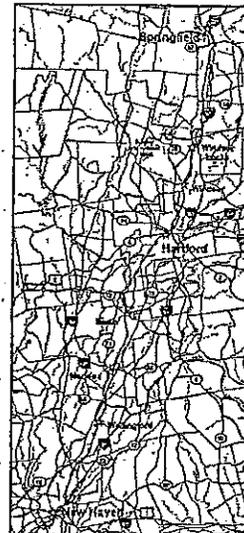
Hora: 7:00 p.m.

Fecha: 13 de junio, 2012

Lugar 3ro.: **North Haven High School**
221 Elm Street
North Haven, Connecticut

Hora: 7:00 p.m.

Fecha: 14 de junio, 2012



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Affidavit of Publication

State of Connecticut
County of Fairfield

I, Chris Gensur, a billing representative of Graystone Group Advertising, 2710 North Avenue, Suite 200, Bridgeport, CT 06604, do solemnly swear that on:

Date: May 17, 2012
Ad Title: CT DOT - No 170-2296
New Haven - Hartford - Springfield

Appeared in: La Voiz De Connecticut
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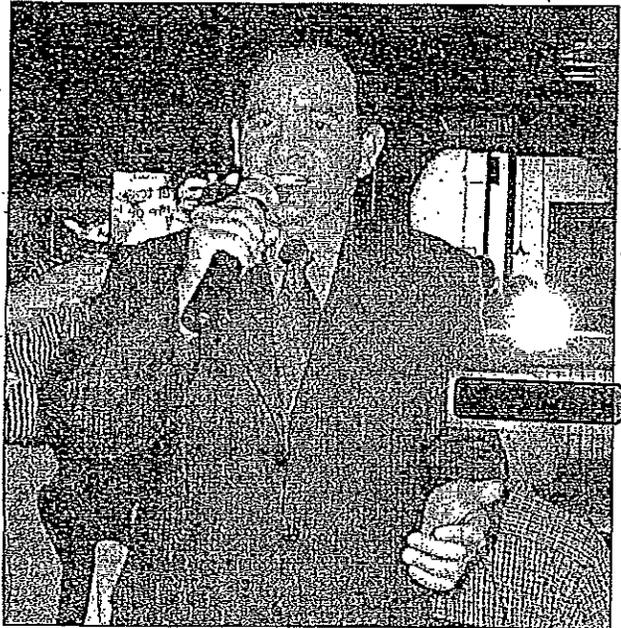
Subscribed and sworn to this 14th day of June, 2012 before me.

KATHLEEN VITKO
NOTARY PUBLIC
State of Connecticut
My Commission Expires
July 31, 2017

Notary Public

Kathleen Vitko

Los Ayer's de Colombia llenan "Casa Blanca"



Octavio Corrales, de la agrupación "Los Ayer's" y los hermanos Corrales de Colombia, en una gira por los Estados Unidos que durará un par de meses, cantó en el Casa Blanca, de 83 Wall Street, un lugar que semana a semana presenta espectáculos artísticos de factura Internacional.

STAMFORD. Composiciones de los años 60, 70 y 80, fue el repertorio que don Octavio Corrales, del recordado grupo los "Ayer's de Colombia", interpretó en una velada especial en el Restaurante Casa Blanca, como parte de una gira artística que el reconocido intérprete realiza en la Unión Americana.

Sencillo, con una voz dulce, a eso de las once de la noche, cuando literalmente no había en donde poner un pie

en el restaurante Casa Blanca, considerado hoy por hoy, uno de los centros nocturnos más acogedores de Norwalk; ingresó don Octavio Corrales y tras los acordes del estribillo del pasillo ecuatoriano "El Aguacate" entró cantando: "Tu eres mi amor... mi dicha y mi tesoro...", recibiendo en el mismo instante una "ola" de aplausos que más tarde se transformarían en cumplidos durante su intervención del artista.

Corrales, acompañado de la banda musical de su hijo, Juan David Corrales, recorrió la geografía colombiana de norte a sur, incluyendo al Ecuador, en donde varios temas de factoría ecuatoriana fueron coreados por los presentes como éxitos del país cafetalero.

Amor Senderito del Alma, Mama Vieja, como se adora el sol; fueron interpretados por Corrales, que con mucha versatilidad se cambió de un

género a otro, para culminar con varios pasillos del repertorio ecuatoriano.

Con mucho entusiasmo Corrales dijo por varias ocasiones: "¡Esta canción con mucho cariño y la cantamos todos!", en tanto que el animador insistentemente decía: "¡Hacemos una ola, la ola, la ola!", para prontamente permitir cantar a don Octavio Corrales.

Aun emocionado, por su presencia en Norwalk, Octavio Corrales agradeció al público la gentileza de acom-

pañarlo en esa noche, a la que calificó de importante porque se reencontró con su hija mayor que vive en Boston, su hijo Juan David, que reside en Stamford y trajo consigo a la pequeña que vive en país natal, Colombia.

"Hoy estoy emocionado porque me reencontré con mi público y con mis hijos", dijo el artista y presentó a su hija mayor, quien guiará un parecido con la joven actriz Bibiana Corrales, reconocida artista de telenovelas que se transmiten en la cadena de televisión Telemundo y por un papel importante en la novela Doña Bárbara.

Tras preguntarle por qué inició cantando el tema de César Segundo Guerrero Tamayo, el pasillo El Aguacate, Corrales respondió que tenía muy cariño por ese país y que sabía que su compositor vivió muchos años en Pasto,

Departamento de Nariño, en Colombia (su país natal) y que de antemano sabía la historia sobre esta composición, compuesta en Quito, canción que causa mucha controversia en cuanto al porqué de su nombre, porque en ninguna parte del tema hace relación a esta planta.

"Doña Juanita Guerrero Caicedo, hija del compositor, contó que su padre había salido en compañía de varios amigos a dar un paseo por el centro de la capital ecuatoriana, llevaba su guitarra y de cuando en cuando entonaban este pasillo que aún no tenía título. Al pasar por el mercado galantearon a una guambrita (joven) que en ese instante vendía y comía aguacates, le ofendieron un pipo que no le gustó y ella contestó la galantería echando la pepa del aguacate, de ahí el nombre del pasillo", contó el artista.



CONNECTICUT DEPARTMENT OF TRANSPORTATION

visó de disponibilidad de documentación ambiental y audiencia pública

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PROYECTO DE LÍNEA DE ALTA VELOCIDAD PARA TRANSPORTE FERROVIARIO INTERCITY NEW HAVEN-HARTFORD-SPRINGFIELD

El documento ambiental estará ahora disponible el martes 8 de mayo de 2012. Habrá las siguientes audiencias públicas a las 7:00 pm:

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1815 Stanley Street, New Britain, CT

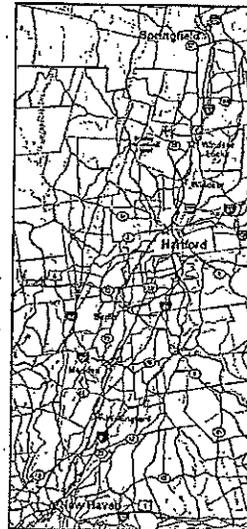
Hora: 7:00 p.m.
Fecha: 7 de junio, 2012

Lugar 2do.: Assnuntuck Community College
170 Elm Street, Enfield, CT

Hora: 7:00 p.m.
Fecha: 13 de junio, 2012

Lugar 3ro.: North Haven High School
221 Elm Street, North Haven, Connecticut

Hora: 7:00 p.m.
Fecha: 14 de junio, 2012



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El equipo de estudio estará disponible en cada audiencia, de 6:00 a 7:00 pm para discutir las mejores propuestas. La audiencia comenzará a las 7:00 pm. Los comentarios pueden ser hechos verbalmente o por escrito en cada audiencia pública. También se aceptarán comentarios enviados por correo o a través del portal: <http://www.nhhsrail.com> hasta el 22 de junio de 2012. Preguntas escritas o comentarios por correo, pueden ser enviados a: Mr. Mark W. Alexander, Transportation Assistant Planning Director, P.O. Box 317546, Newington, CT 06131-7546 o a la dirección de correo electrónico: Mark.W.Alexander@ct.gov

Hay disponibles copias del documento ambiental para su revisión en: City or Town Clerk's Office and the Public Libraries (bibliotecas públicas) de las municipalidades afectadas a lo largo del corredor ferroviario, South Central Regional Council of Governments (Concejos regionales de Gobierno), the Central Connecticut Regional Planning Agency, the Capitol Region Council of Governments, y la comisión Pioneer Valley Planning Commission.

Encuestas en español en Biblioteca Ferguson

STAMFORD. En un esfuerzo por determinar la mejor manera de asignar los recursos de la Biblioteca Ferguson, se llevará a cabo una encuesta en la comunidad para medir cómo los residentes de Stamford utilizan la biblioteca y qué servicios son más importantes para ellos. La información ayudará a guiar a la Biblioteca en el desarrollo de un nuevo plan estratégico, prevista para realizarse del 4 al 18 de junio.

La encuesta de dos semanas, que se extenderá 4 hasta 18 junio, estará disponible

en el sitio web de la Biblioteca, www.fergusonlibrary.org. También habrá copias de papel. El cuestionario ha sido traducido al español, ruso, francés y orllo, estimándose que puede ser contestado en alrededor de ocho minutos.

Las preguntas incluyen consultas sobre el uso de la biblioteca, cómo los clientes están o no satisfechos con los programas y servicios de biblioteca, y cuáles son los recursos más importantes para ellos.

También hay una serie de preguntas para los no usuarios que pueden ayudar a identi-

ficar mejor los servicios que los usuarios potenciales de la biblioteca. La encuesta ayudará al establecer prioridades sobre los gastos de Ferguson, y ayudará a orientar las decisiones sobre materiales, programación, horarios y otras operaciones de la biblioteca.

El estudio está siendo conducido por Berk Consulting, una empresa con sede en Seattle que recientemente completó una encuesta en la Biblioteca de Greenwich. Los Amigos de la Biblioteca Ferguson están financiando el proyecto.

Affidavit of Publication

State of Connecticut
County of Fairfield

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Date: May 24, 2012
Ad Title: CT DOT - No. 170-2296

New Haven - Hartford - Springfield

Appeared in: La Voz Hispana De Connecticut
publication and the newspaper extracts hereto annexed were clipped from the above named issue of said newspaper.

Subscribed and sworn to this 19th day of June, 2012 before me.

KATHLEEN VITKO
NOTARY PUBLIC
State of Connecticut
My Commission Expires
July 31, 2017

Kathleen Vitko

Notary Public

Barrio hispano se tiñe de sangre



Los niños de la escuela Side by Side, debieron ser escoltados por la policía, para llegar al lugar en donde estaban sus progenitores; mientras observaban disimuladamente el trabajo de investigación policial.
(Foto: Nancy Chapman)

caron a la escena a la altura de South Norwalk y se iniciaron las pruebas de identificación marcando cuatro casquillos de bala en la calle principal de South, marcando también un casquillo junto a una mancha de sangre en la vereda y se acordó el lugar con una cinta amarilla, a pesar de que la policía debió levantar con frecuencia la cinta para que los escolares y sus padres logren transitar, porque el incidente tuvo lugar cerca de la escuela Side by Side, a la altura de la calle Chestnut.

Investigadores en la escena. Aunque no hizo ningún comentario sobre la investigación, indicó que la policía estaba en buscando a cuatro hombres, "Los detectives están buscando a posibles testigos", afirmó. El tiroteo tuvo lugar alrededor de las 3:20 de la tarde. El Departamento de detectives tras el tiroteo se acor-

con los despachos de emergencia. Alrededor de una docena de oficiales e investigadores permanecieron en la escena hasta pasada las 6:00PM, aunque los oficiales dijeron no estar seguros de cuando el occiso se abrió a la calle. El Departamento de detectives tras el tiroteo se acor-

Es el segundo homicidio del año en Norwalk. José Bateman, 20 años, fue asesinado a balazos (3 de febrero, cerca del complejo de la Comunidad de Avalonbay Belden Avenue

NORWALK. Un hombre presuntamente de origen afroamericano murió tras recibir un disparo en el estómago mientras caminaba por la calle South Main en horas de la tarde, reportó la policía. El tiroteo ocurrió frente a 119 South Main St., cerca de la intersección de la avenida Woodward en South Norwalk, puso en la lista como el segundo asesinado en lo que va del año, corroboró el jefe de la Policía, Harry Rilling. El accidentado fue llevado al Hospital de Norwalk, donde fue declarado muerto, dijo el jefe policial, aunque el nombre de la víctima aun no fue revelado sino hasta que su familia sea notificada, aunque de acuerdo a varios testigos parece que era un hombre de origen hispano. El jefe policial dijo que el tiroteo no tiene relación con ningún incidente previo en el área. "Estalló una discusión y hubo un tiroteo en forma espontánea", dijo Rilling, después de haber hablado con los



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PROYECTO DE LÍNEA DE ALTA VELOCIDAD PARA TRANSPORTE FERROVIARIO INTERCITY NEW HAVEN-HARTFORD-SPRINGFIELD

El documento ambiental estará ahora disponible el martes 8 de mayo de 2012. Habrá las siguientes audiencias públicas a las 7:00 pm:

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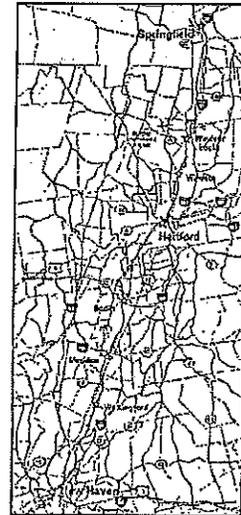
Hora: 7:00 p.m.
Fecha: 7 de junio, 2012

Lugar 2do.: Asnuntuck Community College
170 Elm Street
Enfield, CT

Hora: 7:00 p.m.
Fecha: 13 de junio, 2012

Lugar 3ro.: North Haven High School
221 Elm Street
North Haven, Connecticut

Hora: 7:00 p.m.
Fecha: 14 de junio, 2012



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Lugar 2do.: Asnuntuck Community
College
170 Elm Street
Enfield, CT

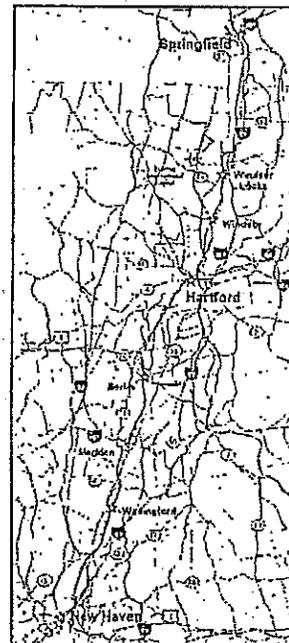
Hora: 7:00 p.m.

Fecha: 13 de junio, 2012

Lugar 3ro.: North Haven High School
221 Elm Street
North Haven, Connecticut

Hora: 7:00 p.m.

Fecha: 14 de junio, 2012



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of Governments, y la comisión Pioneer Valley Planning Commission.

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County of Fairfield

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Subscribed and sworn to this 14th day of June, 2012 before me.

KATHLEEN VITKO
NOTARY PUBLIC
State of Connecticut
My Commission Expires
July 31, 2017

Kathleen Vitko
Notary Public

From Page 1

Budget adopted, leaders change

the path which we're trying to go"

Democrats disagreed, saying certain lines of the budget were unnecessary and could be cut without disturbing the balance of the town. Barry Niccol and Christopher Painter suggested changes, including a \$94,000 reduction in the general budget. The money was surplus, and Painter said the funds should be allocated for the new fiscal year, which begins July 1. Democrats also disagreed with the addition of a new treasurer account position at Town Hall.

While Painter supported the addition of a new information technology position, Barry opposed the hiring of any new employees, especially because the Board of Education is cutting 22 positions.

"People of Southington are looking for a leaner, a more efficient type of government," Barry said. "This does not bring anything to this town in terms of saving money... I would be surprised if we are the only town in the state of Connecticut that's adding jobs."

"Maybe we are the only town (adding jobs), but we're Southington and we don't stay in the box," Romano responded.

"With the right people in place, there is more done for less," Republican Cheryl Lounsbury said. "We have to give Mr. Brumback the ability to work and do that."

Republicans Albert Natale agreed with Lounsbury, saying that the treasurer account position provides a skill set that's going to manage investments down the road.

"We have to give (Brumback) the freedom and liberty to increase and add positions," Lounsbury said.

The Republicans accused the Democrats of trying to micromanage the town manager, but the Democrats said they were doing their due diligence for the sake of saving taxpayers' money.

Democrats also disagreed with a \$50,000 line item for moving expenses for the North Center School facility. Palmieri said \$10,000 was put in last year's budget and it wasn't used. Salary increases were also up for debate, and Barry was vocal in opposition of a 9.5 percent raise for the superintendent of the highway department, while other department heads will only get 2.5 percent increases.

"It's a bad message for other town employees that aren't getting increases," Barry said. "I think it's ridiculous."

Brumback said the larger raise was offered due to increases made when the employee was hired.

Pocock said that an \$80,000 position in the Parks Department was scrapped, and the position was consolidated so the superintendent of the highway department would be given extra responsibilities. He said it's not right to leave someone uncompensated for taking on a larger workload.

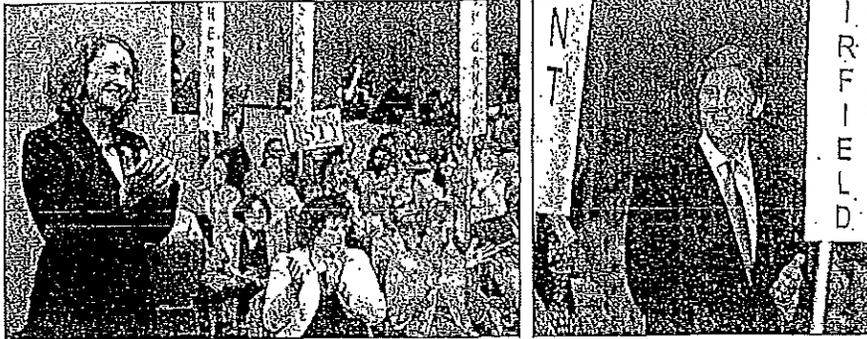
"We should be rewarding so others want to step up like that," Romano said.

Democrats also challenged a line item for hiring outside legal counsel and a \$100,000 flexibility account.

Democrats did win one battle, persuading Republicans to change the name of the flexibility account to a training account because the word "flexibility" was ambiguous.

"We didn't save any money but we got a name change," Barry said as a chorus of laughter rose from members of the public.

Record-Journal Commitment To Our Communities



Left: Elizabeth Esty applauds as Michael Long, a Simsbury delegate, speaks in support of her 5th Congressional District candidacy at the Democratic nominating convention Monday in Waterbury. Esty got 86 votes, qualifying for a primary. Right: Candidate Dan Roberti, of Kent, greets supporters at the start of the convention. Roberti got 54 votes, also qualifying for a primary to challenge the party's nominee, House Speaker Christopher Donovan, of Meriden.

Donovan nominated but faces primary challenges

ington Valley and Litchfield County hills. He is also expected to lead a swing district that enjoys strong Democratic support in New Britain, Waterbury and Meriden, but also has an abundance of Independent and GOP voters.

Donovan, a community organizer and a labor supporter, accepted his party nomination with a promise to protect workers' rights and benefits, Medicaid and Social Security.

"The issues that I care about cut across party lines," Donovan said at the convention, which was held at Rottella Intermediate Magnet School. "I'm talking about jobs. People want jobs. Democrats, Republicans, independents, they want jobs."

Donovan said the cheering crowd. "They want to be treated fairly. That's the underlying theme here. Let's have fairness and dignity and respect for everyone."

Esty's supporters made the case to delegates that she would be more electable in a tough district. Esty won a state House seat in 2006 against a

votes, even in Simsbury and split Farmington's 14 delegates with Donovan.

Roberti's backers made the same case for their candidate. The next public relations presentation won half of his support from Waterbury, which changed its final votes in his favor. Waterbury, which had 42 votes, gave 24 to Roberti by the end of the evening.

Robert Heids, who nominated Roberti, said he was the only candidate who has a job plan. Roberti also received backing from Southbury, whose town committee spokeswoman, Vivian Templeton, credited him for helping to shift longtime Republican control to the Democrats in the last election.

"Dan is the perfect Democrat to follow in Chris Murphy's footsteps," Templeton said. Donovan received six complimentary speeches from all parts of the district.

Meriden City Councilor Cathy Ballista credited him for the work he has done on behalf of children in the district.

Donovan was instrumental in getting Meriden grants to help finance a \$20 million renovation project on two high schools in the fifth hour of the recent legislative session.

He is a champion of the children," Ballista said. "It was he who ultimately led the way." Candidate Randy Yile was also nominated at Monday's convention but failed to garner any delegate votes.

The 5th District race is considered by both parties to be the most hotly contested race in the state. Republicans will choose their party's nominee from a field of five candidates on Friday. Several GOP candidates have said they, too, will seek a primary.

Frant Porch News: Sunday, Monday and Wednesday in the Record-Journal



GOP opponent in Republican-leaning Cheshire. But she lost the seat two years later to Rep. Alfred Adinolfi, R-Cheshire. She blames the loss and that of other Democrats on their voter to repeal the death penalty.

Simsbury delegate Michael Long reminded voters what's at stake if the party doesn't reelect a candidate who can win against the GOP's nominee, Esty, a Cheshire lawyer, said her goal for the evening was "to get on the ballot."

"This could be the most important convention of my life," Long said. "We are facing a critical time in our country's history. We can't win without independents. We can't take this campaign for granted."

Esty appeared to have support in the Farmington Valley and Litchfield County towns. She won all of Cheshire's 13

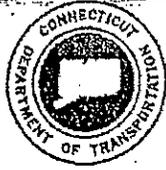
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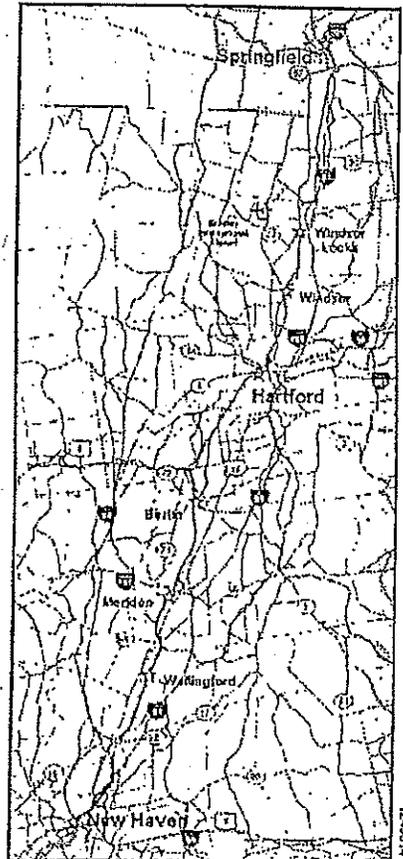
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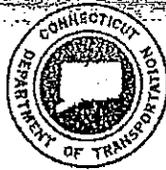
Appeared in: Meriden Journal
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Subscribed and sworn to this 14th day of June, 2012 before me.

KATHLEEN VITKO
NOTARY PUBLIC
State of Connecticut
My Commission Expires
July 31, 2017

Notary Public

Kathleen Vitko



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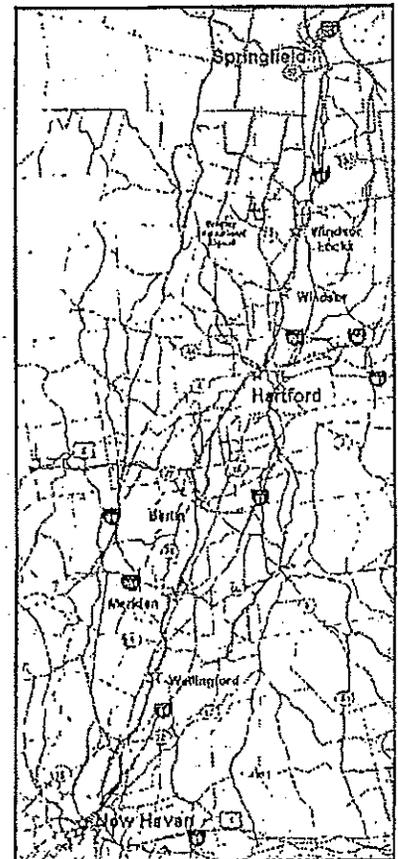
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Notary Public

O'Donnell: Private investigator arrested for witness tampering

Continued from A1

her some portion of any wrongful incarceration settlement with the state. If the men were ultimately freed from prison.

"What can I say," attorney Joseph Vione, who represents Gould, said when reached by phone and told that O'Donnell had been arrested. "My heart goes out to him."

In another twist, Vione said he received certain information in a closed hearing in the case in Superior Court in Vermont a few weeks ago that moved him to seek permission to withdraw from the case.

He said some information "was not forthcoming to me."

Vione could not discuss any other details, but said his partner, attorney Peter Tsimbrikos, who originally was Taylor's counsel, was allowed to withdraw and Vione will stay until the latest habeas is concluded.

Both attorneys were special public defenders in the habeas petitions.

Asked if he thought O'Donnell badgered witnesses, Vione said: "One man's badgering is another man's aggressive investigation."

A former prosecutor in Massachusetts, Vione said he was never there when O'Donnell talked to witnesses, as that would have been improper. "I let investigators and police do their investigations," he said.

Vione said he has worked with O'Donnell, who was a former Cheshire police officer and a retired Division of Criminal Justice investigator, for several years. O'Donnell is a Connecticut licensed private investigator.

Siles said the investigators that her original testimony in 1995 was mainly true. "I would throw things in there that weren't true. You know that weren't, that weren't true, but as far as, you know, what I saw and what you know, the important stuff was true."

The investigators said she has both the stereo and television in her possession, and they were able to track down the purchase of the TV from a Walmart in Newington and match the credit

card purchase with O'Donnell's alleged signature.

Keith Wertz, a New Haven detective at the time of the Vega murder, was one of the first on the scene. He told investigators that whenever he would meet him, O'Donnell would push his theories about the murder, which he thought was "weird."

Another witness in the case, Pamela Youmans, told the court that O'Donnell, "he hounds me."

In the first habeas in 2009, she denied statements allegedly attributed to her by O'Donnell. O'Donnell was released on \$75,000 bail and will be arraigned May 15 in Superior Court. Tsimbrikos could not be reached for comment.

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Ross: Yale appears in court in crash

Continued from A1

In a crowded parking lot near the Yale Bowl before the annual Yale-Harvard football game in November.

A police inspection of the truck showed no defect.

An arrest warrant affidavit released Monday revealed never-disclosed accounts of the morning.

A passenger in the U-Haul was driving the vehicle "shot forward" and Ross yelled, "I've got my foot on the brake. I'm not pressing the accelerator."

Another passenger stated the truck was stopped for three or four minutes behind a large crowd at the entrance gate to Lot D when the truck accelerated and began running over people.

A Yale sergeant stated she looked toward the U-Haul when its engine started revving loudly and said Ross appeared angry, with his face contorted and his mouth wide open, as if he was yelling.

Sarah Short, one of the victims, explained she was walking with a friend, Nancy Barry, across the entrance of the lot when

she heard an engine rev. She looked back because the sound got louder and closer.

Then the U-Haul ran over them.

"She stated she could hear the thumping of their bodies underneath the truck," the affidavit said.

In all, three women were run down. Barry died.

Ross is not charged with intentionally accelerating into the crowd, but rather with negligent homicide with a motor vehicle and reckless driving, both misdemeanors.

The defense points to a vehicle malfunction. Jonathan Barclay, who was in the front seat with Ross, told police that the vehicle lurched forward and was uncontrollable as Ross yelled that he wasn't pushing the accelerator.

Patrick Dolan, another Yale student, told Dow in a deposition that he rented the U-Haul a day earlier and noticed that the engine seemed particularly loud and the brakes and accelerator seemed slightly unresponsive.

He said he considered returning the vehicle to U-Haul for an exchange, but didn't.

At the time of the crash, about 20 to 30 people were in the rear of the truck along with several kegs as the vehicle traveled from an off-campus fraternity to the league party planned for that is known simply as "The Game."

In court Monday, Dow requested a continuance in the case until June 12, and asked that Ross, who came to court with his mother, not be required to appear.

Outside, Dow disputed that Ross got any special consideration from his state's attorney's office.

He said the prosecution did accommodate his client to allow him to surrender on the warrant Friday after his last final exam of the semester, but he stated that accommodation probably would be made for anybody facing a "six-month misdemeanor" charge.

"It's a tragedy all around," said Dow. "This guy is the type of client that is a privilege to represent."

A certified mechanic for New Haven police inspected the truck and stated that he found no issue with the gas pedal, cable or throttle body, and that the brake system was in good working order.

East Haven: Police in compliance with law

Continued from A1

New Haven, Hamden, Ansonia, Milford and Cheshire, already were in compliance.

Other police departments that compiled in 2010 and continued to comply in 2011 include Avon, Bloomfield, Bristol, Danbury, Greenwich, Middletown, Naugatuck, New Canaan, Newington, New London, Plainville, Ridgefield, Simsbury, Stamford, Stannington, Trumbull, Vernon, Waterbury, Watertown, Westport, Watersfield and Wilton.

Two police departments that compiled in 2010, Norwalk and Putnam, failed to fully comply last year, according to the African-American Affairs Commission's figures.

While East Haven has yet to supply profling reports since January, the month that four police officers were arrested in race-driven FBI raids and charged with violating the civil rights of Latino residents, African-American Affairs Commission Executive Director Glenn Cassis said the law simply requires that the reports be filed by the end of the year.

The commission "didn't receive any reports on East Haven traffic stops in 2010, and received only April through September in 2009, Cassis said.

East Haven police Lt. Joseph Sims affirmed that the department hadn't sent reports since January and said it's "because we were so busy here. Right now, we're down personnel in the record room, as well. We're trying to keep up as best we can."

The African-American Affairs Commission released the 2010 figures at the request of the New Haven Register. The release came coincidentally on the day that the state House of Representatives approved by

a 131-1 vote changes to strengthen the act, which will result in the reports going instead to the Office of Policy & Management.

The bill passed the Senate last month by a 31-3 vote and will become law once Gov. Dannel Malloy signs it, said Michael Avon, East Haven's former state representative and now undersecretary for criminal justice policy and planning for OGA.

The African-American Affairs Commission has said on numerous occasions — and Cassis reiterated — that the commission lacks the resources to do any such analysis.

The hope in moving the reports into OPA's purview is that the change will enable more detailed analysis.

Lawlor said that at his request, Andrew Clark, director of the Institute for Municipal and Regional Policy at Central Connecticut State University, has formed a new committee, chaired by former longtime state Rep. Bill Dwyon, D-New Haven, and former Branford Chief of Police John DeCarlo, to determine how the profling information can best be used.

An effort to better use the information got a new push earlier this year.

That's when the state Department of Transportation informed Lawlor that a \$1.8 million federal grant from several years earlier designed specifically to collect and use traffic stop information was about to expire, he said.

The new committee "will determine what process to go through to begin crunching the numbers," Lawlor said.

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Leclaire: Prosecutor wants suspect in jail

Continued from A1

police said.

According to Stroll, Leclaire, 43, has 55 "entireties" on criminal cases since 1993, including 15 for murder. He is on bail awaiting sentencing June 5 in three cases with charges ranging from unlawful restraint to threatening.

Stroll said he spoke to the victim in the kidnapping Monday morning and she said she was very concerned and wanted Leclaire prosecuted to the fullest extent of the law.

In 2006, Leclaire emerged as a central figure in a scandal involving several Madison police officers who allegedly consorted with prostitutes while on the job. Leclaire, a childhood friend of a Madison cop, brought prostitutes to "hang out" with his friend and three other officers working the night shift.

On Saturday, police said, he instructed the victim to make withdrawals from banks and then take him to buy crack cocaine. The ordeal began after 1 a.m. when the woman got off work.

Hours later, she was able to text the words "help" and "get the police" to her boyfriend, who lives in Wallingford and already had been looking for her because she was late.

He gave information to Wallingford police, who relayed it to New Haven police, and the boyfriend returned to his search.

She sent text messages to him several other times and, at one point, told him she was near a supermarket on Grand Avenue. He spotted the car, informed police where they were and waved officers down when they began arriving.

Officers said the woman was crying hysterically and kept saying "help me, help me."

With police detaining Leclaire at gunpoint, she jumped out of the car and "ran for her life" to her boyfriend, police wrote in a report.

The woman said Leclaire at one point made her drive toward Wooster Street and said he wanted to kiss her and to have her "touch him." When she refused, he said he would rape her if she didn't. She refused again.

Leclaire was taken to the hospital for chest pains because he said he smoked too much crack cocaine, police said. While he was being loaded into the ambulance, he allegedly shouted a racial epithet at a black police officer.

When questioned by police, Leclaire said the victim offered him a ride after he claimed his car broke down and needed \$19.

He told police he lied and really needed the money for drugs. After he bought the drugs, he said he got back into the woman's car and they drove off. He said he didn't remember the rest.

His public defender asked the judge that Leclaire receive mental health treatment and be placed on a strict watch



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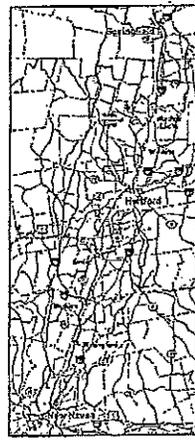
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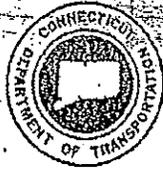
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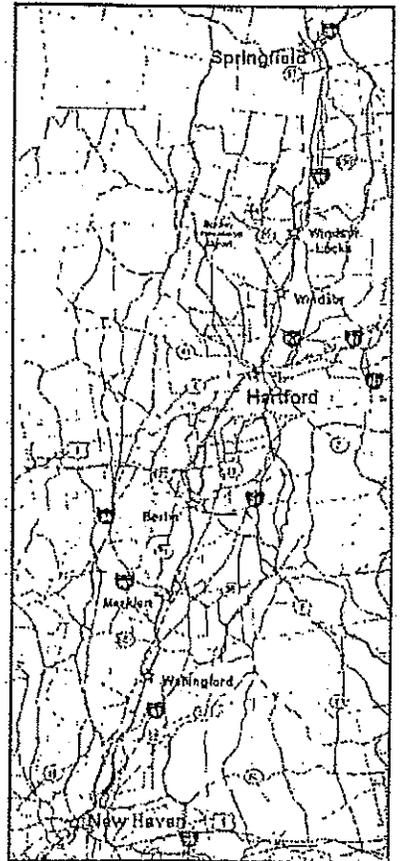
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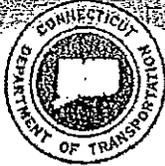
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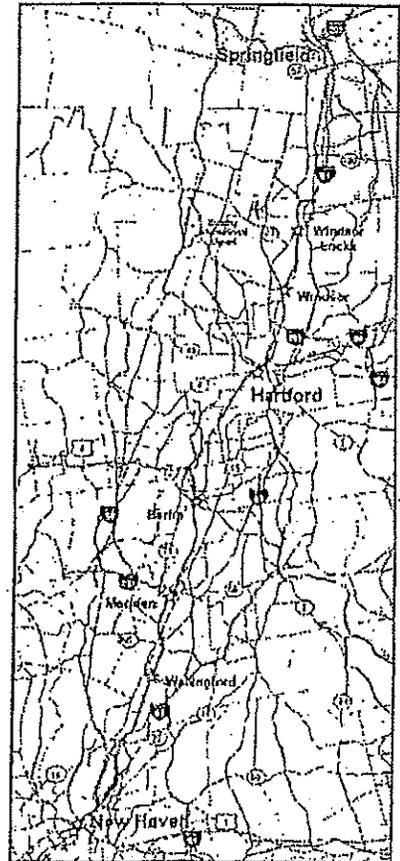
Time: 7:00 p.m.
Date: June 7, 2012

Location 2: Asnuntuck Community
College
170 Elm Street
Enfield, CT

Time: 7:00 p.m.
Date: June 13, 2012

Location 3: North Haven
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221 Elm Street
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Time: 7:00 p.m.
Date: June 14, 2012



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Kathleen Vitko

NATION/WORLD

G-8 leaders hope Greece stays in eurozone

Associated Press

CAMP DAVID, Md. — President Barack Obama and other leaders of the Group of Eight industrial nations expressed hope Saturday that Greece will remain in the eurozone as it huddled for a shite-sleeves summit aimed at keeping Europe's economic troubles from multiplying and spreading around the world.

Circling up around a

table in a rustic cabin at the presidential retreat, the leaders underlined the need to keep bringing deficits down through austerity measures.

They also agreed that targeted spending on things like education and public works projects is needed to solve Europe's financial crisis.

"All of us are absolutely committed to making sure that growth and stability and fiscal consolidation

are part of an overall package," Obama said.

Germany's Angela Merkel, for her part, said growth and deficit cutting reinforced each other "and that we have to work on both threads, and the participants have made that clear, and I think that is great progress."

The G-8 leaders' joint statement from the woods of Camp David reflected both hope and a recognition of the daunting economic challenges they face.

"The global recovery shows signs of promise, but significant headwinds persist," it said.

The summit brought together leaders of the United States, Germany, France, Canada, Italy, Britain, Russia and Japan in an effort to figure out how to tame Europe's debt crisis while also increasing the demand for goods and spurring job growth.

Officials: Trio planned to attack Obama's HQ

Associated Press

CHICAGO — Three men accused of making Molotov cocktails had been planning to attack President Barack Obama's campaign headquarters, Mayor Rahm Emanuel's home and other targets during this week-end's NATO summit, prosecutors said Saturday.

The three were arrested Wednesday when police raided an apartment on the city's South Side ahead of the two-day meeting.

Defense attorneys alleged that the arrests were an effort to scare the thousands of people expected to protest at the gathering of world leaders. They told a judge that undercover police were

the ones who brought the Molotov cocktails.

This is just propaganda to create a climate of fear," defense attorney Michael Duetsch said.

Duetsch said two undercover police officers or informants who called themselves "Moby" and "Gloves" were also arrested during the Wednesday

raid, and defense attorneys said they later lost track of the two. "Who believes this is all a setup and entrapment to the highest degree," Duetsch said.

The trio was charged with providing material support for terrorism, conspiracy to commit terrorism and possession of explosives.

Search is on for 4 missing in boat crash

Associated Press

Crews searched Saturday for four people missing after an overnight boat crash on the Mississippi River in Iowa, although one official said it was mostly a recovery effort.

Iowa Department of Natural Resources spokesman Kevin Baskins said the timing of the collision, about 1:45 a.m. Saturday, made it difficult for rescuers to find people in the water.

He said, the effort is "leaning more toward recovery" than rescue.

"Officers in boats, at this point, looking for anything we can find," Baskins said, describing the search.

Two jon boats — flat-bottomed boats often made



of aluminum — crashed in the O'Connell Slough area of the Mississippi River for any evidence of four missing people thrown from a boat after an early morning crash Saturday near Burlington Iowa.

department. One of the boats was carrying 11 people; the other had a single passenger.

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"Adopt-A-Pet for the Holidays" will publish Wednesday, May 23, 2012. Deadline for participation is Monday, May 21, 2012. Call Lynn at 203-789-5415 for more information.

New Haven Register

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The environmental document is available beginning on Tuesday, May 8, 2012 for a 45 day public review and comment period. Public Hearings will be held at the following locations:

Location 1: Torp Theatre, Davidson Hall, Central Connecticut State University, 1615 Stanley Street, New Britain, CT
Time: 7:00 p.m.
Date: June 7, 2012

Location 2: Asenluck Community College, 170 Elm Street, Enfield, CT
Time: 7:00 p.m.
Date: June 13, 2012

Location 3: North Haven High School, 221 Elm Street, North Haven, CT
Time: 7:00 p.m.
Date: June 14, 2012

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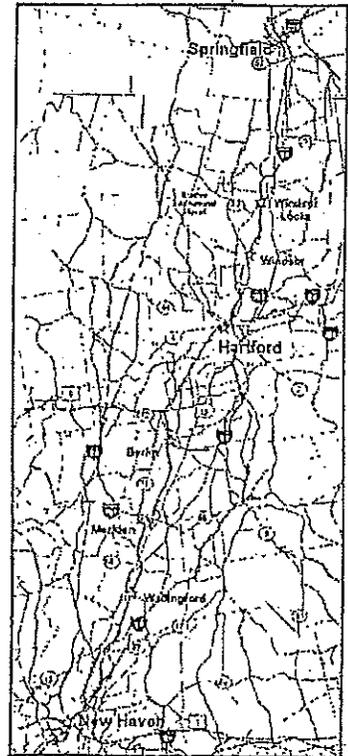
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Kathleen Vitko

Casino: Westfield interested in gaming firm

Continued from Page A1

city's revenue. Budget directors would be a thing of the past, he said.

Kaplan recently is grappling with a \$1 million budget

deficit. "These come along once in a while," Kaplan said about the potential for a development as big as a casino. "My job is to provide an opportunity for the best possible situa-

tion, and then have the voters decide." Of the 220 acres, 50 would be used for the casino, and entertainment and other development would use 85 acres, he said. There still would be a lot

of open space left on the site, he said. Jeffrey R. Daley, city administrator, said he hasn't heard too much from the public about a potential casino.

"I think people are waiting to see exactly what the impacts would be on our community," Daley said. "It's an important decision that the city of Westfield's residents are going to have to make."

Son of Connecticut, which has long considered land across from the Massachusetts Turnpike exit in Palmer for a resort casino and Amstar Casino of Las Vegas, which bought the former Westfield site off Page Boulevard in Springfield for \$18 million for a casino. Hard Rock had wanted to pursue a license for Holyoke, but the mayor is strongly opposed to casino.

Sarno: State aid sought in Boston

Continued from Page A1

ministrative and financial officer Timothy Haines, director of finance for the city and city budget director Leona D'Amico, "welcome that."

Springfield is heavily dependent on state aid, receiving about \$330 million in state aid this fiscal year. That comes to about 61 percent of Springfield's annual \$532 million budget, which includes \$330 million for schools.

Since he took office in 2003, Sarno said a category of aid called unrestricted state aid has dropped by about 58 percent to about \$29 million this year. Sarno said that cut has caused some fiscal problems in the city.

Sarno has warned of possible layoffs and service cuts for the new fiscal year that starts July 1.

Sarno offered four major proposals for helping the city's budget, including revising Proposition 2.5.

Under the tax-limiting law, the total annual property tax revenue raised by a community cannot exceed 2.5 percent of the assessed value of all taxable property contained in a community. Springfield is currently at that ceiling, meaning it cannot raise the total amount of property tax revenues and it also cannot collect extra property taxes from any new growth, according to Sarno's package.

Sarno said he would like legislation that would allow Springfield to effectively collect about \$1.1 million annually in new growth beyond the ceiling imposed by Proposition 2.5.

pal government, which includes police, fire, public works and other services, to spend money on school department operations.

Right now, the city must also pay for about \$23 million a year in school-related transportation costs. Sarno said he would support legislation that would shift some or all of that spending to the schools.

Under state law, the city also has to pay for debt service for schools including borrowing for replacing the Elias Brooks Elementary School and replacing the Stacy Dwyer School, both damaged by the June 1 tornado. Sarno's package indicates he would support a bill that would allow surplus money in the school department to pay for the city's required portion of debt service.

state scholarship fund for public school students in the city of Springfield.

Sarno told legislative leaders that the city is taking steps independently to raise revenues, such as a proposal to increase the annual trash fee by as much as \$10 a year for five years. Sarno said the city is also using tools provided in recent state laws, such as past approval of a local 0.75 percent tax on meals and a plan to seek approval of the City Council to hike the city's motor tax to 0.6 percent.

In a phone interview, Barbara Anderson, executive director of Citizens for Limited Taxation, said she would oppose Sarno's proposed change to Proposition 2.5, saying it could cause a "slippery slope" in reductions to the law.

Moose: Return to Massachusetts

Continued from Page A1

most of its history. However, finding that densities of deer were as much as five times higher on these tracts than outside the watershed, and that young trees were not growing on the forest floor because of deer browsing and the impact of their droppings, the state instituted a deer hunting season at Quabbin in 1931 to bring down their densities. Soon, deer densities there were in line with the rest of the

state, and tree seedlings reappeared on the forest floor.

However, moose can eat perhaps 10 times as much in a day as a deer. "If you go to a stand that was harvested 10 years ago," Watkins said, "you'll still see signs of recent browse and the impact of their droppings. But many of the saplings and young trees will have already grown well over your head and out of browse range for moose."

"So are there impacts? Yes," he said. "But moose aren't in my opinion overly impacting regeneration, certainly not to the extent that deer did prior to the turn of the 1900s."

"If you look at the end of winter, it seems (moose) have eaten everything in (an acre cleared by logging)," said Watkins, "but by the end of summer, that regrowth is remarkable." He believes the state's moose population has, for the time being, stabilized

at about 1,000 animals.

Thomas J. O'Shea, the assistant director of wildlife for the state Division of Fisheries and Wildlife, said a bill to legalize moose hunting in the state Legislature to establish an annual moose hunting season, and there is again such a bill this year.

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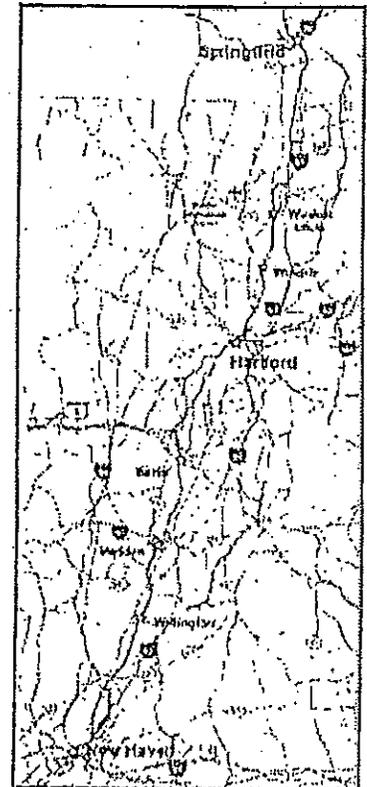
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Kathleen Vitko

Run: Industry backs Sen. Brown's bid

Continued from Page A1

Many donors who gave the maximum allowable contribution to Brown's campaign committee are top players in financial firms in Massachusetts. James Moore, managing director of the Bancorp Capital Partners, CEO of Bancorp Boston, a financial services investment company, Paul Edgerley, a managing director at Bain Capital, who is also a major donor to Romney and others.

Brown also has a joint fundraising committee with the National Republican Senatorial Committee, which can accept donations of up to \$5,000, with \$5,000 going to Brown and the rest to the committee. A Boston Globe analysis of Brown's joint committee found that nearly half the donations to that committee came from the financial industry. Among those who gave the maximum donation, according to Federal Election Commission filings, were: Timothy Barrow, a general partner at the investment firm Hartz Partners in Boston; Howard Cox of Florida, an advisory partner at the Greylock Partners venture capital firm; and Walter Donovan, chief investment officer of Boston's Putnam Investments.

The Massachusetts Democratic Party has asked Brown for making donations from Wall Street, arguing that Brown is beholden to financial interests. After JPMorgan Chase lost \$2 billion in a trading error, the Massachusetts Democratic Party pointed out that JPMorgan employees donated \$20,000 to Brown. Warren spokeswoman Alice Harty said, "Scott Brown is taking care of Wall Street and Wall Street is taking care of Scott Brown. That's how it works in the financial industry. We're not there for the man and Elizabeth Warren will be there taking them out, standing up

for hard-working families and seniors to protect their investments." Democrats point to Brown's attempt to weaken the Dodd-Frank bill. Brown was one of only three Republicans to support the bill. But he said his intent is to "strip" Dodd-Frank to eliminate a \$18 billion fee on major financial institutions and to weaken a provision that would have prohibited banks from making certain types of investments.

Brown spokesman Colin Reed said, "Scott Brown's fundraising is no different than President Obama, Elizabeth Warren, (Democratic Massachusetts Senator) John Kerry or the rest of the Democratic delegation, except Professor Warren hypocritically attacked Scott Brown for it. Scott Brown was the tie-breaking vote in favor of the Wall Street reform bill which imposed new regulations on the financial industry." Obama, a Democrat, received \$7.5 million from the financial, insurance, and real estate industries this election cycle, according to the Center for Responsive Politics; Romney got \$17.8 million; Kerry, the 2004 Democratic presidential nominee, got \$53,000 this election cycle and \$30 million during his career.

Reed Brown's bills are unusually high. Brown has received a total of \$4.1 million from the financial sector since he ran for Senate in a 2010 special election. Brown's predecessor, Democratic Sen. Edward Kennedy, got \$1.7 million from the financial sector between 1989 and 2010. Massachusetts Rep. Barney Frank, a Democrat, former chairman of the powerful House Financial Services Committee, and co-sponsor of Dodd-Frank, took \$1.3 million from the financial sector during his last run in 2010 and \$4.4 million since 1989. Messenger fell for several of Brown's major donors in the financial industry were not returned. Tabbot, of the Financial Services Roundtable, said he believes support for Brown

is not based on one particular vote. "If you look at his track record, he has a understanding of issues facing the financial services industry," Tabbot said. Economist Brian Gottlieb, principal at PolEcon Research, who has worked on either campaign, said it is natural that people in finance would be wary of Warren, given that she was considered to lead the Consumer Financial Protection Bureau. Warren also chaired a congressional committee overseeing the use of federal money for the Troubled Asset Relief Program, the bailout of financial institutions.

"An aggressive role around consumer protection would naturally, in any industry, make those in the industry that are regulated more nervous about a candidate," Gottlieb said. "People in the financial services industry have concerns about the financial protection agency, what it was, what it might do." Warren has been a strong proponent of government regulation of the financial industry. She fought against changes to the U.S. bankruptcy code, passed in 2003, that made it harder for individuals to file for bankruptcy. The financial industry has noticeably been the single

largest source of money in U.S. elections. Bob Blersack, senior fellow at the Center for Responsive Politics, said the industry will be particularly involved in a year when the balance of the Senate is up for grabs. "When control of the Senate is at stake and every Senate race is going to count for a lot this year, industries that feel there's a big regulatory agenda where the makeup of Congress is important to them will be involved in lots of places," he said. Steve Schorschberg is a political blogger for The Republican and MassLive.com. Follow her reporting at masslive.com/politics

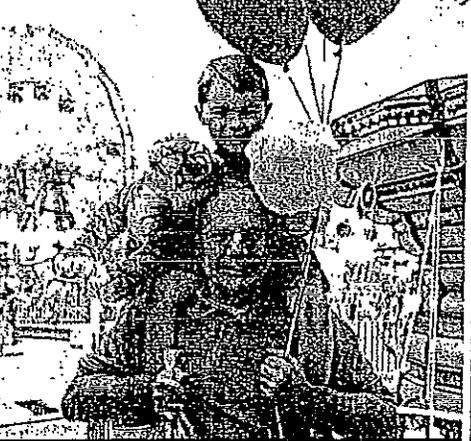
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*Source: Carelon Outcome Analysis System, January 2010 to March 2011

CONNECTICUT DEPARTMENT OF TRANSPORTATION

Notice of Availability of Environmental Document and Public Hearing

An Environmental Assessment/Connecticut Environmental Impact Evaluation has been published for State Project No. 170-2296

NEW HAVEN-HARTFORD-SPRINGFIELD LINE HIGH SPEED, INTERCITY PASSENGER RAIL PROJECT

The environmental document is available beginning on Tuesday, May 8, 2012 for a 45 day public review and comment period. Public Hearings will be held at the following locations:

Location 1: Torp Theatre, Davidson Hall Central Connecticut State University 1616 Stanley Street New Britain, CT	
Time: 7:00 p.m. Date: June 7, 2012	
Location 2: Annucluck Community College 170 Elm Street Enfield, CT	
Time: 7:00 p.m. Date: June 13, 2012	
Location 3: North Haven High School 221 Elm Street North Haven, CT	
Time: 7:00 p.m. Date: June 14, 2012	

PLEASE JOIN US AT THE PUBLIC HEARINGS IN JUNE.

All hearing locations are accessible in person with disabilities. Deaf and hearing impaired persons or persons speaking a language other than English wishing to attend these meetings and receiving an interpretation may make arrangements by contacting the Department's Office of Communications at (860) 394-3062 (voice only) at least five days prior to the meeting.

The study team will be available at each hearing from 6:00 p.m. - 7:00 p.m. to discuss the proposed improvements. The hearing presentation will begin at 7:00 p.m. Comments may be given verbally or in writing at each public hearing. Comments will also be accepted by mail or via the project website <http://www.cthighspeed.com> through June 31, 2012. Written comments or concerns sent by mail can be directed to Mr. Mark W. Alexander, Transportation Assistant, Project Director, P.O. Box 317204, Newington, CT 06111-7244 or at e-mail address: Mark.W.Alexander@dot.gov

A copy of the environmental document is available for review via the project website <http://www.cthighspeed.com> and at the Connecticut Department of Transportation, each City or Town Clerk's Office and the Public Libraries in the affected municipalities along the rail corridor, the South Central Regional Council of Governments, the Central Connecticut Regional Planning Agency, the Capital Region Council of Governments, and the Pioneer Valley Planning Commission.



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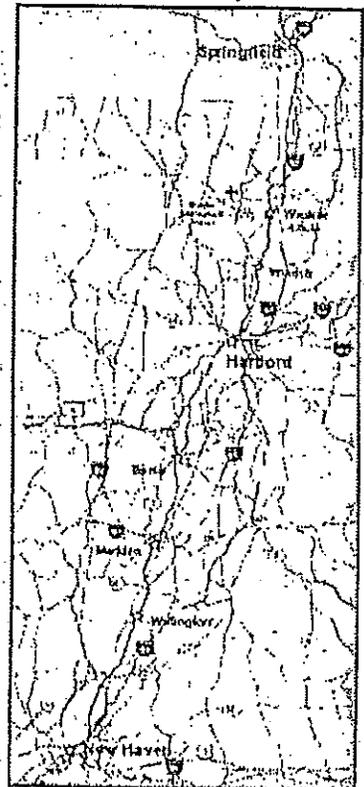
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Affidavit of Publication

State of Connecticut
County of Fairfield

I, Chris Gensur, a billing representative of Graystone Group Advertising, 2710 North Avenue, Suite 200, Bridgeport, CT 06604, do solemnly swear that on:

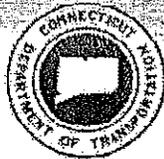
Date: May 15, 2012
Ad Title: CT DOT - Notice of Availability
State Project No. 170-2296

Appeared in: Spring Field Republican
publication and the newspaper extracts hereto annexed were clipped from the above named issue of said newspaper.

Subscribed and sworn to this 14th day of JUNE, 2012 before me.

KATHLEEN VITKO
NOTARY PUBLIC
State of Connecticut
My Commission Expires
July 31, 2017

Kathleen Vitko
Notary Public



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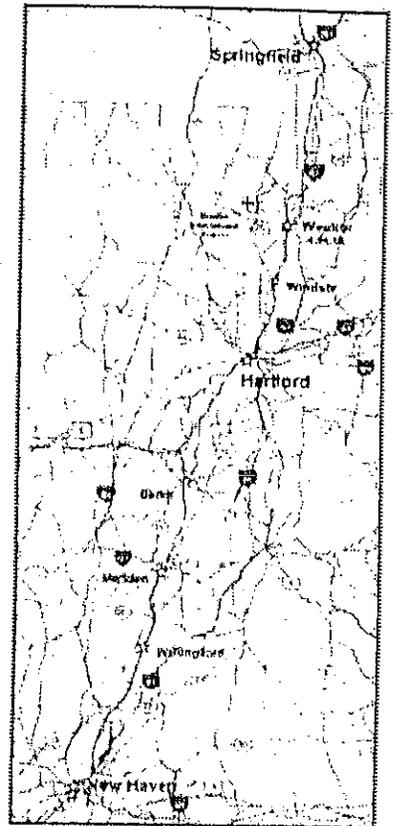
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Obama trying to undermine Romney's record on jobs

By KEN THOMAS
Associated Press

WASHINGTON — President Barack Obama tried Monday to launch Mitt Romney as a corporate titan who got rich by cutting rather than creating jobs, opening a new effort to undercut the Republican's claims that his background of business success is just what America needs in a time of deep economic uncertainty.

At the center of the Obama campaign effort are a new website, TV ad and online video including interviews with online workers at a Kansas City, Mo., steel mill that Romney's former private equity firm failed to successfully restructure. Workers lost jobs and health care benefits. Pensions were reduced.

"It was like a vampire. They came in and sucked the life out of us," says steelworker Jack Cobb, 44, John Wageman, 50, said. "Mitt Romney had a lot of money that they made off this plant. We view Mitt Romney as a job destroyer."

Countering the criticism, Romney's campaign said the former Massachusetts governor welcomes an off-season conversation with Obama about jobs. Romney's campaign has argued that he helped spur tens of thousands of jobs in the public and private sectors and pointed to a net job loss during Obama's presidency, most of which occurred during the first few months of his administration. Obama has touted the creation of 4.2 million new jobs over the last 20 months as his policies took hold.

Both candidates are seeking to pivot to voters' No. 1 issue, the economy, and away from the social issues that dominated after the president announced his support for gay marriage last week.

Obama steered clear of criticizing Romney during a comment speech at Barnard College in New York, though he included a passing reference to nearby Wall Street, saying "Some folks in the financial world have not exactly been model corporate citizens."

He left the more direct slandering to surrogates and dispatched Vice President Joe Biden to Ohio to castigate Romney over his record at Bain. That was one of several events planned to highlight the Republican's role in the company he founded.

Romney, meanwhile, prepared to deliver a speech Tuesday in Iowa on reducing the huge federal debt. Monday's dreary global financial backdrop set the stage for a sharp debate in the coming weeks between the candidates over their competing economic philosophies, and it highlighted the public's unhappiness with big business and government institutions alike.

JPMorgan Chase's decision that it lost more than \$2 billion on bad trading bets renewed calls for tighter oversight of the nation's biggest financial institutions, a position that Obama has supported, and Romney has opposed. And world markets were tentative as Greece weighed whether to renege on the terms of its painful austerity program and leave the Euro currency bloc. That could hurt Obama's attempts to accelerate the limp U.S. recovery.

Obama said in an interview with ABC's "The View" that JPMorgan Chase's loss underscored the need for Wall Street rules passed by Congress two years ago, many of which have not yet gone into effect. Obama said his bank less also showed the sharp differences over how he and Romney view the role of government.

Romney and Obama alike contend that in a nation where unemployment is hovering around 8 percent, voters will choose a president based on economic arguments. Obama is trying to persuade voters to stick with him as he heralds an economic rebound, as sluggish as it is. Romney counters that only he — with his deep background in business — knows how to jumpstart the nation's job market. The two men have little in common in their views of how to get the country moving.

Obama has pumped money into the economy to stimulate growth and has cut some taxes, though he also advocates raising taxes on the wealthiest Americans. Romney argues that lower taxes across the board and fewer government restrictions are the answer. Both are trying to win over an electorate that is furious with Wall Street and distrustful of corporations, and Obama's new campaign effort was squarely aimed at working-class voters, a group that has been reluctant to support the president in the past.

Obama's TV ad was scheduled to run in five battleground states: Iowa, Ohio, Pennsylvania, Virginia and Colorado — and was part of a larger \$25 million, month-long campaign. But it was limited in scope.

Republican officials bristled at the ad but said the Obama team was airing the 2-minute spot only on Wednesday in the five states. The ad was expected to run during the evening news, directing viewers to an Obama website about Romney's economic record and a longer, 6-minute version of the ad appearing online.

As Obama's campaign was raising Romney's record in private equity, the president himself was leading to two fundraisers, including a \$35,000-per-person dinner at the home of Hamilton "Tony" James, the president of Blackstone Group, the nation's largest private equity firm.

Romney's campaign, meanwhile, aggressively worked behind the scenes to counter the Obama campaign's Bain message.

It released a Web video about a successful steel company that Bain invested in called Steel Dynamics. The video shows steelworkers describing the company as the embodiment of the American dream, noting that the company grew from a workforce of 1,400 to more than 5,000. That video was not immediately planned for television.

Romney also dispatched Ed Gillespie, a senior campaign strategist, to a conference call with conservative bloggers on Monday to respond to the Obama ad. The campaign planned to frame the attacks on Romney's record at Bain as an "attack on free enterprise," and to cast the auto bailout as an example of private equity at work.

Romney advisers predicted the attacks on the presidential Republican nominee's record at Bain would backfire because most voters understand that some business ventures succeed while others fail. "This is someone who understands how the economy works, and I think most Americans also understand that there's never guaranteed success," Gillespie said.

Earlier this year, Romney previewed his counterattack, saying of Obama: "He's now been a venture capitalist at So-

lyndra, Ficker, Tesla and he's been a private equity guy at General Motors and Chrysler. So I'll be talking about his record when I'm facing him."

Romney, a multimillionaire, left Bain in 1999 to run the Salt Lake City Olympic Games but maintained a financial interest in the company after departing. He has said that his firm had a strong overall track record, creating jobs in prominent companies like Staples and Sports Authority, while acknowledging that some companies Bain invested in were unsuccessful.

The Obama ad, which reprises criticism leveled at Romney during the Republican primaries, focuses on one of those unsuccessful companies.

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The Commonwealth of Massachusetts

DEPARTMENT OF PUBLIC UTILITIES

NOTICE OF FILING, PUBLIC HEARING AND PROCEDURAL CONFERENCE

D.U.L. 12-25 April 23, 2012

Petition of Bay State Gas Company, d/b/a Columbia Gas of Massachusetts, pursuant to C.G.S. § 164, § 91 and 22C C.M.R. § 7.00 (2011), for Approval of a General Increase in Gas Distribution Rates Proposed in Towns of Andover, Mass. 01810 through 120.

On April 11, 2012, Bay State Gas Company, d/b/a Columbia Gas of Massachusetts ("Columbia Gas" or "Company") filed a petition with the Department of Public Utilities ("Department") for approval of a general increase in gas distribution rates. The Department has scheduled this public hearing to discuss the proposed effective date of the proposed rate increase and November 1, 2012, to evaluate the investment requirements of the Company's request. The Company was last granted an increase in gas distribution rates in 2009 in Andover, Massachusetts, D.U.L. 09-01 (2009).

In the instant filing, the Company seeks to increase its annual revenues by \$18.2 million. The Company claims that its proposed increase represents a 6.4 percent increase in current annual operating revenues, or an average of nearly 16 percent increase in the Company's base distribution revenues.

If the petition filed by Columbia Gas is approved as requested, the Company states that its proposed rates will have the following effect:

- a typical residential heating customer using 132 therms of gas per month during the winter season will experience a monthly bill increase of \$9.67 (a 5.7 percent increase in current rates);
- a typical residential heating customer using 22 therms of gas per month during the summer season will experience a monthly bill increase of \$2.84 (a 2.8 percent increase in current rates);
- a typical residential non-heating customer using 10 therms of gas per month during the winter season will experience a monthly bill increase of \$5.94 (a 12.1 percent increase in current rates);
- a typical residential non-heating customer using 12 therms of gas per month during the summer season will experience a monthly bill increase of \$2.52 (a 11.2 percent increase in current rates); and
- commercial and industrial customers will experience an annual bill increase in the range of 3.0 percent to 6.3 percent depending upon the amount of their usage.

For specific bill impacts, please contact the Company as indicated below.

In addition to the required rate increase the Company has put forth other proposals, including the modification of its discounting mechanism and proposed infrastructure replacement. Both of which were approved in the Company's last rate case, D.U.L. 09-36, and proposals related to the recovery of incremental property tax expense and the cost and savings related to the prior rate of Northern Utilities, of which the Company had held all of the stock.

The Attorney General of the Commonwealth of Massachusetts ("Attorney General") has filed a notice to intervene in this matter pursuant to C.G.S. § 12, § 11E. Further pursuant to C.G.S. § 12, § 11B, the Attorney General has filed a notice of retention of experts and consultants to assist in her investigation of the Company's filing, and has requested the Department to suspend its review of the proposed rate increase pursuant to C.G.S. § 12, § 11B) the costs incurred by the Attorney General in her retention of experts and consultants may be recovered in rate.

The Department has scheduled public hearings to receive comments on the Company's proposal as follows:

- May 22, 2012, at 7:00 p.m., at the South Junior High School Auditorium, 105 Keith Avenue East, Brockton, MA 01923.
- May 23, 2012, at 7:00 p.m., at the Springfield City Council-School Committee Chambers, Springfield City Hall, 35 Court Street, Springfield, MA 01103.
- May 24, 2012, at 7:00 p.m., at the Lawrence City Council Chambers, Lawrence City Hall, 200 Common Street, Lawrence, MA 01840.

A procedural conference in this matter will be held at the Department's office on May 17, 2012, at 2:00 p.m. Persons interested in commenting on the Company's filing may appear at any of the public hearings or may file written comments by the close of business (5:00 p.m.) on May 24, 2012.

Any person who desires to participate in the evidentiary phase of this proceeding must file a written petition for leave to intervene or to participate in the proceeding no later than the date of business (5:00 p.m.) on May 11, 2012. A petition filed late may be allowed as a matter of course, unless good cause is shown, for leave under 22C C.M.R. § 1.04(4). To be allowed, a petition under 22C C.M.R. § 1.04(1) must satisfy the standing requirements of C.G.S. § 30A, § 10.

Any person who seeks to intervene in this matter and also desires to contest the Attorney General's notice of retention of experts and consultants must file the intervenor's petition on or before the date of business (5:00 p.m.) on May 11, 2012.

Written comments, petitions for leave to intervene or to participate, and comments on the Attorney General's notice of retention of experts and consultants should be addressed to Mark D. Harlow, Secretary, Department of Public Utilities, One South Station, Boston, MA 02110. Receipt by the Department, and mailing, constitutes filing.

In addition to the above filing requirements, two (2) copies of all materials filed with the Department should be sent to Susan L. Yarnes, Hearing Officer, Department of Public Utilities, One South Station, 5th Floor, Boston, MA 02110; one (1) copy of the documents should be sent to the Company's counsel, Robert J. Kogan, Esq., Kogan Vitale LLP, 245 Franklin Street, Boston, MA 02110; and one (1) copy of these documents should be sent to Mark W. Rogers, Assistant Attorney General, Office of Regulatory Advocacy, One Ashburton Place, Boston, MA 02110.

Further, in addition to paper filings with the Department, all documents also should be submitted to the Department in electronic format using one of the following methods: (1) by e-mail attachment to regaffairs@dpuc.state.ma.us; or (2) on a 3.5" disk or CD-ROM. The text of the original, disk, or CD-ROM must specify: (1) the docket number of the proceeding (D.U.L. 12-25); (2) the name of the person or company submitting the filing; and (3) a brief description of the filing. Documents filed electronically also include the name, title, and telephone number of a person to contact in the event of questions about the filing. All documents submitted in electronic format will be posted on the Department's website: <http://www.mass.gov/dpu>.

A copy of the Company's filing is available for inspection during regular business hours at the following locations: the Department of Public Utilities, 220 State Street, 10th Floor, Boston, MA 02110; Brockton Public Library, Main Library, 204 Alan Street, Brockton, MA 01923; Lawrence Public Library, Main Library, 15 Lawrence, MA 01840; Northampton Public Library, 20 Van Sice, Northampton, MA 01062; the Company's offices, 200 Fittsley Parkway, Westborough, MA 01581; and the Department's offices, One South Station, 5th Floor, Boston, MA 02110. The filing also is available on the Department's website: <http://www.mass.gov/dpu>. Any person desiring further information regarding the Company's filing should contact the Company, Robert J. Kogan, Esq., at (617) 951-1820.

A copy of the Attorney General's notice of retention of experts and consultants is available for inspection at the locations above and during regular business hours at the Attorney General's office, One Ashburton Place, Boston, MA 02110; and at the Department's office, One South Station, 5th Floor, Boston, MA 02110. The Attorney General's notice of retention of experts and consultants also is available on the Department's website: <http://www.mass.gov/dpu>. Any person desiring further information regarding the Attorney General's notice of retention of experts and consultants should contact Mark W. Rogers, Assistant Attorney General, at (617) 727-2200.

Any person desiring further information regarding this matter should contact Susan L. Yarnes, Hearing Officer, Department of Public Utilities at (617) 303-3000.

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